

**CITY OF FORT COLLINS
TYPE 1 ADMINISTRATIVE HEARING
FINDINGS AND DECISION**

HEARING DATE: October 29, 2025

PROJECT NAME: Prospect Ridge Mixed Use – Lot 4

CASE NUMBER: PDP # 250003

APPLICANT
REPRESENTATIVE: Andy Reese
Kimley-Horn and Associates, Inc.
3325 S. Timberline Road
Fort Collins, CO 80525

OWNER: Pacific North Enterprises, LLC
900 Castleton Drive, Suite 118
Castle Rock, CO 80109

HEARING OFFICER: Marcus McAskin

PROJECT DESCRIPTION & BACKGROUND: The Prospect Ridge Mixed Use – Lot 4 Project Development Plan #250003 (the “**PDP**”) proposes to construct a four-story mixed-use building with 48 residential units and 9,352 square feet of ground-floor retail space designed for future commercial and office use (the “**Project**”) on an approximately 2.38-acre parcel within Lot 4 in the Rudolph Farm Subdivision, identified as Parcel #8715306004, situated at the northwest corner of Carriage Parkway and Prospect Ridge Drive (the “**Subject Property**”).

The Subject Property is located within the General Commercial zone district, with an I-25 Activity Center overlay, and the PDP does not propose a zoning change. Access to the Subject Property is provided via Prospect Ridge Road, with an emergency access entrance along Carriage Parkway, both established under a prior approved plan.

The Project will include a mixed-use building that provides approximately 48 residential units, approximately 9,352 square feet of retail space, and approximately 1,650 square feet of amenity area. The amenity areas will be located within Level 2 and Level 4 of the building and will consist of gathering space and seating areas. Twenty-four (24) garage structures, including one (1) accessible garage, are also being proposed. Other proposed improvements include associated infrastructure that consists of site grading, utility service installation, parking and sidewalk improvements, and landscaping.

The Subject Property, historically used for dry-land farming, is currently undergoing grading, utility installation, and road construction as part of a separate infrastructure plan. The Subject Property and surrounding area have been subject to various development proposals, removed from the floodplain through an upstream stormwater project, and incorporated into the boundary of the Rudolph Farm Metro District. The site is designated as Planning Area 2 (PA-2) in the amended 2023 Overall Development Plan (the “**ODP**”), which specifies that PA-2 supports density and land uses consistent with the General Commercial zone district at the time of application.

There are no requests for modifications of standards or for alternative compliance associated with the Project.

The Subject Property and surrounding properties are further described in the Development Review Staff Report prepared for the October 29th public hearing on the PDP. A copy of the staff report is attached to and incorporated in this decision as ATTACHMENT A (the “**Staff Report**”). Additional background on the Project and analysis of the PDP’s compliance with the City of Fort Collins Land Use Code (the “**LUC**”) and with the City’s Comprehensive Plan are provided in the Staff Report. The Project is further detailed in the plan documents included in the record of this case (reference the documents identified in the “Evidence” section below).

The Staff Report recommends approval of the PDP.

SUMMARY OF DECISION: Approved.

ZONE DISTRICT: General Commercial (CG).

HEARING: The Hearing Officer opened the virtual public hearing on Wednesday, October 29, 2025, at approximately 5:30 p.m., and reviewed the Order of Proceedings and Rules of Conduct for Administrative Hearings with the Applicant and members of the public present.

EVIDENCE: Prior to or at the hearing, the Hearing Officer accepted the following documents as part of the record of this proceeding:

1. Staff Report inclusive of Exhibit A to Report (summary of applicable standards) (33 pages) (attached to this Decision as ATTACHMENT A).
2. Project Narrative (2 pages).
3. Site Plan (2 pages).
4. Landscape Plan (4 pages).
5. Architectural Elevations (7 pages).
6. Overall Development Plan (1 page).
7. Preliminary Utility Plans (12 pages).
8. Lighting Plan (3 pages).
9. Drainage Report dated July 9, 2025 (54 pages).
10. Traffic Study dated July 9, 2025 (13 pages).
11. Fire Access Exhibit (1 page).

12. Resolution 2018-028 of the City Council of the City of Fort Collins Approving the Consolidated Service Plan for the Rudolph Farms Metropolitan District Nos. 1-6, dated March 6, 2018.
13. Confirmation of Publication dated October 14, 2025, evidencing proof of publication of Notice of Hearing in *The Fort Collins Coloradoan* on October 14, 2025.
14. Notice of Public Hearing dated October 14, 2025.
15. PowerPoint presentation prepared by Staff for the October 29th public hearing (33 slides).
16. The City's Comprehensive Plan, Land Use Code, and the formally promulgated polices of the City are all considered part of the record considered by the Hearing Officer.
17. Rules of Conduct for Administrative Hearings.

TESTIMONY: The following persons testified or participated during the virtual hearing:

From the City: Kai Kleer, Senior City Planner
Shawn Mellinger, City Engineering (did not provide comment)

From the Applicant: Andy Reese
Kimley-Horn and Associates, Inc.
3325 S Timberline Road
Fort Collins, CO 80525

Michael Noda, Partner (did not provide comment)
Daniel Wilde, Project Designer (did not provide comment)
NEO Studio LLC
3560 Walnut Street, Unit A
Denver, CO 80205

From the Public: None.

The public comment portion of the hearing was opened at approximately 6:21 p.m. and closed at approximately 6:21 p.m.

The virtual public hearing was closed at approximately 6:23 p.m.

FINDINGS

1. Evidence presented to the Hearing Officer established the fact that notice of the public hearing was properly posted, mailed, and published.
2. The Staff Findings set forth in the Staff Report are incorporated herein as findings of the Hearing Officer.

3. Based on testimony provided at the virtual public hearing and a review of the materials in the record of this case, the Hearing Officer concludes as follows:
 - A. The PDP satisfies and aligns with the purposes of the LUC set forth in LUC §§ 1.2.2(A) through (N). Specifically, the Project complies with LUC § 1.2.2(A), as it is consistent with the City Plan and its adopted elements.
 - B. The PDP complies with the applicable standards of Article 2, Article 3, Article 4, Article 5 and Article 7 of the LUC.
 - C. The PDP complies with the applicable procedural requirements of Article 6 of the LUC.

DECISION

Based on the findings set forth above, the Hearing Officer hereby approves the Prospect Ridge Mixed Use – Lot 4 Project Development Plan (PDP # 250003) for the Subject Property as submitted.

DATED this 3rd day of November, 2025.



Marcus A. McAskin Hearing Officer

ATTACHMENT A

Staff Report

Prospect Ridge Mixed Use – Lot 4 (PDP # 250003)

STAFF REPORT

Administrative Hearing



Project No.: PDP250003

Project Address: Parcel 8715306004. No listed address.

Project Zoning: General Commercial (G-C)

Project Applicant: Andy Reese
Kimley-Horn
3325 S Timberline Road
Fort Collins, CO 80525

Property Owner: Pacific North Enterprises, LLC
900 Castleton Drive, Suite 118
Castle Rock, CO 80109

City Staff: Kai Kleer, Senior City Planner

1. PROJECT DESCRIPTION

The Project Development Plan proposes the development of a 2.38-acre site, identified as Lot 4 in the Rudolph Farm Subdivision, with Parcel Number 8715306004, situated at the northwest corner of Carriage Parkway and Prospect Ridge Drive. The development comprises a single mixed-use building with 48 residential units and 9,352 square feet of ground-floor retail space, accompanied by detached garages to serve future residents. The northern boundary of the site adjoins a utility and irrigation ditch corridor containing the Timnath Reservoir Inlet Canal, Greely Water Line, and Larimer Weld Canal. The eastern and southern boundaries are adjacent to the local and collector street system. The project is subject to a Type 1 Review process, with an Administrative Hearing Officer as the decision maker.



Figure 1: Vicinity Map

2. STAFF RECOMMENDATION

Staff recommends approval of the Project Development Plan (PDP)

3. EXECUTIVE SUMMARY

The Prospect Ridge Mixed-Use Lot 4 Project Development Plan is located within the General Commercial zone district, with an I-25 Activity Center overlay, at the northwest corner of Carriage Parkway and Prospect Ridge Drive. In 2018, the City Council approved the formation of the Rudolph Farm Metro District. The formation allows the district to issue debt to fund improvements to the Prospect/I-25 interchange and site infrastructure that was approved as part of the Rudolph Farm Infrastructure Project. The proposed development includes a four-story mixed-use building with 48 residential units and 9,352 square feet of ground-floor retail space designed for future commercial and office use. Access to the site is provided via Prospect Ridge Road, with emergency access entrance along Carriage Parkway, both established under a prior approved plan. The building features a contemporary architectural design, with the ground floor incorporating distinct elements such as a covered patio, storefront window system, and corner pediment to differentiate the retail and residential components and enhance the building's relationship to the street corner.

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4. KEY CONSIDERATIONS

The project was submitted in July 2025 and proceeded through two rounds of review. The project had minor considerations with primary emphasis of review being focused on architectural design of the building and multi-family garages. The applicant team worked diligently to respond to context-specific standards of the I-25 Activity Center and newly adopted mixed-use building standards.

5. PUBLIC FEEDBACK

As part of the public hearing notice requirements, the City notified all property owners within 1,000 feet or more of the project boundary. At the time of the project's submission, a sign was posted along the project area's perimeter to indicate that the project is under review.

A neighborhood meeting is not required for Type 1 development applications and, therefore, was not held.

Previous input from the infrastructure plan and a nearby proposed multi-family project, approved by the Planning and Zoning Commission in August, has primarily focused on traffic impacts to nearby residential neighborhoods, including Fox Grove and Clydesdale Estates (located approximately one-quarter mile north of the site).

City staff has worked with the applicant to respond to these concerns early on with the infrastructure project. The applicant is providing a collector street design that utilizes on-street parking and traffic calming measures that will be implemented through the infrastructure plan.

6. SITE BACKGROUND

The subject property, historically used for dry-land farming, is currently undergoing grading, utility installation, and road construction as part of a separate infrastructure plan. It was annexed into the City of Fort Collins as part of the Galatia Annexation in 1991 (see Figure 2: Galatia Annexation Boundary). Since annexation, the site and surrounding area have been subject to various development proposals, removed from the floodplain through an upstream stormwater project, and incorporated into the boundary of the Rudolph Farm Metro District. The site is designated as Planning Area 2 (PA-2) in the amended 2023 Overall Development Plan (ODP), which specifies that PA-2 supports density and land uses consistent with the General Commercial zone district at the time of application. The ODP establishes parameters for development, ensuring coordination of infrastructure needs—such as streets, paths, drainage, access, and site constraints—across multiple development applications and future developers.

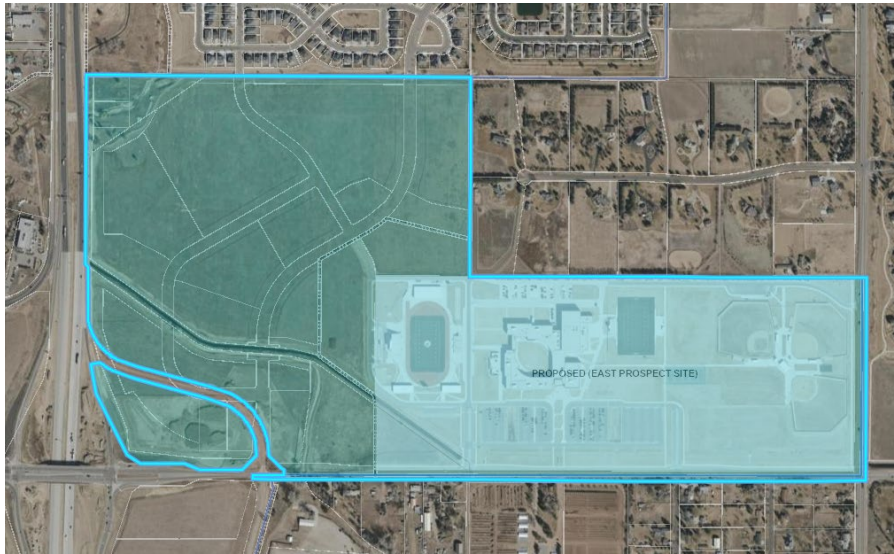


Figure 2: Galatia Annexation Boundary

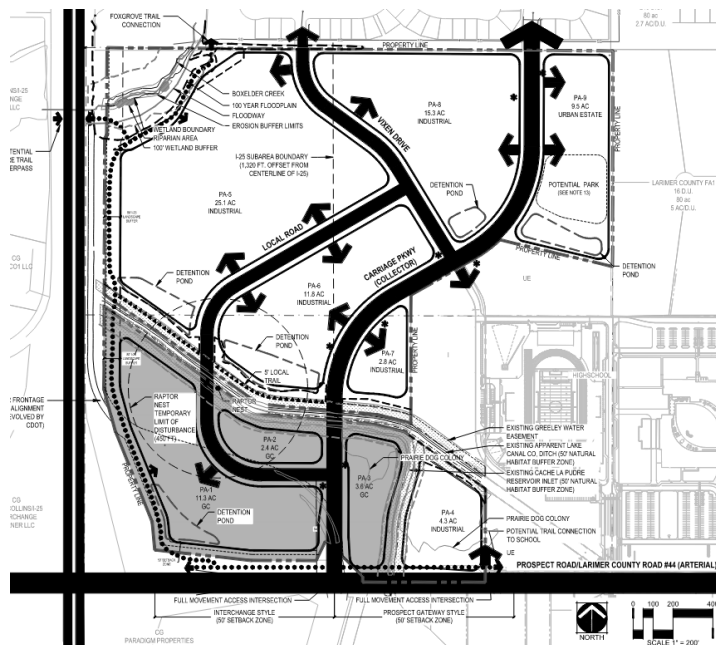


Figure 3: Prospect/I-25 Overall Development Plan (2023)

7. COMPREHENSIVE PLAN & POLICY ALIGNMENT

The purpose of this section is to assess the proposed development project's alignment with the community's long-term vision, as outlined in the City's Comprehensive Plan (City Plan) and relevant subarea plans. City Plan serves as the overarching policy framework for the city's growth and development, addressing key areas such as public safety, neighborhood quality, economic vitality, environmental services, parks and open spaces, utility services, and transit and transportation infrastructure. Subarea plans, as components of the City Plan, focus on specific neighborhoods, providing detailed policies, strategies, and recommended actions tailored to their unique contexts while remaining consistent with citywide objectives. This analysis employs a hierarchical approach, starting with high-level regional and citywide goals and, where applicable, narrowing to subarea plan considerations.

A. City Plan (2019)

The project aligns with the community's vision for new neighborhoods by incorporating and addressing many of the Neighborhood Livability and Social Health Policies, which include:

- Is located within a priority location for infill and redevelopment (Policy LIV 2.2)
- Enhances the community's I-25/Prospect gateway appearance through building architecture. (Policy LIV 3.3)
- Provides context-sensitive design using natural, earth tone materials and colors that respond to community and gateway characteristics which include buff sandstone. (Policy LIV 3.6)
- Incorporates context-sensitive building features such as the pedimented corner to respond to the lot's frontage at an intersection. (Policy LIV 4.2)
- Creates more opportunities for housing choice. (Principle LIV 5)

B. I-25 SUBAREA PLAN (2003) & NORTHERN COLORADO REGIONAL COMMUNITIES I-25 CORRIDOR PLAN (2001)

The I-25 Subarea Plan and the Northern Colorado Regional Communities I-25 Corridor Plan were developed concurrently, with overlapping boundaries along the I-25 corridor in Northern Colorado. The regional plan, adopted by Fort Collins on November 20, 2001, focused broadly on establishing a baseline and enhanced set of design standards for the entire corridor spanning from north Fort Collins to south of Berthoud. In 2003, the City adopted the enhanced standards from the corridor plan through the development of a more specific subarea plan that more deeply considered local issues and opportunities of land use, natural areas, design, and local character as it relates to Fort Collins relationship to the interstate. Specifically, the subarea plan builds on the regional plan, emphasizing specific zoning and development guidelines to maintain openness and views to the mountains, designates Activity Centers where concentrated development is appropriate, and implements standards to prevent a commercial strip development along I-25.

Residential development near I-25 is tightly regulated and limits low-density single-family homes from being within a quarter-mile of the highway to try and help mitigate noise impacts from the freeway. The plan also preserves open lands, such as the City's Resource Recovery Farm, and promotes multi-modal transportation options, and provides block standards intended to promote multi-modal transportation system and to reduce reliance on I-25 for local trips.

Staff would like to note that one major element of the subarea plan that has since changed is the anticipated commuter rail service to the area. During the development of the subarea plan it was assumed through the 1999 North Front Range Transportation Alternatives Feasibility Study that a number of regional improvements would take place which included the implementation of a commuter rail system. Although this element is out-of-date and the preferred alternative would now utilize existing rail lines that travel parallel to South College Avenue, the previously contemplated park and ride service is still envisioned for the I-25/Mulberry Activity Center. Additionally the trends of development continue to move toward I-25 as highlighted in City Plan in which the City, Timnath, and Windsor are focused on

establishing attractive gateways and attracting land uses that help repay the substantial community investment in interchange beautification.

Regarding the proposed development plan, staff finds that the project is consistent with the plan's vision and policies. The plan explicitly supports mixed-use development in the I-25/Prospect Road Activity Center and also provides the following alignment with the following stated principles, policies, and goals of the Plan:

- Policy I-25-LU-1.1 Subarea land use will focus on several activity centers surrounded by residential neighborhoods. Activity centers should be designed enabling new development to support efficient alternative modes of transportation. Activity centers should provide a mix of urban uses, including employment, commercial, and residential
- Policy I-25-LU-3.3 Multi-family residential uses should locate in either activity centers to benefit and help support alternative modes of transportation, or within the Low Density Mixed-Use Neighborhood area within 1/2-mile north and south of East Mulberry Street
- Principle I-25-CAD-1: The subarea's community entryways via I-25 should be designed to create a sense of place and a positive experience.
- Policy I-25-T-1.1 The subarea's transportation system will support the development of interconnected regional and local transit, bicycle connections, and an integrated pedestrian system.

Overall, staff finds that the project aligns with all stated principle and policy objectives of the subarea plan. The I-25 Corridor Plan (2003, which was built to be in alignment with City Plan land use policy, guides development in the northeast to create a variety of housing choices, pedestrian-friendly neighborhoods, and access to parks, trails, and open spaces. The plan acknowledges that existing irrigation ditches and canals will limit the ability to provide street crossing, act as important wildlife and multi-modal trail corridors, and will need to be improved in various ways to address stormwater issues and allow for future development capacity within the subarea.

The project addresses much of the principle and policy guidance within the subarea plan by:

- Providing a mix of urban uses within a designated Activity Center that include both commercial and residential into a mixed-use building (Policy I-25 LU-1.1).
- Providing a housing type that allows for housing opportunities for a variety of income levels. (Principle I-25-HSG-1).
- Incorporating a friendly architectural style designed to create a sense of place and positive experience. (Principle I-25-CAD-1)
- Provide multi-family housing that's located within an activity center to benefit and help support alternative modes of transportation (Policy I-25-LU-3.3).

8. DEVELOPMENT PLAN ANALYSIS & FINDINGS

This organization of the following sections are arranged in a specific order to address how a project responds to critical considerations of a development site. The organization demonstrates, in importance and hierarchy, how and in which ways a site can be designed. Land development is shaped first by its ecology, hydrology, geology, and views before considering the location and alignment of streets, pathways, and utilities that create the framework of public and private spaces. Although other topics such as historic preservation and building design are critically important to the community, the framework of private and public space creates the setting for how a building or development responds and integrates into the community.

A. Natural Habitat, Drainageways, Parks, and Trails

Applicable LUC Standards and Related Plans and Policy

5.4 - Development Infrastructure

5.5.5 – Parks and Trails

5.6.1 – Natural Habitat & Features

[North Front Range Metropolitan Active Transportation Plan](#) (2021)

[Fort Collins Strategic Trails Plan](#) (2025)

Staff Analysis and Findings

Overview

Much of the work required to be considered by this section has been completed through the planning, approval, and current construction of the [Rudolph Farm Infrastructure Plan](#). The Plan includes elements to address Natural Habitat Buffer Zones, I-25 buffer yards, Prospect Road Streetscape Program, ditch corridors, regional stormwater facilities, utility, and transportation infrastructure.

Drainageways & Stormwater

This project proposes utilizing the drainage flow as designed within the Rudolph Farm Infrastructure Plan (Figure 4: Drainage Plan) and will provide a lower impervious surface than anticipated and directs all flows towards Rain Garden 1 and Pond 1 through the adjoining street stormwater system. During larger storm events that are expected to cause Pond 1 to overflow, stormwater is designed to enter a 24" culvert that eventually connects into Boxelder Creek on the west side of the interstate. No additional on-site measures or considerations are required.

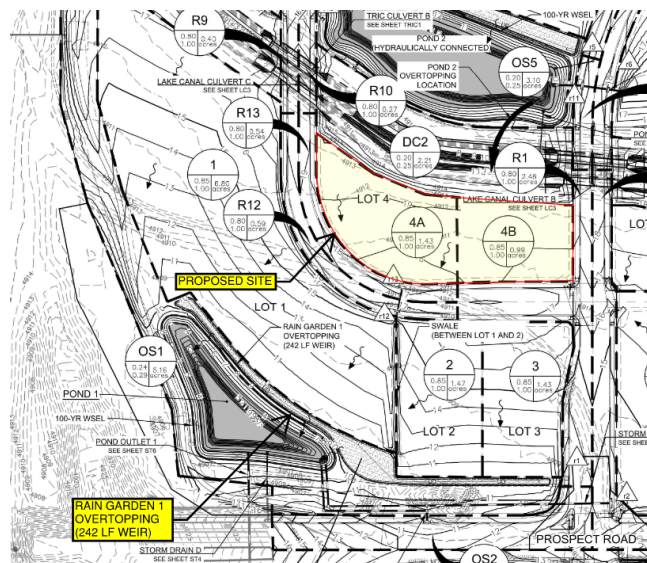


Figure 4: Drainage Plan

Natural Habitat Buffer Zone

As with the stormwater system, the Natural Habitat Buffer Zone was established under the infrastructure plan. The limits of this feature is depicted within the proposal's plan set (Figure 5: Landscape Plan – Natural Habitat Buffer Zone Limits) and although the project site itself is not encumbered by the NHBZ, the proposed landscaping design incorporates a transition to this area to minimize impacts, consistent with Section 5.6 of the Land Use Code. Environmental Planning staff has confirmed that the project will not allow any light trespass to enter the area and finds the project maintains compliance with relevant standards.

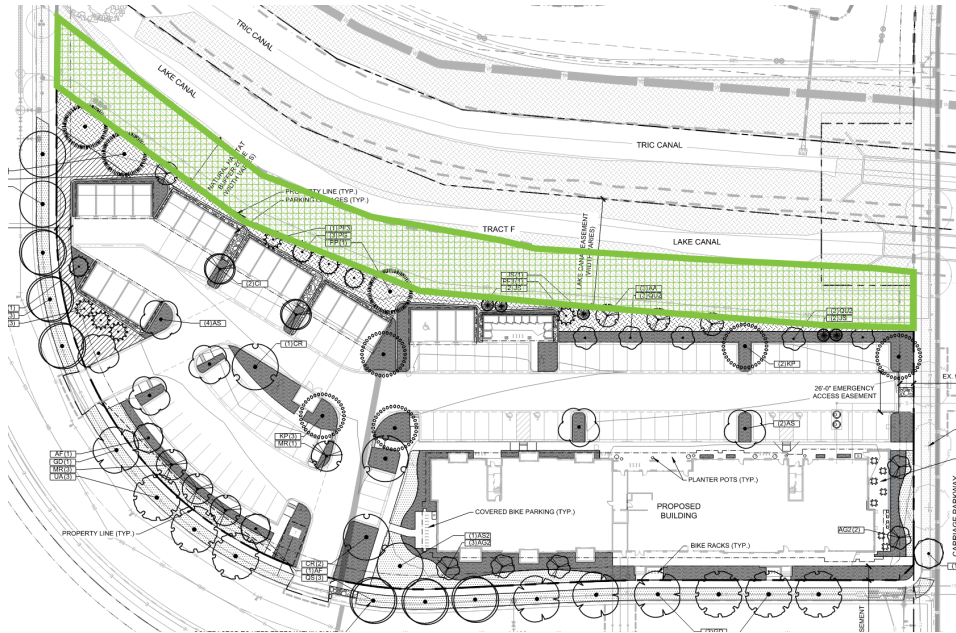


Figure 5: Landscape Plan – Natural Habitat Buffer Zone Limits

Parks & Trails

The project does not propose new parks or trails but connects to existing pedestrian networks via sidewalks along Prospect Ridge Drive and Carriage Parkway. The sidewalks along the site will provide a direct connection to the planned city-owned park at the northeast edge of the planning area as well as to the internal private trail running parallel to the north side of the Timnath Reservoir Inlet Canal. The private trail will extend east to Timnath Middle-High School and west to the city-owned regional trail system that extends north and south along the edge of Interstate 25 (Figure 6: ODP Trail and Park Locations).

Staff finds that meets all relevant standards and requirements of this topic area.

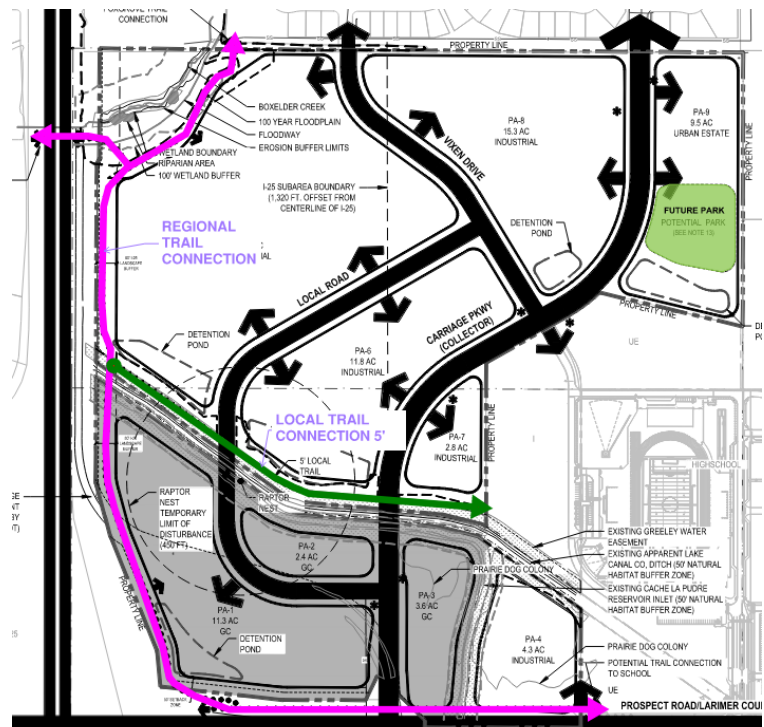


Figure 6: ODP Trail and Park Locations

B. Neighborhood Design and Building Placement

Applicable LUC Standards and Related Plans and Policy

- 2.6.4 – I-25 Development Standards
- 3.1.1 – Mixed-Use Building Type
- 5.9.1 – Building Placement & Site Design
- 5.15.1 – Building & Project Compatibility
- 5.15.2 – Mixed-Use, Institutional and Commercial Buildings

Staff Analysis & Findings

The project is located within the I-25 Activity Center, an overlay district envisioned to provide a compact mix of urban uses, including employment, multifamily residential, commercial, and retail. The design standards outlined in the Land Use Code are divided into several components, including landscaping and screening, block patterns and building orientation, and design controls to enhance the quality and appearance of multifamily residential, mixed-use, commercial, and industrial developments.

Neighborhood design was carefully considered as part of the Rudolph Farm Infrastructure Project. One of the primary design considerations was to embody the spirit of the Activity Center's block standards, which envision compact, walkable blocks, while acknowledging the site's significant constraints, including proximity to I-25, grade changes created by the Prospect Rd overpass, limited access from Prospect Road, Natural Habitat Buffer Zones, required landscape buffers, and large utility corridors, including the Greeley water line and two ditches. Overall, the Infrastructure Plan provides the necessary framework of streets within the Activity Center for future infill, with this project being the second to be considered within the planning area south of the utility corridor.

The project responds to its site context by orienting the building toward the intersection of Carriage Parkway and Prospect Ridge Drive. The building is set back 20 to 30 feet (Figure 7: Limits of Outdoor Patio Space) from the street sidewalk, exceeding the standard 15-foot build-to line. This approach is permissible, provided the project includes elements such as outdoor patio space and enhanced street-level activation. On the eastern edge, the patio features an overhanging canopy and a low architectural wall designed to provide outdoor seating for future retail tenants. Along the internal private street, an elevated 8-foot-wide walkway will include outdoor benches and seating. These spaces will adjoin the building's façade, which features a storefront window system, authentic high-quality materials, and overhanging canopies, combining to promote pedestrian friendliness as envisioned.

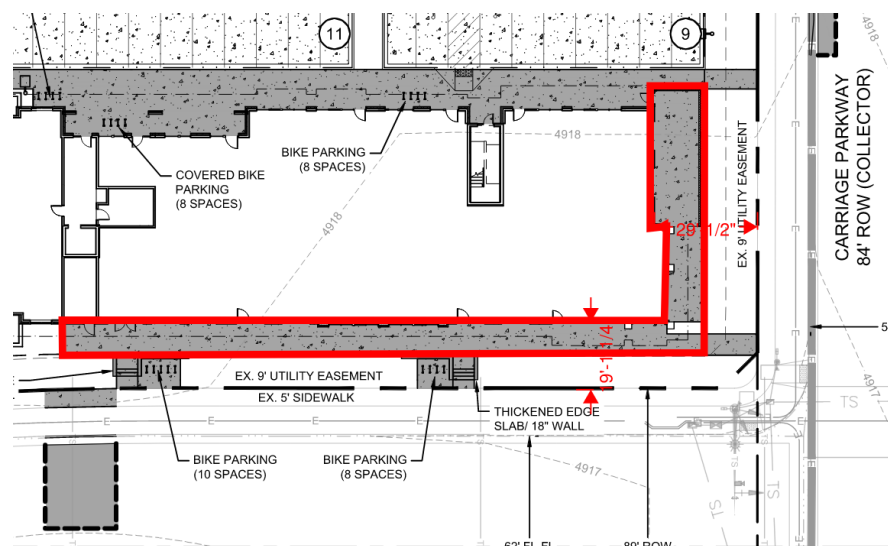


Figure 7: Limits of Outdoor Patio Space

Staff finds that the project meets all applicable neighborhood design and building placement standards.

C. Access, Parking, and Circulation

Applicable LUC Standards and Related Plans and Policy

5.9.1 – Access, Circulation, and Parking

[Larimer County Urban Area Street Standards](#)

[Fort Collins Pedestrian Plan \(2011\)](#)

[Active Modes Plan](#)

Staff Analysis and Findings

Access, parking, and circulation comply with Article 5 (Division 5.9, Section 5.9.1) and related standards. Direct vehicular access is provided via two two-way drive aisles from Prospect Ridge Drive, with internal circulation flowing through the parking lot to minimize conflicts. An emergency access easement, capable of supporting 80,000 pounds, connects to Carriage Parkway in accordance with Poudre Fire Authority standards, and fire lane markings will be approved prior to occupancy. Pedestrian access is facilitated by multiple walkway connections (Figure 9: Walkway Connections) from Prospect Ridge Drive and Carriage Parkway, routing users to building frontages and parking stalls.

The project includes two vehicular drives connecting to Prospect Ridge Road and seven walkway connections integrating with the adjacent street sidewalk system. These surrounding street sidewalks enhance both local and regional connectivity to key destinations, including Timnath Middle/High School, the regional trail, and nearby neighborhoods such as Fox Grove and Clydesdale Estates.

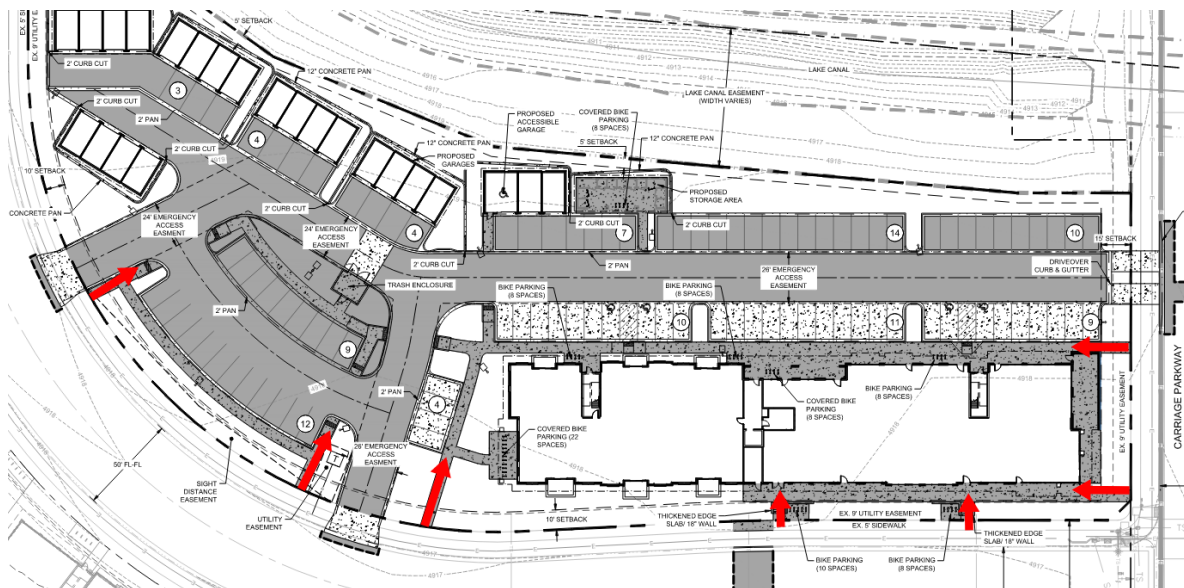


Figure 8: Walkway Connections

The project provides 120 vehicle parking spaces (97 surface and 23 garage, including 5 accessible), exceeding requirements under Colorado House Bill 24-1304, which imposes no minimum for multi-unit residential developments; retail parking meets the 2 spaces per 1,000 square feet standard. Bicycle parking totals 103 spaces (42 fixed and 61 enclosed, including 23 in garages), satisfying requirements of 1 space per bedroom for residential (97 bedrooms) and 1 per 4,000 square feet for retail, with 60% enclosed for residential and 20% for retail. Large parking areas are segmented with landscaping to enhance aesthetics, shading, and safety, and drive aisles are designed to separate pedestrians from Staff has determined that the project complies with all relevant standards and requirements related to access, parking, vehicle, pedestrian, and bicycle infrastructure.

D. Landscaping

Applicable LUC Standards and Related Plans and Policy

5.10.1 – Landscaping and Tree Protection

Staff Analysis and Findings

The project offers a diverse planting scheme that includes 38 canopy trees (e.g., Rocky Mountain Glow Maple, Shumard Oak), 17 evergreen trees (e.g., Baker Blue Spruce, Ponderosa Pine), and 18 ornamental trees (e.g., Flame Amur Maple, Radiant Crabapple), achieving species diversity percentages from 4% to 8% per type. Shrub areas cover 12,662 square feet, with ground covers including 9,892 square feet of Tahoma 31 Bermudagrass sod and native seed mixes to support habitat continuity. Parking lot perimeter landscaping beds, once fully developed at Final Development Plan, are anticipated to screen at least 75% of the perimeter with grasses and shrubs. A primary consideration during the development plan review was enhancing screening and privacy between ground-floor residential units and Prospect Ridge Drive to better mitigate noise, headlight spill, and the high traffic volume anticipated from commercial users to the south.

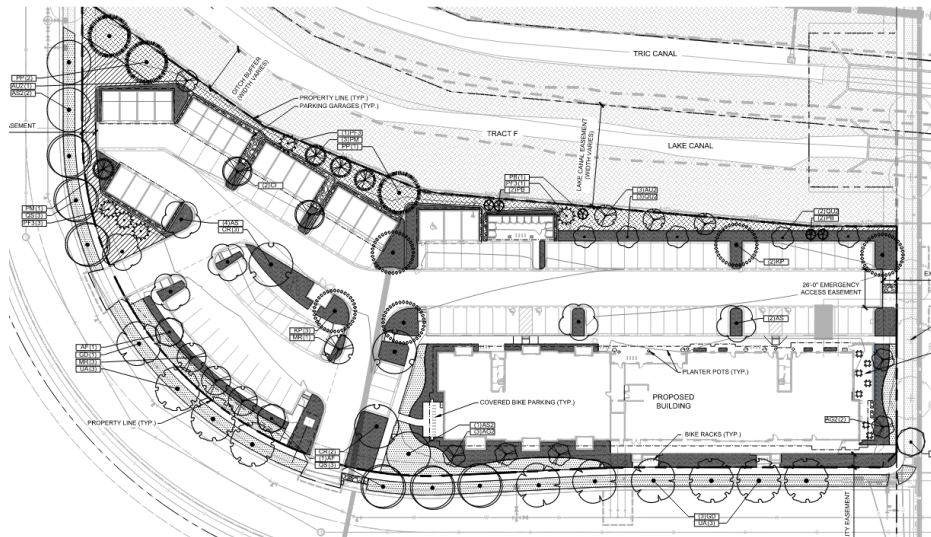


Figure 9: Landscape Plan

Staff finds that the project meets all relevant standards and requirements for landscaping.

E. Architectural Design

Applicable LUC Standards and Related Plans and Policy

2.6.4 – I-25 Development Standards

3.1.1 – Mixed-Use Building Type

5.15.1 – Building and Project Compatibility

5.15.2 – Mixed-use, Institutional and Commercial Buildings

Staff Analysis & Findings

As mentioned in the Key Considerations section of this report, the architectural review centered around material use, responding to the site's context, and breaking down the building into modules/simplifying the use of materials. Below is the progression of design provided by the applicant, with the pre-hearing update providing full compliance with all architectural standards.



Figure 10: Round 1 South Elevation



Figure 11: Round 2 South Elevation



Figure 12: Pre-Hearing Revision South Elevation

The project was evaluated under multiple, sometimes overlapping, design standards within the I-25 Activity Center Overlay, building compatibility requirements, and mixed-use building type standards. These standards emphasize the following principles:

- Promoting an urban relationship between the building and street and providing visually appealing ground-floor design that is human scaled, provides transparency in commercial settings, and utilizes high quality materials.
- Creating a design that contributes to the community's high quality image and uniqueness using authentic materials (brick, buff sandstone) and earth-toned colors.
- Provide a design that contributes to a cohesive place within the zone district and area that the project is located.

A large portion of the ground floor designated for future commercial users and aligns with the stated vision and height allowances for this area of the community. The building's design breaks its mass into distinct commercial and residential sections through residential fenestration patterns, storefront glazing, and the use of stone and brick at the base, meeting the massing and durability requirements of the I-25 Corridor Activity Center. The commercial ground floor incorporates features such as overhanging brackets, large storefront windows, and a sloped roof over an outdoor patio, creating an active and welcoming street-level design.

The building responds to its corner location at Carriage Parkway and Prospect Ridge by a deliberate projection of the building's corner mass. The architectural element is designed to create a focal point drawing attention to the intersection through the use of an elevated and distinct roofline with exposed beams, distinct window groupings, and use of a high quality authentic material that extend to the top of the third floor.

Beyond the building's corner focal point, the design breaks down the wall plane through the use of material changes, insets, projections, and meaningful changes to the roofline. Colors reflect the surrounding residential area, and influence of the buff sandstone used as a standard for the City of Fort Collins interchange style. Partially inset balconies, which provide screen panels for privacy, further reducing the building's bulk, enhancing its fit with the neighborhood and maintaining a human-scale appearance.



Figure 13: View of South and East Facing Elevation



Figure 14: North and East Facing Elevation

The project proposes six provides a 3-space and 4-space variation. The massing is broken down through changes in materials, colors, and rooflines as necessary.

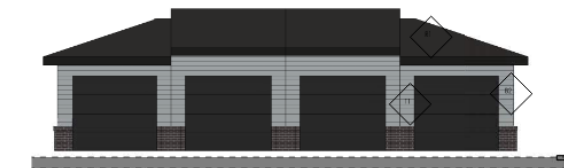
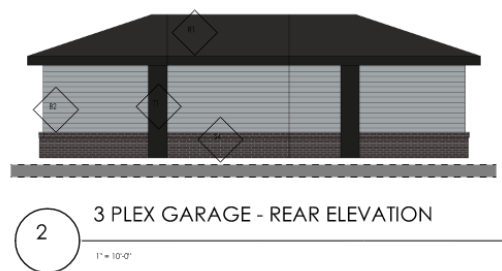
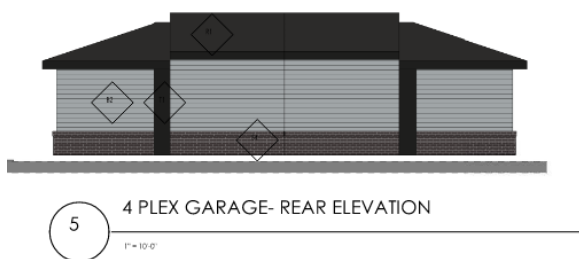


Figure 16: 4-Space Garage

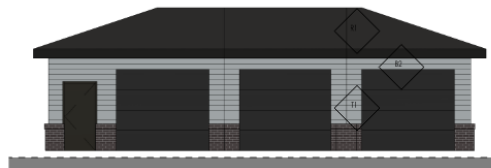


Figure 15: 3-Space Garage

Staff finds that the development meets and exceeds all architectural design standards.

F. Lighting

Applicable LUC Standards and Related Plans and Policy

[Lighting Context Area Map](#)

5.12.1 – Exterior Site Lighting

Staff Analysis and Findings

The site is located within the LC2 Lighting Context Area where lighting levels are moderate and intended for locations that are mixed-use and include dense residential and commercial users, such as the General Commercial zone district.

The project utilizes three styles of luminaire seen below which are concealed, fully shielded with sharp cut-off to minimize up-light, spill, glare, and diffusion.

The photometric plan demonstrates compliance with a maximum calculated light trespass of 0.3 foot-candles (meeting the 0.30 foot-candle limit) and 0 trespass adjoining the Natural Habitat Buffer Zone with a maximum CCT of 3000 K. Using the hardscape method for 37,531.43 square feet, the total base lumen allowance is 93,829 lumens, with proposed total lumens at 93,460. BUG ratings are within limits for most luminaire types. Fixtures use concealed, fully shielded sources with sharp cut-off to minimize glare and up-light, and no exposed neon or illuminated striping is proposed. Mounting heights (10-20 feet) and placements ensure illumination is directed downward, protecting the ecosystem and reducing night glow.

Overall, the layout supports safety and visibility along pathways and building entrances, consistent with LC2 goals. It's anticipated that I-25 lighting will have some level of impact on the ambient lighting of the area that will likely further increase nighttime visibility.



Figure 17: Proposed Lighting Fixtures

Staff finds that the lighting plan demonstrates necessary compliance with all relevant sections of the Lighting Code.

G. Trash Enclosure

Applicable LUC Standards and Related Plans and Policy

Design Considerations for Trash and Recycling Systems

5.11.1 - Trash and Recycling Enclosures

Staff Analysis and Findings

The project proposes trash enclosure positioned within the main parking field of the site. The enclosure is anticipated to provide service for both the residential and commercial components of the development, both of which can access the enclosure through an internal walkway system. The enclosure provides necessary person door, gates, and concrete service pad. The proposed structure is clad with lap siding that is compatible with the primary materials of the development.

During final plan review, the applicant will be required to provide further details to ensure durability of the structure such as metal curbing to stop dumpster from hitting the structure's interior walls and reinforced corners/bollards to ensure long-term durability of the structure when rolling bins in and out of the structure.

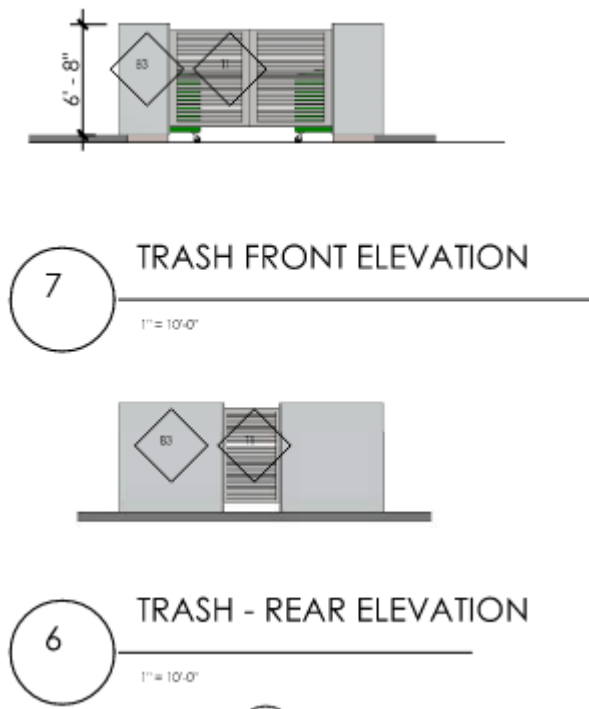


Figure 18: Elevation View of Trash Enclosure

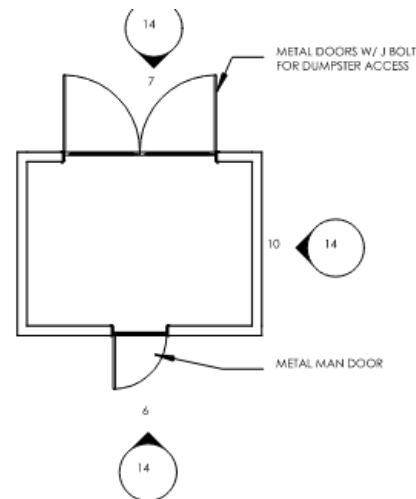


Figure 19: Trash Enclosure Site Plan View

Staff finds that the enclosure meets all necessary requirements of the Land Use Code and associated Design Considerations.

9. REVIEW PROCESS & PROCEDURES

This Project Development Plan (PDP) requires infrastructure engineering at least a 50% completion level – sometimes more due to a site’s complexity. Elements such as street layout, neighborhood design, , building location, architectural design and materials, parking, and vehicle/pedestrian circulation must be fully developed, while elements like trash enclosures, shrub and grass species selection, irrigation, and seed mixes should be developed to demonstrate general compliance with applicable standards. Project Development Plan approval does not confer project vesting and is valid for a period of three years. To fully vest a project, the applicant/developer ius required to submit a Final Development Plan which complies with all technical design requirements of the City and any applicable external agencies before the recording with Larimer County, thus establishing the vested right to develop the property as proposed.

The project was initially submitted and reviewed during a pre-application meeting on October 4, 2023. Following a significant pause, the formal submission occurred on July 9, 2025. The project then underwent two rounds of iterative staff review before being scheduled for consideration by the Administrative Hearing Officer. The City provides a public hearing notice, mailed to all properties within 1,000 feet of the project’s boundary.

The Administrative Hearing Officer holds decision-making authority to approve, conditionally approve, or deny the Overall Development Plan (ODP) amendment and the development plan. A decision must be rendered within 10 days of the hearing and is appealable within a 14-day period following the decision, with the City Council serving as the appellate body.

10. FINDINGS

In evaluating the Prospect Ridge Mixed-Use – Lot 4 Project Development Plan, the staff makes the following findings of fact:

1. The Project Development Plan satisfies and aligns with the purpose of the Land Use Code, as stated in Sections 1.2.2(A) through (N). Specifically, the project complies with Section 1.2.2(A), as it is consistent with the City Plan and its adopted elements.
2. The Project Development Plan demonstrates compliance with the applicable standards of Article 2 – Zone Districts, as detailed in this staff report and Exhibit A and evidenced by the submittal materials attached to this report.
3. The Project Development Plan demonstrates compliance with the applicable standards of Article 3 – Building Types, as detailed in this staff report and Exhibit A and evidenced by the submittal materials attached to this report.
4. The Project Development Plan demonstrates compliance with the applicable standards of Article 4 – Use Standards, as detailed in this staff report and Exhibit A and evidenced by the submittal materials attached to this report.
5. The Project Development Plan demonstrates compliance with the applicable standards of Article 5 – General Development and Site Design, as detailed in this staff report and Exhibit A and evidenced by the submittal materials attached to this report.
6. The Project Development Plan demonstrates compliance with the applicable standards of Article 7 – Rules of Measurement and Definitions, as detailed in this staff report and Exhibit A and evidenced by the submittal materials attached to this report.

11. RECOMMENDATIONS

Staff recommends approval of the Prospect Ridge Mixed-Use – Lot 4 Project Development Plan application, PDP250003.

12. EXHIBITS

Exhibit A	Staff Compliance Findings of Applicable Codes , Summarizes compliance with Land Use Code, linked to Comprehensive Plan & Policy Alignment and Findings sections.
Exhibit B	Project Narrative , Applicant's description of project purpose and design, referenced in Project Description and Review Process sections.
Exhibit C	Site Plan , Layout of proposed development, referenced in Proposed Development – Building Placement section.
Exhibit D	Landscape Plan , Landscaping and tree protection details, referenced in Proposed Development – Landscaping section.
Exhibit E	Architecture , Architectural drawings and materials, referenced in Proposed Development – Historic Preservation and Architectural Design section.
Exhibit F	Overall Development Plan – Master Plan depicting parameters for development including conceptual roadway alignment, natural features, drainageways, land use, and zoning.
N/A	Public Comment , Neighborhood Meeting Summary and any other compiled public feedback through phone, email, or in-person communication.

13. LINKED EXHIBITS

The following materials are referenced in the report or are exhibits of supporting documentation and available via the provided links:

Exhibit G	Utility Plan , Infrastructure plans, referenced in Proposed Development – Natural Areas section.
Exhibit H	Lighting Plan
Exhibit I	Drainage Report
Exhibit J	Traffic Study
Exhibit K	Fire Truck Turning Movement
Exhibit L	Rudolph Farms Metropolitan District NOS 1-6 (Resolution 2018-028)

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Exhibit A – Applicable Standards

1. ARTICLE 2 – ZONE DISTRICTS

A. Division 2.3 Commercial

Section 2.3.4 – General Commercial District (CG)

The General Commercial District is intended to be a setting for development, redevelopment and infill of a wide range of community and regional retail uses, offices and personal and business services. Secondly, it can accommodate a wide range of other uses including creative forms of housing.

While some General Commercial District areas may continue to meet the need for auto-related and other auto-oriented uses, it is the City's intent that the General Commercial District emphasize safe and convenient personal mobility in many forms, with planning and design that accommodates pedestrians.

Staff Finding	Minimum Standard	Summary of Standard
Complies	Building Types	The following building types are permitted in the CG District: • Mixed-Use, Apartment, Row House and Duplex. • ADU only with an existing Detached House. • Detached Accessory Structure. • See Division 3.1 for more details. • All nonresidential buildings permitted under this Section, including industrial buildings, shall meet the standards for mixed-use and commercial buildings contained in Section 5.15.2 of this Code.
Complies	Building Height	4-Stories Maximum
Complies	Prospect Road Streetscape Program	Prospect Road Streetscape Program
Complies	Outdoor Spaces	Pedestrian-oriented outdoor spaces shall be placed next to activity areas that generate the users (such as street corners, shops, stores, offices, day care and dwellings). Because liveliness created by the presence of people is the main key to the attractiveness of such spaces, to the maximum extent feasible, the development shall link outdoor spaces to and make them visible from streets and sidewalks. Sculpture, kiosks or shelters are encouraged to be prominently placed in outdoor spaces.

Section 2.6.4 – I-25 Development Standards

The purpose of this Section is to provide standards to implement the model standards outlined in the "Development Standards for the I-25 Corridor" and the "Fort Collins I-25 Corridor Subarea Plan," in addition to the standards contained elsewhere in this Land Use Code.

Staff Finding	Minimum Standard	Summary of Standard
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Complies	2.6.4(D)(1) - Building Placement	Minimum setback of any building on a lot, tract or parcel of land adjoining the I-25 right-of-way shall be two hundred five (205) feet from the centerline of I-25
Complies	2.6.4(E) - Landscaping Standards	1. Parking Lot Perimeter Landscaping. At least seventy-five (75) percent of the perimeter of all parking areas shall be screened from nearby streets, public rights-of-way, public open space and nearby uses by at least one (1) of the following methods: a. A berm at least three (3) feet high with a maximum slope of 3:1 in combination with evergreen and deciduous trees and shrubs; b. A hedge at least three (3) feet high, consisting of a double row of shrubs readily capable of growing to form a hedge, planted three (3) feet on center in a triangular pattern; c. A decorative fence or wall between three (3) and four (4) feet in height in combination with landscaping including, without limitation, evergreen and deciduous trees and shrubs.
Complies	2.6.4(F) – Commercial Building Design Standards	The principles of the commercial design standards contained within this section is to ensure that buildings provide aesthetic and functional design for roof forms, building facades, and material selections to ensure consistency within the corridor. For structures with a footprint under 10,000 square feet, roofs must have a minimum pitch of 5:12, incorporate a modified Mansard roof to appear as though it covers the entire structure, and include at least one design element—such as projecting gables or dormers, hips, horizontal or vertical breaks, or three or more roof planes—for every 50 lineal feet of roof. For structures exceeding 10,000 square feet, roofs must feature at least two of the following: parapet walls with three-dimensional cornice treatment not exceeding one-third of the supporting wall's height, overhanging eaves extending at least three feet beyond supporting walls, sloping roofs not exceeding the average height of supporting walls with a minimum slope of one foot vertical rise per one foot horizontal run, or three or more roof slope planes. Building facades facing public streets, adjoining developments, or pedestrian frontage must be articulated, fenestrated, and proportioned to human scale along at least 60% of the facade, using features like windows, entrances, arcades, arbors, or awnings. Facades facing primary access streets require clearly defined, highly visible customer entrances with at least two elements, such as canopies, porticos, overhangs, recesses or projections of at least 3% of wall length, arcades, distinctive roof forms, arches, outdoor patios, display windows, or planters with landscaped areas or seating. Material selections must include stucco, brick, stone, or tinted, textured masonry block to provide visual interest at the pedestrian level, while smooth-faced gray concrete block, tilt-up concrete panels, and metal as a primary exterior surface (exceeding 10% of the facade) are prohibited, though metal may be used as trim or for roofing. Facade colors must be low-reflectance earth tones, and high-intensity primary colors are prohibited on roof areas visible from public or

		private rights-of-way or public open spaces. These standards ensure structures are visually appealing, pedestrian-friendly, and contextually appropriate.
Complies	2.6.4(H) – Service Areas, Outdoor Storage, and Mechanical Equipment	This regulation outlines requirements for the placement and screening of service areas and equipment in buildings, designed to ensure visual order and compatibility with the surrounding environment. Below is a concise summary, structured for clarity and accessibility: Location of Service Areas: Loading docks, outdoor storage yards, and service areas must be positioned at the sides or rear of a building. If the site borders Interstate 25 (I-25), these areas must not face I-25 and should be placed on other sides of the building. This ensures service areas are less visible from prominent viewpoints, maintaining aesthetic appeal. Screening Requirements: All outdoor storage yards, loading docks, service areas, and mechanical equipment or vents larger than 8 inches in diameter must be hidden by screens. Screens must be at least as tall as the equipment they conceal and match or complement the primary building's colors and materials. Chain-link fences, with or without slats, are not permitted for screening. For equipment still visible due to lower site elevation (e.g., compared to nearby roads), full enclosures are required. These enclosures may include vents for airflow, but vents must not exceed 25% of the enclosure's facade.
Complies	2.6.4(L) – Minimum Residential Density in Activity Centers	Minimum residential density in activity centers shall be twelve (12) dwelling units per gross acre.

2. ARTICLE 3 – BUILDING TYPES

A. Division 3.1 – Residential Building Types

Section 3.1.1 – Mixed Use

In “mixed-use” buildings, there shall be a combination of retail, office, and/or residential spaces within one or several buildings. Mixed-use buildings are usually in more urban areas and can vary in their size and number of stories. A mixed-use building type can be identified by its approachable and pedestrian friendly look. The buildings may be farther away from the street with wider sidewalk areas, street plantings, or outdoor seating.

Staff Finding	Minimum Standard	Summary of Standard
Complies	Zone Districts	Mixed-Use building types are allowed in the General Commercial zone district and I-25 Overlay district.
Complies	Building Height	See zone district.
Complies	Build-to Lines	0-15 feet maximum.
Complies	Build-to Line Exceptions	Plaza, courtyard, Landscaping, low walls, fencing patio or garden or railings, a tree canopy and/ or other similar site improvements are required.
Complies	Façade Base	All building facades must feature a distinct "base" to provide a visually recognizable foundation. This base may include, but is not limited to: Thicker walls, ledges, or sills. Textured materials, such as stone or masonry. Integrally colored and patterned materials, like smooth-finished stone or tile. Lighter or darker colored materials. Mullions or panels. Planters. These elements ensure the base is distinguishable and enhances the building's aesthetic foundation.
Complies	Façade Top	All facades must have a recognizable "top" to provide a clear visual cap to the structure. Acceptable features include, but are not limited to: Cornice treatments using integrally textured materials (e.g., stone or masonry) or differently colored materials, excluding simple colored stripes or bands. Sloping roofs with overhangs and brackets. Stepped parapets. These features create a defined and aesthetically pleasing top to the building.
Complies	Massing	For buildings with footprints exceeding 10,000 square feet, facades must incorporate recesses or projections to break up the massing. Building bays must: Be no wider than 30 feet.

		Include at least two of the following to define a bay: • A change in plane. • A change in height. • A change in texture or masonry pattern, windows, or treillage with vines. • An equivalent element that subdivides the wall into human-scale proportions. This requirement ensures large buildings appear less monolithic and more visually proportionate.
Complies	Four Sided Design	Building materials used on all sides of the structure must be consistent with those used on the front facade. This ensures a cohesive appearance across all elevations, maintaining aesthetic quality and uniformity regardless of the viewing angle.
Complies	Entryways	All primary entrances must include a clearly defined and recessed or framed element, such as an awning, arcade, or portico, to provide shelter for pedestrians. Primary entrances must open to a connecting walkway with pedestrian frontage, ensuring accessible and safe entry. Awnings must be no shorter than the width of a single storefront, providing adequate coverage. Buildings with vehicle bays or service doors for intermittent or nonpublic access (e.g., self-serve car washes or mini-storage warehouses) are exempt from the pedestrian frontage requirement.

3. ARTICLE 4 – USE STANDARDS

A. Division 4.2 Table of Primary Uses

Staff Finding	Minimum Standard	Summary of Standard
Complies	Mixed-Use Dwelling Units	Permitted subject to Type 1 (Administrative Review).

B. Division 4.3 – Additional Use Standards

Section 4.3.1 – Residential Uses

Staff Finding	Minimum Standard	Summary of Standard
4.3.1(I)	Mixed-Use Dwelling Unit	Facility Amenities limited to the use of residents in the building such as but not limited to a leasing office gym, and pool are not considered separate use.

4. ARTICLE 5 – GENERAL DEVELOPMENT AND SITE DESIGN

A. Division 5.3 – Residential Development

Section 5.3.1 – Residential Developments

Staff Finding	Minimum Standard	Summary of Standard
Complies	5.3.5 – Garage Design	The standards for rear walls of multi-unit garages aim to enhance visual appeal and ensure human-scale design, accessibility, and alignment with internal building divisions. For perimeter garages located within 65 feet of a public right-of-way or property line, the rear wall length must not exceed 60 feet, with a minimum of 7 feet of landscaping required between adjacent garages. Additionally, no rear wall facing a street or neighboring development may exceed 30 feet without incorporating at least two articulation features, such as a change in wall plane by at least 6 inches, material or masonry pattern variation, roof plane change, windows, doorways, false door/window openings with frames, sills, and lintels, or equivalent vertical elements that divide the wall into human-scaled proportions or reflect internal divisions. For all garages, rear doorways must be provided as deemed necessary by the decision maker to allow direct access to living units without requiring residents to walk around the garage. Furthermore, a vertical trim detail subdividing the siding pattern is required at intervals not exceeding two internal parking stalls (approximately 20 to 24 feet), and while the articulation features for perimeter garages are encouraged, they are not mandatory for all garages.

B. Division 5.9 – Building Placement & Site Design

Section 5.9.1 – Access, Circulation and Parking

Staff Finding	Minimum Standard	Summary of Standard
Complies	General Standard	The standards outlined in this section aim to create safe, efficient, and attractive parking and circulation systems within developments, prioritizing accessibility and connectivity for all users, including pedestrians, cyclists, individuals using mobility assistance devices, and vehicles. The primary principles include ensuring safe separation of pedestrians and cyclists from vehicles through design elements like special paving, raised surfaces, and clear signage; providing accessible infrastructure such as curbcuts and ramps that comply with the Americans with Disabilities Act; and integrating site amenities like bike racks and benches to enhance convenience and promote walking and cycling. Developments must connect seamlessly to the city's bicycle and pedestrian networks, ensuring direct and continuous pathways to key destinations like commercial districts and transit stops. Parking requirements are tailored to specific land uses, with minimum and maximum spaces defined to balance demand and prevent over-parking, while shared parking is encouraged for mixed-use developments to optimize space. Drive-in facilities and loading zones are designed to

		minimize conflicts with pedestrian access, and large parking lots must be segmented with landscaping to improve aesthetics and functionality. Overall, these standards seek to foster a safe, accessible, and visually appealing environment that supports diverse transportation modes and enhances community connectivity.
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C. Division 5.10 – Landscaping and Tree Protection

Section 5.10.1 – Landscaping and Tree Protection

Staff Finding	Minimum Standard	Summary of Standard
Complies	General Standard	The standards outlined in this section aim to promote a robust urban forest and enhance environmental and social benefits through comprehensive landscape and tree protection plans. The key principles include ensuring significant tree canopy cover to reduce erosion, manage stormwater runoff, conserve water, mitigate air pollution, reduce glare and heat, and enhance aesthetic appeal. Project plans should integrate with existing vegetation patterns, support functional goals such as defining spaces, providing privacy, and managing microclimates, while protecting significant trees and natural habitats. Additionally, the standards seek to improve the pedestrian environment, enhance neighborhood appearance, and ensure continuity between developments.

D. Division 5.11 – Trash & Recycling Enclosure

Section 5.11.1 – Trash & Recycling Enclosures

Staff Finding	Minimum Standard	Summary of Standard
Complies	General Standard	The purpose of this standard is to ensure the provision of areas, compatible with surrounding land uses, for the collection, separation, storage, loading and pickup of trash, waste cooking oil, compostable and recyclable materials. This standard is supplemented by the Enclosure Design Considerations and Guidance Document issued by the Director and available linked in this report.

E. Division 5.12 – Exterior Site Lighting

Staff Finding	Minimum Standard	Summary of Standard
Complies	General Standard	The purpose of this Section is to ensure adequate exterior lighting for the safety, security, enjoyment and function of the proposed land use; conserve energy and resources; reduce light trespass, glare, artificial night glow, and obtrusive light; protect the local natural ecosystem from damaging effects of artificial lighting; and encourage quality lighting design and fixtures

F. Division 5.15 – Building Standards

Section 5.15.1 – Building and Project Compatibility

Staff Finding	Minimum Standard	Summary of Standard
Complies	General Standard	The standards in this section aim to ensure that new buildings and their uses blend well with the surrounding area in terms of physical design and operational impact. The primary principles focus on achieving compatibility by aligning proposed developments with the existing context of the neighborhood, ensuring that buildings and activities complement the area's character and function. These standards work in tandem with more specific building and zoning regulations outlined elsewhere in the code, providing a cohesive framework for development. They apply to all aspects of development plans, including buildings and additional uses, and guide decision-makers in evaluating whether proposed uses are appropriate for the area, promoting harmony and consistency in the community's built environment.

Section 5.15.2 Mixed-Use, Institutional, and Commercial Buildings

Staff Finding	Minimum Standard	Summary of Standard
Complies	General Standard	The standards in this section aim to create an urban environment designed to a human scale, ensuring that mixed-use and nonresidential buildings are visually appealing, pedestrian-friendly, and contextually appropriate. The key principles include designing buildings with significant architectural interest to avoid large, imposing structures, prioritizing pedestrian-oriented street-level design with attractive walkways, and ensuring buildings complement their surroundings through tailored materials, features, and activity areas. Buildings must orient main entrances toward connecting walkways, incorporate varied massing to break up large forms, and avoid false or superficial design elements. Facades facing streets or walkways must be articulated with features like windows, entrances, or awnings across at least 50% of their surface, with no blank wall exceeding 30 feet. All sides of a building must use consistent, high-quality materials, and entrances should include sheltering elements like awnings or arcades. Buildings require distinct base and top treatments, such as textured materials or cornices, to enhance visual structure. Special architectural features may encroach slightly into public rights-of-way, but drive-through lanes are limited to 10

		feet in width, and certain types of exterior lighting, like exposed neon or illuminated striping, are prohibited to maintain aesthetic coherence and pedestrian focus. These standards promote cohesive, human-scaled, and context-sensitive development that enhances the urban environment.
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5. ARTICLE 6 – ADMINISTRATION

A. Division 6.3 Common Development Review Procedures

Staff Finding	Minimum Standard	Summary of Standard
Complies	6.3.6(A) – Mailed Notice	Mailed Notice. The Director shall mail written notice to the owners of record of all real property within 1,000 feet (of the property lines of the parcel of land for which the development is planned
Complies	6.3.6(B) – Posted Notice	The real property proposed to be developed shall also be posted with a sign, giving notice to the general public of the proposed development. For parcels of land exceeding ten (10) acres in size, two (2) signs shall be posted.
Complies	6.3.6(C) – Published Notice	Notice of the time, date and place of the public hearing/ meeting on the development application and the subject matter of the hearing shall be published in a newspaper of general circulation within the City at least seven (7) days prior to such hearing/meeting

6. ARTICLE 7 – DEFINITIONS

A. Division 7.2 Definitions

Relevant Definitions

Bicycle parking, enclosed shall mean bicycle storage in lockers, a garage, a room or other space within a parking structure or other building, including a shed or carport. All types of enclosed bicycle storage must be easily accessible to entrances and walkways, secure, lighted and protected from the weather. Each storage space shall provide a minimum of six (6) square feet in area. The storage space shall not impede fire exits or be located so that parked bicycles interfere with public access.

Bicycle parking, fixed shall mean bicycle parking that allows the bicycle frame and both wheels to be securely locked to the parking structure. The structure shall be of permanent construction such as heavy gauge tubular steel with angle bars permanently attached to the pavement foundation. Fixed bicycle parking facilities shall be at least two (2) feet in width and five and one-half (5½) feet in length, with additional back-out or maneuvering space of at least five (5) feet.

Building mass shall mean the three-dimensional bulk of a building: height, width and depth

Compatibility shall mean the characteristics of different uses or activities or design that allow them to be located near or adjacent to each other in harmony. Some elements affecting compatibility include height, scale, mass and bulk of structures. Other characteristics include pedestrian or vehicular circulation, access and parking design. Other important characteristics that affect compatibility are landscaping, lighting, noise, odor and architecture. Compatibility does not mean "the same as." Rather, compatibility refers to the sensitivity of development proposals in respecting the character of existing development.

Connecting walkway shall mean (1) any street sidewalk, or (2) any walkway that directly connects a main entrance of a building to the street sidewalk without requiring pedestrians to walk across parking lots or driveways, around buildings or around parking lot outlines that are not aligned to a logical route

I-25 activity center (located as described in the I-25 Subarea Plan) shall mean an area of concentrated development containing more than one (1) principal land use type and generally served by high frequency transit. Such land uses may include office, retail, residential or service uses such as hotels, motels and personal and business services. In an I-25 activity center, the different types of land uses are in close proximity, planned as a unified complementary whole, and functionally integrated to the use of vehicular and pedestrian access and parking areas

Local street shall mean a street that is anticipated to carry under two thousand five hundred (2,500) vehicle trips per day in traffic volume at desirable speeds of up to twenty-five (25) miles per hour, and that provides access to abutting property and primarily serves local traffic

Massing shall refer to the perception of the overall shape, form, and size of a building.

Mixed use shall mean the development of a lot, tract or parcel of land, building or structure with two (2) or more different uses, including, but not limited to, residential, office, retail, public uses, personal service or entertainment uses (but not including accessory uses), designed, planned and constructed as a unit.

Pedestrian-oriented development shall mean development that is designed with a primary emphasis on the street sidewalk and/or connecting walkway access to the site and building, rather than on auto access and parking lots. In pedestrian-oriented developments, buildings are typically placed relatively close to the street and the main entrance is oriented to the street sidewalk or a walkway. Although parking areas and garages may be provided, they are not given primary emphasis in the design of the site.

Pedestrian scale (human scale) shall mean the proportional relationship between the dimensions of a building or building element, street, outdoor space or streetscape element and the average dimensions of the human body, taking into account the perceptions and walking speed of a typical pedestrian.

Private street shall mean a parcel of land not dedicated as a public street, over which a public access easement for street purposes has been granted to the City, and where the instrument creating such easement has been recorded or filed in the Office of the Clerk and Recorder of Larimer County. The public access easement shall allow for access by police, emergency vehicles, trash collection and other service vehicles, utility owners and the public in general.

Vested property right shall mean the right to undertake and complete the development and use of property under the terms and conditions of a site specific development plan.

Walkway shall mean an off-street pedestrian path.