

**CITY OF FORT COLLINS
TYPE 1 ADMINISTRATIVE HEARING
FINDINGS AND DECISION**

HEARING DATE: March 15, 2021

RE-OPENED HEARING DATE: April 5, 2021

PROJECT NAME: Alpine Bank (1608, 1610, 1618 S. College)

CASE NUMBER: PDP #200020

APPLICANT: Zell Cantrell
Galloway & Company
6162 S. Willow Drive #320
Greenwood Village, CO 80111

OWNER/LESSOR: Remington North, LLC
1400 S. Colorado Blvd. Suite 410
Denver, CO 80222

HEARING OFFICER: Lori B. Strand

PROJECT DESCRIPTION:

The Alpine Bank Project Development Plan (the “PDP” or “Application”) proposes to construct an approximately 8,242-square foot two-story bank and office building with two (2) drive-through teller lanes and one (1) drive-through ATM lane on an approximately 0.9-acre parcel of land located at 1608, 1610, and 1618 S. College (Parcel Nos. 9724216001, 9724216003, 9724216004, 9724216005, and 9724216006), at the southeast corner of the W. Prospect Road and S. College Avenue intersection (the “Property”).

Currently located on the Property are three vacant buildings which front S. College Avenue. The northernmost building (1608 S. College) and southernmost building (1618 S. College) were constructed in the 1960s and have been used for various retail and commercial uses. The building located in the middle of the Property (1610 S. College) is a Craftsman bungalow, constructed in 1928, which was used until the late 1970s as a residence (the “Architecturally Significant Building”); since then, it has been used for commercial purposes. Access to the existing buildings come from multiple curb-cuts on S. College Avenue, the public alley along the eastern edge of the Property, and a curb-cut on W. Prospect Road.

The project proposes to relocate the Architecturally Significant Building to the southwestern portion of the Property and to demolish all other existing structures on the Property. The project includes: twenty-eight (28) on-site parking spaces, interior sidewalks, and landscape areas; a new 10-foot-wide sidewalk and 8-foot tree lawn along S. College Avenue; the construction of a northbound deceleration/right-hand turn lane within S. College Avenue; a reduction in the curb-cuts off S. College Avenue to one; and removal of the curb-cut off W. Prospect Road. In addition to the single curb-cut off S. College Avenue, the Property will continue to be accessed by the public alley at the eastern edge of the Property that runs north-south from W. Prospect Road and Parker Street.

The Property is zoned General Commercial (C-G) and falls within the Transit-Oriented Development (TOD) Overlay Zone.

The project includes a proposal to plat the Property consistent with the PDP.

The PDP is subject to an Administrative Hearing (Type 1) review.

The Applicant/Owner requests a Modification of Standard to Land Use Code (“LUC”) Section 3.2.2(J). Section 3.2.2(J) requires that any vehicular use area containing six (6) or more parking spaces or 1,800 or more square feet to: (i) be setback at least five (5) feet along a lot line and (ii) have a minimum average of entire landscaped setback area of five (5) feet. The City determined that this requirement applies along the eastern (alley-side) boundary of the Property. A related standard in LUC Section 3.2.1(E)(4) sets forth landscaping requirements for the required setback areas. The Applicant/Owner requests to modify the standard in Section 3.2.2(J) to allow 9 parking spaces to be directly accessed from the alley and to shift the landscaped area required by Section 3.2.1(E)(4) inward on the Property as shown on Sheets 2 and 6 of the Site Plans for Alpine Bank.

The Applicant/Owner also requests a Modification of Standard to LUC Section 3.4.7 to allow for the relocation of the Architecturally Significant Building.

Staff recommends approval of the PDP and the requested Modifications of Standards, subject to one recommended condition of approval.

BACKGROUND:

Applicable project background is detailed in the Development Review Staff Report prepared for this Application, a copy of which is attached to this decision as **ATTACHMENT A** and is incorporated herein by reference.

The surrounding zoning and land uses are set forth below:

	North	South	East	West
Zoning	Community Commercial (C-C)	General Commercial (C-G)	Low Density Mixed Use Neighborhood (L-M-N)	General Commercial (C-G)

Land Use	Offices, retail and restaurant uses; C.S.U. Medical Health Center	Offices, Service Shops	Residential	Shopping Center – Retail, Office, and Restaurant; future hotel
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SUMMARY OF DECISION: Approved with one condition.

ZONE DISTRICT: General Commercial and Transit Oriented Development Overlay Zone.

HEARING: The Hearing Officer opened a virtual public hearing at approximately 6:05 p.m. on Monday, March 15, 2021 and re-opened the virtual public hearing at approximately 5:45 p.m. on Monday, April 5, 2021. At the initial and re-opened public hearing, the Hearing Officer reviewed the Order of Proceedings and Rules of Conduct for Administrative Hearings with the Applicant and members of the public present.

EVIDENCE: The Hearing Officer accepted the following documents as part of the record of this proceeding:

1. Development Review Staff Report prepared for Alpine Bank PDP #200020. A copy of the Staff Report is attached to this decision as **ATTACHMENT A** and is incorporated herein by reference.
2. Zoning and vicinity map.
3. Picture of posted notice sign #570 with email indicating picture was taken on 12/7/2020.
4. Confirmation of order, dated 3/1/2021, evidencing proof of publication of Notice of Hearing in the *Fort Collins Coloradoan* on 3/8/2021.
5. Confirmation of order, dated 3/23/2021, evidencing proof of publication of Notice of Re-Opened Hearing in the *Fort Collins Coloradoan* on 3/24/2021.
6. Copy of written notice of virtual public hearing dated March 1, 2021.
7. Copy of written notice of re-opened virtual public hearing dated March 22, 2021.
8. Project Narrative by Galloway & Company (2 pages)
9. Applicant Request for Modification to LUC Section 3.2.2(J), regarding vehicular use area setback requirements, dated 1/20/2021, from Galloway & Company (4 pages).
10. Applicant Request for Modification to LUC Section 3.4.7, regarding relocation of the Architecturally Significant Building, dated 12/2/2020 from Galloway & Company (6 pages).
11. PDP site plan set (11 sheets).

12. Material Board for Alpine Bank, Galloway & Company (1 page).
13. Alpine Bank Subdivision plat (1 sheet).
14. Alpine Bank Traffic Impact Study (98 pages).
15. Utility Plans for Alpine Bank (9 sheets).
16. Exhibit A Condition of Approval (1 sheet).
17. Landmark Preservation Commission Staff Report by Maren Bzdek, Senior Historic Preservation Planner, revised 2/16/2021 (9 pages).
18. Applicant submittal to Landmark Preservation Commission, dated January 25, 2021, from Galloway & Company (21 pages).
19. Landmark Preservation Commission minutes from February 17, 2021 regular meeting.
20. Copies of power point presentations presented during the initial public hearing and re-opened public hearing by Jason Holland.
21. Copy of power point presentation presented during the initial public hearing by Applicant.
22. Copy of Applicant waiver of LUC Section 2.2.7(D)(1) requirement that the Hearing Officer issue a written decision within ten (10) working days following the public hearing.
23. Rules of Conduct for Administrative Hearings.
24. Administrative (Type 1) Hearing: Order of Proceedings.
25. The City's Comprehensive Plan, Midtown Plan, Land Use Code, and the formally promulgated ordinances and polices of the City are all considered part of the record considered by the Hearing Officer.

TESTIMONY: The following persons testified at the initial public hearing or re-opened public hearing:

From the City:	Jason Holland, City Planner Maren Bzdek, Senior Historic Preservation Planner Spencer Smith, City Engineer
From the Applicant:	Zell Cantrell, Galloway & Company
From the Public:	None.
From the Public (via email):	None.

The March 15th hearing on this matter was closed at approximately 7:25 P.M. The April 5th re-opened hearing on this matter was closed at approximately 6:00 P.M.

FINDINGS

1. A public hearing on PDP #200020 was held on March 15, 2021 (the “initial public hearing”). On March 16, 2021, a member of the public notified City staff that the Zoom link for the initial public hearing was not posted on the City’s website as advertised in the public notice. After consultation with the City of Fort Collins City Attorney’s Office, the Hearing Officer determined that members of the public were not given an adequate opportunity to participate in the initial public hearing. The Hearing Officer determined that the public hearing on PDP #200020 should be re-opened to provide members of the public an opportunity to comment on the project. At the City’s and Hearing Officer’s request, the Applicant waived the requirement in LUC Section 2.2.7(D)(1) that the Hearing Officer issue a written decision within ten (10) working days following the initial public hearing.
2. The public hearing on PDP #200020 was re-opened on April 5, 2021 (the “re-opened public hearing”). A complete recording of the initial public hearing was available for the public to view from the City’s website. As such, Jason Holland, City Planner, provided only a brief overview of the project during the re-opened public hearing. Members of the public were then given an opportunity to comment on the project. However, no members of the public chose to testify at the re-opened public hearing.
3. Testimony of Jason Holland, City Planner, and evidence presented to the Hearing Officer established the fact that notice of the initial public hearing and the re-opened public hearing were properly posted, mailed, and published.
4. As required by City Council Ordinance 079, Series 2020 (the "City Ordinance"), the Hearing Officer, in consultation with City staff, determined that it was desirable to conduct the initial public hearing and the re-opened public hearing by remote technology so as to provide reasonably available participation by parties-in-interest and by the public, consistent with the requirements of the City Ordinance, because meeting in person would not be prudent for some or all persons due to a public health emergency.
5. On February 17, 2021, the City of Fort Collins Landmark Preservation Commission unanimously recommended that the Hearing Officer approve the Alpine Bank project, finding that Applicant’s proposal to move and rehabilitate the Architecturally Historic Building complies with the Secretary of Interior’s Standards for Rehabilitation, that the relocation of the building is sufficiently supported by satisfaction of three of the four criteria for modification of standards contained in the LUC (specifically, the criteria in Subsections 2.8.2(H)(1), (2), and (4)), and that the design of the new bank building complies with all six of the design compatibility standards contained in LUC Section 3.4.7(E), Table 1.

6. At the initial public hearing, Maren Bzdek, City of Fort Collins Senior Historic Preservation Planner, testified that LUC Section 3.4.7 requires compliance with the Secretary of Interior treatment standards (to the maximum extent feasible) and that these standards typically discourage relocation of a historic building. However, Ms. Bzdek explained that there are situations in which the treatment standards support a relocation, including the situation presented by the Alpine Bank project. Ms. Bzdek noted that from Landmark staff and Landmark Preservation Commission's perspective, a Modification of Standard from Section 3.4.7 is not necessarily required; nonetheless, the Landmark Preservation Commission found that the Hearing Officer's approval of the requested Modification of Standard to Section 3.4.7 would not be detrimental to the public good and that the criteria in Subsections 2.8.2(H)(1), (2), and (4) were met.
7. Based on testimony provided at the initial and re-opened public hearing and a review of the materials in the record of this case, the Hearing Officer concludes as follows:
 - A. The Application complies with the applicable procedural and administrative requirements of Article 2 of the Land Use Code.
 - B. The Modification of Standard to LUC Section 3.2.2(J)) to allow 9 parking spaces to be directly accessed from the alley and to shift the landscaped area required by Section 3.2.1(E)(4) inward on the Property as shown on Sheets 2 and 6 of the Site Plans for Alpine Bank: (i) will not be detrimental to the public good and (ii) will alleviate an unusual and exceptional practical difficulty, and an exceptional and undue hardship on the Applicant/Owner, not caused by the Applicant/Owner. With respect to foregoing, the Hearing Officer specifically finds:
 - i. The dedication of twelve (12) feet of additional right-of-way along S. College Avenue and the 10-foot landscaped sidewalk required for this project results in a narrower site and creates an exceptional physical condition, unique to the Property. The strict application of Section 3.2.2(J) (and Section 3.2.1(E)(4)) would result in an unusual and exceptional practical difficulty, and an exceptional and undue hardship on the Applicant/Owner, not caused by the Applicant/Owner.
 - ii. The Modification of Standard promote the public good by facilitating a site design that consolidates two existing access points along Prospect Road into the existing alley-access point and accomplishes much-needed infrastructure improvements along S. College Avenue. The Modification will thereby enhance traffic safety. The Modification will also enhance pedestrian safety by orienting pedestrian circulation away from the drive-through lane.

- C. The Modification of Standard to LUC Section 3.4.7 to allow for the relocation of the Architecturally Significant Building: (i) will not be detrimental to the public good; (ii) will promote the general purpose of Section 3.4.7 equally well or better than would a development plan which complies with Section 3.4.7; (iii) without impairing the intent and purpose of the LUC, will result in a substantial benefit to the City by reason of the fact that the project will substantially address an important community need specifically and expressly defined in the Comprehensive Plan, and strict application of the standard would render the project practically infeasible; and (iv) will not diverge from Section 3.4.7 except in a nominal, inconsequential way when considered from the perspective of the entire PDP and will continue to advance the purposes set forth in LUC Section 1.2.2. With respect to foregoing, the Hearing Officer specifically finds:
- i. The plans to relocate the Architecturally Significant Building are equal or better than a plan that would leave the building in its current location. The Modification allows the building to be preserved, reused, and incorporated into the PDP in a manner that will enhance the overall development and improve public safety. Specifically, public safety will be improved by the construction of the deceleration/right-hand turn lane along S. College Avenue, the installation of the 10-foot sidewalk, and the reduction of curb-cuts on the Property along S. College Avenue.
 - ii. The Modification provides for the Architecturally Significant Building to maintain a similar orientation and relationship to the street as its existing location. The architecture of the proposed Alpine Bank building also will compliment the historical features of the Architecturally Significant Building.
 - iii. The Modification will provide a substantial benefit to the City by furthering City Plan Policy LIV 10.7 (page 47 of City Plan), which encourages the creative reuse of historic resources in redevelopment activities.
 - iv. Not allowing the relocation of the Architecturally Significant Building would render the Alpine Bank project practically infeasible. As demonstrated by the Applicant's presentation at the initial public hearing, the proposed location of the S. College access drive and northbound right turn lane in relation to the existing location of the building would result in a practically infeasible site design and circulation pattern.

- v. The proposed relocation of the Architecturally Significant Building is nominal and inconsequential when considered from the perspective of the entire PDP because the new location positions the building within the context of the S. College Avenue corridor in a manner that is similar to the building's existing context and relationship to the street.
 - vi. The overall project plan continues to advance the purposes of the LUC Section 1.2.2 including:
 - encouraging innovations in land development and renewal by providing enhanced landscaping, desirable outdoor gathering space and an alternative paving design;
 - fostering the safe, efficient and economic use of the land, the City's transportation infrastructure, and other public facilities and;
 - encouraging patterns of land use which decrease trip length of automobile travel and encourage trip consolidation services by providing a business that is conveniently located near transit and within walking and bicycling distance for nearby residents;
 - increasing public access to mass transit, sidewalks, trails, bicycle routes and other alternative modes of transportation by providing sidewalk and bicycle lane improvements; and
 - encouraging the development of properties within established areas.
- D. Except for LUC Sections 3.2.2(J) and 3.4.7, which standards are recommended for Modifications of Standard, the Application complies with the applicable General Development Standards contained in Article 3 of the LUC, including the Development Standards for the TOD Overlay Zone in LUC Division 3.10.
- E. The Application complies with the applicable C-G district standards contained in LUC Article 4.
8. The Application's satisfaction of the applicable Article 2, 3, and 4 requirements of the Land Use Code is sufficiently evidenced by the Staff Report and the testimony and materials presented at the initial and re-opened hearing.

DECISION

Based on the findings set forth above, the Hearing Officer hereby enters the following ruling:

- A. The Modification of Standard to LUC Section 3.2.2(J) to allow 9 parking spaces to be directly accessed from the alley and to shift the landscaped area required by Section 3.2.1(E)(4) inward on the Property as shown on Sheets 2 and 6 of the Site Plans for Alpine Bank is approved.
- B. The Modification of Standard to LUC Section 3.4.7 to allow for the relocation of the Architecturally Significant Building in the manner shown on the Site Plans for Alpine Bank is approved.
- C. The PDP #200020 is approved for the Property, subject to the following condition of approval: The parking setback for the three (3) parking spaces located in the southeast corner of the Property, as depicted in **ATTACHMENT B** to this decision, shall be widened to provide a landscaped median that is at least five (5) feet wide as measured from the back of the median curbs.

DATED this 6th day of April, 2021.



Lori Strand
Hearing Officer

ATTACHMENT A

Staff Report
Alpine Bank, PDP #200020

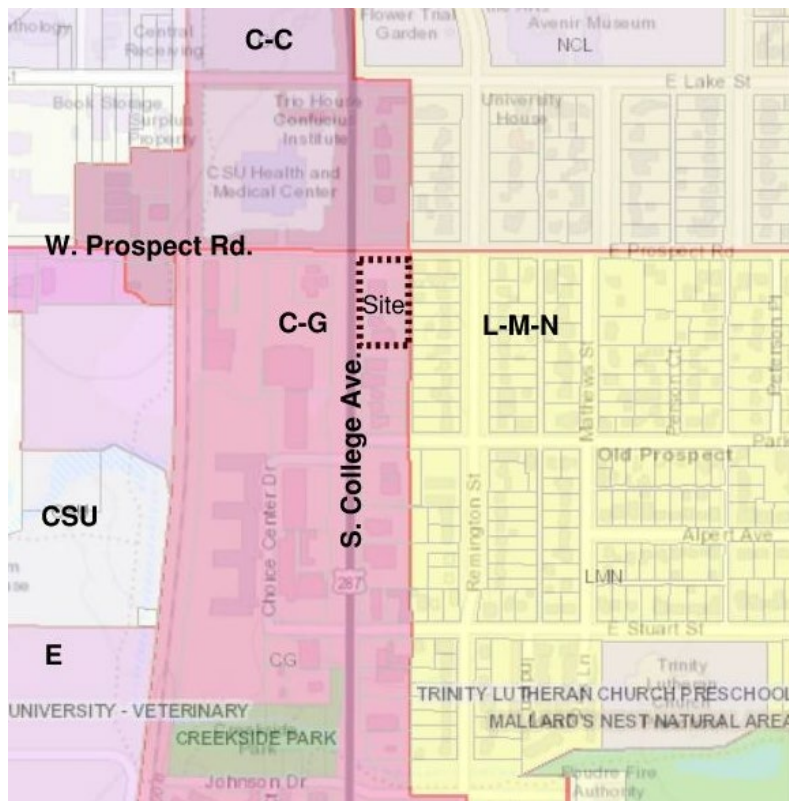
Administrative Hearing: March 15, 2021

Alpine Bank, PDP200020

Summary of Request

This is a request to construct a two story bank/office building and bank drive-through lanes on a 0.9 acre site. The proposed project will replat the existing parcels 9724216001, 9724216003, 9724216004, 9724216005, and 9724216006 (also known as 1608, 1610, and 1618 S College Avenue) into one lot. The two existing structures located on parcels 9724216001 & 9724216003 (1608 S College Avenue) and parcel 9724216006 (1618 S College Avenue) are proposed to be demolished, and the historic craftsman structure on parcel 9724216005 (1610 S College Avenue) is proposed to be relocated to the south end of the site. Access to the site will be taken from S. College Avenue and the alley. 28 parking spaces, interior sidewalks and landscape areas are proposed. A new 10 ft. wide sidewalk is proposed along S. College Avenue. The site is in the General Commercial (C-G) Zone District. Two Modifications of Standards are proposed.

Zoning Vicinity Map



Next Steps

If approved by the decision maker, a Final Development Plan may be submitted for the project.

Site Location

Located near the southeast corner of S. College Avenue and W. Prospect Road

Zoning

General Commercial (C-G)

Property Owner

Remington North, L.L.C.
1400 S. Colorado Blvd. Suite 410
Denver, CO 80222

Applicant/Representative

Zell Cantrell
Galloway & Company
6162 S Willow Drive, #320
Greenwood Village, CO 80111

Staff

Jason Holland, City Planner

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Staff Recommendation

Approval of the PDP and two Modifications, with one condition

1. Project Introduction

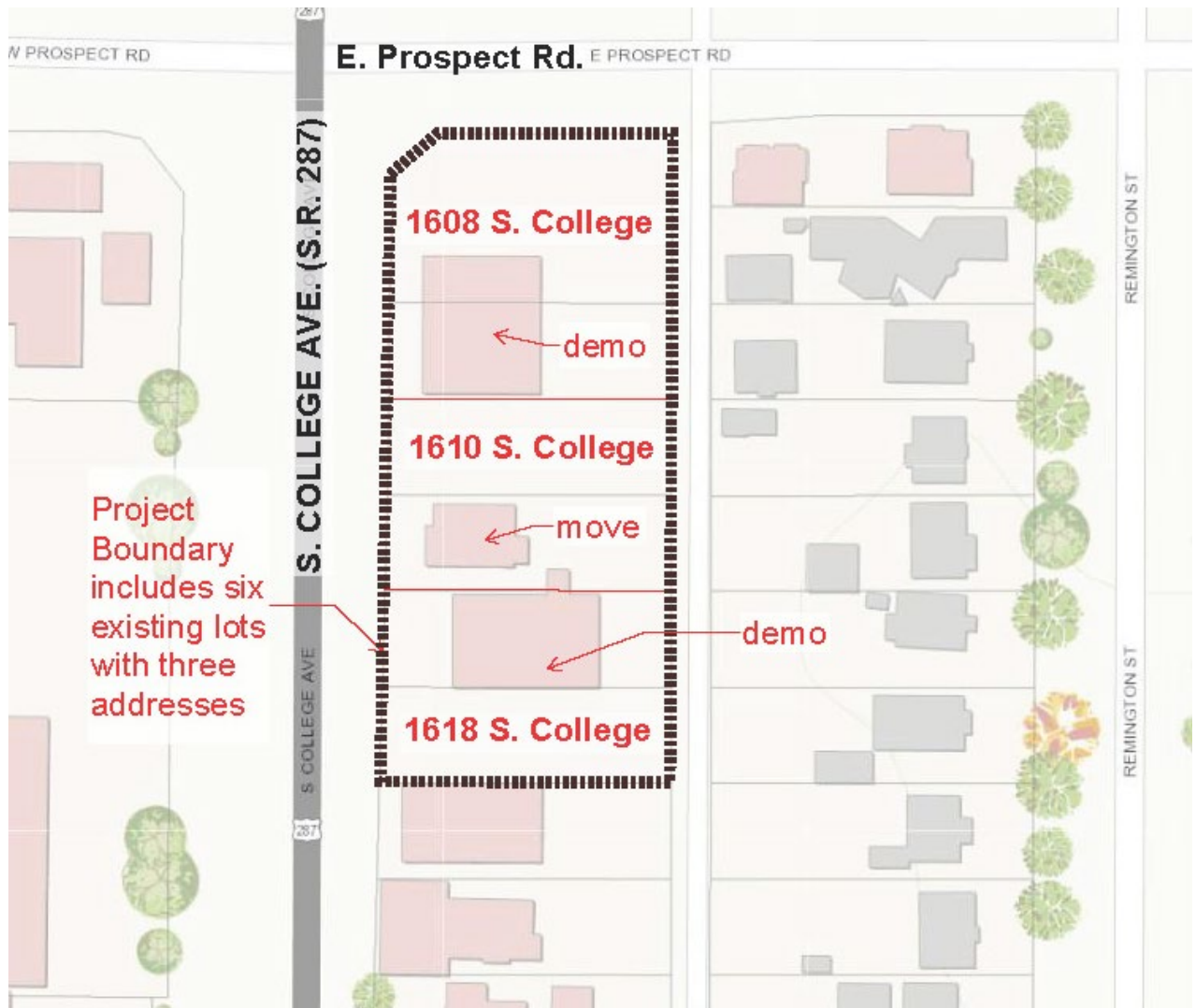
A. PROJECT DESCRIPTION

- This is a request to construct a two story bank/office building and bank drive-through lanes on a 0.9 acre site. The proposed project will replat the existing parcels -- 9724216001, 9724216003, 9724216004, 9724216005, and 9724216006 (also known as 1608, 1610, and 1618 S. College Avenue) into one lot.
- The site is in the General Commercial (C-G) Zone District. The two existing structures located on parcels 9724216001 & 9724216003 (1608 S College Avenue) and 9724216006 (1618 S College Avenue) are proposed to be demolished, and the historic structure on parcel 9724216005 (1610 S College Avenue) will be relocated to the south end of the site.
- Two Modifications of Standards are proposed which address Section 3.2.2(J) for minimum parking lot setbacks along a property line and *Section 3.4.7 Historic and Cultural Resources* for the relocation of an historic building currently located at 1610 S. College Avenue.
- Access to the site will be taken from S. College Avenue and the alley. 28 parking spaces, interior sidewalks and landscape areas are proposed.
- A new 10 ft. wide detached sidewalk and street trees are proposed along S. College Avenue.

B. SITE CHARACTERISTICS

1. Background

The property was annexed into the City in 1925. The property has been used in the past for both commercial and residential uses. The building at 1608 S. College is currently unoccupied and had been last used by Lewan Technology. 1618 S. College Avenue has been occupied by a number of different commercial uses over the years including a hair salon, consignment store and financial services. Both of these buildings are proposed to be demolished. The existing craftsman building located at 1610 S. College was originally a single-family residence and is considered an historic resource. This building has also been used for a number of commercial businesses over the years. The majority of the remaining areas of the property surrounding the buildings are paved and do not meet current parking lot standards.



Surrounding Zoning and Land Use

	North	South	East	West
Zoning	Community Commercial (C-C)	General Commercial (C-G)	Low Density Mixed Use Neighborhood (L-M-N)	General Commercial (C-G)
Land Use	Offices, retail and restaurant uses; C.S.U. Medical Health Center	Offices, Service Shops	Residential	Shopping Center – Retail, Office, and Restaurant; future hotel

2. Comprehensive Plan

A. CITY PLAN (2019)

Under City Plan, the project is located in the Urban Mixed Use District within the City's overall Structure Plan (further described on page 99 of [City Plan](#)). This area envisions high-density development, particularly near City transit stations. Additionally, page 25 of City Plan discusses a focus on maximizing infill/redevelopment in the Midtown area with higher density residential, employment and services in support of City transit facilities.

Notable Principles and Policies in City Plan envision high quality redevelopment in Midtown:

PRINCIPLE LIV 2: Promote infill and redevelopment:

POLICY LIV 2.1 - REVITALIZATION OF UNDERUTILIZED PROPERTIES

Support the use of creative strategies to revitalize vacant, blighted or otherwise underutilized structures and buildings.

POLICY LIV 3.5 - DISTINCTIVE DESIGN

Require the adaptation of standardized corporate architecture to reflect local values and ensure that the community's appearance remains unique.

POLICY LIV 3.6 - CONTEXT-SENSITIVE DEVELOPMENT

Ensure that all development contributes to the positive character of the surrounding area. Building materials, architectural details, color range, building massing, and relationships to streets and sidewalks should be tailored to the surrounding area.

B. MIDTOWN SUBAREA PLAN (2013)

The project is located in the [Midtown Subarea](#). To address the guidelines in the Midtown Subarea Plan, specific standards are included in the Land Use Code under Division 3.10 -- Development Standards for the Transit-Oriented Development (TOD) Overlay Zone.

The Midtown Subarea Plan vision and guidelines emphasize:

1. Excellence in Design: Improvements in Midtown, including buildings, landscapes, and site design should be of high quality. A wide variety of designs that express creativity should be welcomed.
2. High quality architectural design should have a distinct identity that distinguishes it from other parts of the city.
3. Design that is inviting to pedestrians and bicyclists, with attractive, inviting street edges, and active urban plazas and spaces.
4. New development that is higher density, more urban in nature and with buildings that will address S. College Avenue with parking in back. Per the Midtown design guidelines Chapter 6-12, a goal for Midtown is to increase the density of development such that most parking will be in structures, either in facilities primarily designed for parking, or in a building in which parking serves other uses on the site. However, some surface parking will continue to be necessary. Where it does occur, the visual impact of surface parking should be minimized.
5. Parking should be subordinate and masked by buildings or landscape and located mostly internal to the blocks. Connections should be provided through large blocks to allow for easier pedestrian access and circulation.
6. Landscapes should include a palette that is rich, distinctive and coordinated. High quality plants and materials should be used and creativity in landscape is also encouraged to contribute to a sense of identity.

7. Site design should reinforce the urban fabric, taking into consideration pedestrians, visual interest, and high quality resident experiences. Each site should consider its surroundings and respond appropriately to the context around it.
8. Maintaining maximum parking ratios rather than minimum requirements for commercial development allows developers the flexibility of reducing parking as they see fit, and lowering parking supplies will further encourage customers and employees to access the area by means other than single occupant vehicles. However, while developers should be allowed the flexibility of reducing supply, they should still demonstrate that their site can accommodate anticipated parking without causing significant spillover into adjacent properties.

3. Public Outreach

A. NEIGHBORHOOD MEETING

Pursuant to *Section 2.2.2 – Step 2: Neighborhood Meetings*, a neighborhood meeting is not required for Administrative (Type 1) projects.

B. PUBLIC COMMENTS

Any communication received between the public notice period and hearing will be forwarded to the Hearing Officer to be considered when making a decision on the project.

4. Article 2 – Applicable Standards

A. PROJECT DEVELOPMENT PLAN PROCEDURAL OVERVIEW

The PDP complies with all applicable Development Review Procedures in Division 2.2 of the Land Use Code:

1. Conceptual Review - CDR200057

A conceptual review meeting was held on August 6, 2020.

2. Project Development Plan Submittal – PDP 200020

The first submittal of this project was completed on December 4, 2020.

3. Neighborhood Meeting

2.2.2 – Step 2: Neighborhood Meetings -- Not applicable.

4. Notice (Posted, Written and Published)

Posted Notice: December 7, 2020, Sign # 570

Written Hearing Notice: March 1, 2021, 125 addresses mailed.

Published Hearing Notice: March 8, 2021, Coloradoan Confirmation # 4625734

B. DIVISION 2.8 – MODIFICATIONS OF STANDARDS

The applicant requests two Modifications of Standards and provides justification letters attached to this staff report.

The Land Use Code is adopted with the recognition that there will be instances where a project would support the implementation of City Plan, but due to unique or unforeseen circumstances would not meet a specific standard of the Land Use Code as stated. The modification process and criteria in Land Use Code Division 2.8.2(H) provide for evaluation of these instances on a case-by-case basis, as follows:

Land Use Code Modification Criteria:

“The decision maker may grant a modification of standards only if it finds that the granting of the modification would not be detrimental to the public good, and that:

(1) the plan as submitted will promote the general purpose of the standard for which the modification is requested equally well or better than would a plan which complies with the standard for which a modification is requested; or

(2) the granting of a modification from the strict application of any standard would, without impairing the intent and purpose of this Land Use Code, substantially alleviate an existing, defined and described problem of city-wide concern or would result in a substantial benefit to the city by reason of the fact that the proposed project would substantially address an important community need specifically and expressly defined and described in the city's Comprehensive Plan or in an adopted policy, ordinance or resolution of the City Council, and the strict application of such a standard would render the project practically infeasible; or

(3) by reason of exceptional physical conditions or other extraordinary and exceptional situations, unique to such property, including, but not limited to, physical conditions such as exceptional narrowness, shallowness or topography, or physical conditions which hinder the owner's ability to install a solar energy system, the strict application of the standard sought to be modified would result in unusual and exceptional practical difficulties, or exceptional or undue hardship upon the owner of such property, provided that such difficulties or hardship are not caused by the act or omission of the applicant; or

(4) the plan as submitted will not diverge from the standards of the Land Use Code that are authorized by this Division to be modified except in a nominal, inconsequential way when considered from the perspective of the entire development plan, and will continue to advance the purposes of the Land Use Code as contained in Section 1.2.2.

Any finding made under subparagraph (1), (2), (3) or (4) above shall be supported by specific findings showing how the plan, as submitted, meets the requirements and criteria of said subparagraph (1), (2), (3) or (4).

1. Description of the Modification to *Section 3.2.2(J) Setbacks*:

The Applicant proposes to shift the landscaped setback along the east property line, with 9 parking spaces along the alley having no landscaped setback, thus allowing the parking spaces to be directly accessed from the alley.

This standard provides minimum and average dimensions for vehicle use area setbacks along street rights-of-way and perimeter lot lines:

3.2.2(J) Setbacks. Any vehicular use area containing six (6) or more parking spaces or one thousand eight hundred (1,800) or more square feet shall be set back from the street right-of-way and the side and rear yard lot line (except a lot line between buildings or uses with collective parking) consistent with the provisions of this Section, according to the following table:

	<i>Minimum Average of Entire Landscaped Setback Area (feet)</i>	<i>Minimum Width of Setback at Any Point (feet)</i>
<i>Along an arterial street</i>	15	5
<i>Along a nonarterial street</i>	10	5
<i>Along a lot line *</i>	5	5

* *Setbacks along lot lines for vehicular use areas may be increased by the decision maker in order to enhance compatibility with the abutting use or to match the contextual relationship of adjacent or abutting vehicular use areas.*

Additionally, this setback standard is referenced in the parking lot perimeter landscaping standard in Section 3.2.1(E)(4), which requires landscaping in the minimum setback areas required by Section 3.2.2(J).

2. Applicant’s Justification for the Modification to Section 3.2.2(J) Setbacks:

The Applicant’s modification request is attached with this staff report. The Applicant contends that the modification meets two of the four criteria:

Criteria 3 of 4 – “by reason of exceptional physical conditions or other extraordinary and exceptional situations, unique to such property, including, but not limited to, physical conditions such as exceptional narrowness, shallowness or topography, or physical conditions which hinder the owner’s ability to install a solar energy system, the strict application of the standard sought to be modified would result in unusual and exceptional practical difficulties, or exceptional or undue hardship upon the owner of such property, provided that such difficulties or hardship are not caused by the act or omission of the applicant”

Applicant:

“Section 2.8.2(H) of the Fort Collins Land Use Code (LUC) provides that “the decision Maker may grant a modification of standards only if it finds that the granting of the modification would not be detrimental to the public good.” Even more than not being detrimental to the public good, the requested modification would benefit the public for the following reasons: (i) allow for the redevelopment of this quadrant of this prominent intersection with a first class community oriented banking service not currently present in the community; and (ii) consolidate two existing access points along Prospect Road into a single existing access point (Alley) and (iii) allow for improvements along S. College Avenue in a manner that will enhance traffic safety in a manner consistent with the other three quadrants of this intersection.”

“Additionally, we believe integration of the alley and parking into the overall site circulation allows for more efficient movement of vehicles, bicycles, and pedestrians throughout the proposed development and surrounding areas more safely and conveniently adding to the overall attractiveness and integration of the project into the existing context. Pedestrian safety is addressed by orienting limited pedestrian circulation away from the drive-through lane and providing pedestrian dedicated circulation from the alley-oriented parking to the bank building. If parking were flipped with access from interior of the site, there would be a direct conflict between pedestrian traffic and vehicular traffic in the drive-through lanes. Flipping the parking to be accessible from the alley eliminates this conflict and make could use of an existing public ROW.”

“We believe the requirement for the ROW dedication and subsequent improvements along College Avenue create an exceptional physical condition by narrowing the property such that full compliance with the LUC becomes difficult while working to develop a functional site plan that will lead to the long term success of this redevelopment. Relief from this set-back requirement along a portion of the property line will allow for a functional solution within the context of both existing and imposed contextual site conditions.”

Criteria 4 of 4 – “the plan as submitted will not diverge from the standards of the Land Use Code that are authorized by this Division to be modified except in a nominal, inconsequential way when considered from the perspective of the entire development plan, and will continue to advance the purposes of the Land Use Code as contained in Section 1.2.2.”

Applicant:

"We believe the plan as submitted does not significantly diverge from the standards defined in the Land Use Code but instead represents a nominal modification when compared to the overall benefits offered by the proposed development plan. While a 5' landscape set-back is not provided the entire length of the eastern property line, we have been able to incorporate a wider landscape median interior to the site that allows for a landscape buffer between the more intensive drive-through use and the adjacent property to the East. We have been able to effectively create a wider landscape set-back or buffer immediately adjacent to the most intensive use on the site. Offsetting this buffer from the property line interior to the site has also allowed us to incorporate additional landscape islands and subsequently larger shade trees closer to the property line. These additional islands are the minimum 8' wide and 17' deep which when combined with the larger shade trees provides both a greater horizontal and vertical landscape set-back/buffer than would be provided by a straight 5' landscape set-back. We believe this additional landscaping advances the purposes of the LUC by providing additional landscape set-back/buffer between our proposed use and adjacent properties to the East."

3. Staff Analysis and Findings of Fact for the Modification to *Section 3.2.2(J) Setbacks*:

Staff finds that the request for the Modification of Standard to *Section 3.2.2(J) Setbacks* is justified by the applicable standards in 2.8.2(H)(3) and 2.8.2(H)(4):

The Modification satisfies criterion 2.8.2(H)(3) – By reason of exceptional physical conditions or other extraordinary and exceptional situations, unique to such property, including, but not limited to, physical conditions such as exceptional narrowness, shallowness or topography, or physical conditions which hinder the owner's ability to install a solar energy system, the strict application of the standard sought to be modified would result in unusual and exceptional practical difficulties, or exceptional or undue hardship upon the owner of such property, provided that such difficulties or hardship are not caused by the act or omission of the applicant.

- A. Staff finds that the modification meets criterion 2.8.2(H)(3) and is not detrimental to the public good because:
 - 1) The dedication of 12 feet of additional right-of-way along S. College Avenue and the 10-foot landscaped sidewalk requirement creates a narrower site, and no drive through lanes or parking areas are permitted between the building and the streets;
 - 2) Strict compliance with the standard can only be achieved by providing the parking within the drive-through area, which is an exceptional practical difficulty.

The Modification satisfies criteria 2.8.2(H)(1)(4) – The plan as submitted will not diverge from the standards of the Land Use Code that are authorized by this Division to be modified except in a nominal, inconsequential way when considered from the perspective of the entire development plan and will continue to advance the purposes of the Land Use Code as contained in Section 1.2.2.

- B. Staff finds that the modification meets criterion 2.8.2(H)(4) and is not detrimental to the public good because:
 - 1) A 6-foot wide landscape median is proposed along the drive through lanes which provides screening for this area;
 - 2) An additional landscape island is provided in near the middle of the nine parking spaces which provides additional tree coverage and landscaping along the alley;
 - 3) The overall project plans continue to advance the purposes of the Land Use Code as contained in Section 1.2.2 including;

(B) encouraging innovations in land development and renewal by providing enhanced landscaping, desirable outdoor gathering space and an alternative paving design;

(C) fostering the safe, efficient and economic use of the land, the city's transportation infrastructure, and other public facilities and;

(F) encouraging patterns of land use which decrease trip length of automobile travel and encourage trip consolidation services by providing a business that is conveniently located near transit and within walking and bicycling distance for nearby residents;

(G) increasing public access to mass transit, sidewalks, trails, bicycle routes and other alternative modes of transportation by providing sidewalk and bicycle lane improvements; and

(L) encouraging the development of properties within established areas.

4. Description of the Modification to Section 3.4.7 Historic and Cultural Resources:

The Applicant proposes to move the structure located at 1610 S. College Avenue, which is eligible for Fort Collins Landmark designation, to a new location at the southern end of the development site and rehabilitate it for adaptive reuse. *Section 3.4.7 – Historic and Cultural Resources* has no provisions in place for the relocation of structures deemed to be of architectural significance.

5. Applicant's Modification Justification for Section 3.4.7 Historic and Cultural Resources:

The Applicant's modification request is attached to this staff report. The Modification of Standard to *Section 3.4.7 – Historic and Cultural Resources* is requested to allow for the relocation of the historically eligible building currently located at 1610 S. College Avenue.

The Applicant contends that the modification meets three of the four criteria:

Criteria 1 of 4 – “The plan as submitted will promote the general purpose of the standard for which the modification is requested equally well or better that would a plan which complied with the standard for which the modification is requested.”

Applicant:

“The plan as submitted which anticipates the relocation of the existing historical resource will promote the general purpose of the standard by allowing the resource to be preserved, reused, and incorporated into the proposed development in a manner that will not only enhance the overall development but also address public safety. The proposed relocation will not adversely affect the integrity of the historic resources on nearby property because the adjacent properties along S. College have not been deemed of historical significance and the relocated building will not be moved from the property. The relocation will also allow for the design of a site plan compatible with and protect the historical resource by integrating it into the overall site plan in a functional manner. “

Criteria 2 of 4 – “the granting of a modification from the strict application of any standard would, without impairing the intent and purpose of this Land Use Code, substantially alleviate an existing, defined and described problem of city-wide concern or would result in a substantial benefit to the city by reason of the fact that the proposed project would substantially address an important community need specifically and expressly defined and described in the city's Comprehensive Plan or in an adopted policy, ordinance or resolution of the City Council, and the strict application of such a standard would render the project practically infeasible.”

Applicant:

“The plan as submitted will benefit the city by allowing for the redevelopment and subsequent off-site improvements at the southeast quadrant of the intersection of S. College & E. Prospect. The proposed

improvements will not only be consistent with recent improvements at the other three quadrants on the intersection but also address an overall community need of improving traffic safety at the intersection through improved design and traffic movement.”

Criteria 4 of 4 – “the plan as submitted will not diverge from the standards of the Land Use Code that are authorized by this Division to be modified except in a nominal, inconsequential way when considered from the perspective of the entire development plan, and will continue to advance the purposes of the Land Use Code as contained in Section 1.2.2.”

Applicant:

“We believe the proposed relocation will not diverge from the standards of the LUC except in a nominal, inconsequential way that will likely not be noticed once the redevelopment and relocation are complete. From an overall perspective the relocation creates a significant positive impact on the redevelopment and future use of the building when compared to leaving the building in the original setting.”

6. Staff Analysis and Findings of Fact for the Modification to *Section 3.4.7 Historic and Cultural Resources*:

Staff finds that the applicant has presented sufficient information to justify a modification of standard to allow for the relocation of the historic Craftsman residence currently located at 1810 S. College Avenue based on satisfaction of three of the four criteria (only one is required). Approval of the Modification is recommended by the Landmark Preservation Commission (LPC), and the staff report and applicant presentation for the LPC meeting are attached with this staff report (Attachments 15 and 16).

Staff finds that the request for the Modification of Standard to Section 3.4.7 to allow the relocation of the historically eligible craftsman building is justified by the applicable standards in 2.8.2(H)(1), 2.8.2(H)(2) and 2.8.2(H)(4):

- A. Staff finds that the modification meets criterion 2.8.2(H)(1) and is not detrimental to the public good because:
 - 1) The plans to move the building are equal or better than a plan that would leave the building in its current location, due to the additional site and setting constraints that are required to accommodate the project entrance from S. College Avenue and the required right-of-way dedication for the northbound right turn lane.
 - 2) The modification is not detrimental to the public good because the modification allows the relocation and adaptive reuse of the building within the project with a similar orientation and relationship to the street as the existing location while allowing for the installation of beneficial site improvements and public infrastructure.
- B. Staff finds that the modification meets criterion 2.8.2(H)(2) and is not detrimental to the public good because:
 - 1) The proposed project plan to relocate the building would provide a substantial benefit to the city by reason of the fact that the proposed project would substantially address an important community need specifically and expressly defined and described in the city's Comprehensive Plan, by addressing City Plan Policy LIV 10.7 (page 47 of [City Plan](#)), which encourages the creative reuse of historic resources in redevelopment activities, and;
 - 2) The strict application of the standard would render the proposed project practically infeasible due to the proposed location of the S. College access drive and northbound right turn lane.
- C. Staff finds that the modification meets criterion 2.8.2(H)(4) and is not detrimental to the public good because:

- 1) The proposed modification to relocate the craftsman building is nominal and inconsequential when considered from the perspective of the entire development plan because the new proposed location positions the historic resource within the context of the S. College Avenue corridor in a manner that is similar to the building’s existing context and relationship to the street and therefore represents a nominal change from its current location, and;
- 2) The overall project plan continues to advance the purposes of the Land Use Code as contained in Section 1.2.2 including:
 - (B) encouraging innovations in land development and renewal by providing enhanced landscaping, desirable outdoor gathering space and an alternative paving design;
 - (C) fostering the safe, efficient and economic use of the land, the city’s transportation infrastructure, and other public facilities and;
 - (F) encouraging patterns of land use which decrease trip length of automobile travel and encourage trip consolidation services by providing a business that is conveniently located near transit and within walking and bicycling distance for nearby residents;
 - (G) increasing public access to mass transit, sidewalks, trails, bicycle routes and other alternative modes of transportation by providing sidewalk and bicycle lane improvements; and
 - (L) encouraging the development of properties within established areas.

5. Article 3 – Applicable General Development Standards

A. DIVISION 3.2 - SITE PLANNING AND DESIGN STANDARDS

Applicable Code Standard	Summary of Code Requirement and Analysis	Staff Findings
Section 3.2.1 Landscaping and Tree Protection		
3.2.1(C) General Standard	<p><i>3.2.1(B) Purpose. The intent of this Section is to require preparation of landscape and tree protection plans that ensure significant canopy cover is created, diversified and maintained so that all associated social and environmental benefits are maximized to the extent reasonably feasible. These benefits include reduced erosion and stormwater runoff, improved water conservation, air pollution mitigation, reduced glare and heat build-up, increased aesthetics, and improved continuity within and between developments. Trees planted in appropriate spaces also provide screening and may mitigate potential conflicts between activity areas and other site elements while enhancing outdoor spaces, all of which add to a more resilient urban forest.</i></p> <p><i>3.2.1(C) General Standard</i></p> <p><i>All developments shall submit a landscape and tree protection plan, and, if receiving water service from the City, an irrigation plan, that: (1) reinforces and extends any existing patterns of outdoor spaces and vegetation where practicable, (2) supports functional purposes such as spatial definition, visual screening, creation of privacy, management of microclimate or drainage, (3) enhances the appearance of the development and neighborhood, (4) protects significant trees, natural systems and habitat, (5) enhances the pedestrian environment, (6) identifies all landscape areas, (7)</i></p>	Complies

	<p><i>identifies all landscaping elements within each landscape area, and (8) meets or exceeds the standards of this Section.</i></p> <ul style="list-style-type: none"> • The project provides a landscape design that meets the purpose and general standard for landscaping and tree protection. The tree canopy provided and preserved meets city requirements. In accordance with objectives one through five of the general standards, shrub and ground cover plantings are arranged to provide screening, visual interest and spatial definition within the parking lot and around the buildings. • Per criterion 8 in the General Standard, compliance with Section 3.2.1 is further described through the following specific landscape and tree protection design standards applicable to the project. 	
<p>3.2.1(D) Tree Planting Standards</p>	<p>This section requires that all developments establish groves and belts of trees along all city streets, in and around parking lots, and in all landscape areas that are located within fifty (50) feet of any building or structure in order to establish at least a partial urban tree canopy.</p> <ul style="list-style-type: none"> • This requirement is met with a combination of existing and proposed trees in compliance with the specific tree planting standards outlined in the sections below. 	<p>Complies</p>
<p>3.2.1(D)(1)(c) Full Tree Stocking</p>	<p>This section requires that full tree stocking be provided in all landscape areas within fifty (50) feet of any building or structure. Landscape areas shall be provided in adequate numbers, locations and dimensions to allow full tree stocking to occur along all high use or high visibility sides of any building or structure, in accordance with the spacing standards outlined in this section:</p> <p>Canopy shade trees: 30' - 40' spacing Coniferous evergreens: 20' - 40' spacing Ornamental trees: 20' - 40' spacing</p> <p>Exact locations and spacings may be adjusted at the option of the applicant to support patterns of use, views and circulation as long as the minimum tree planting requirement is met. Canopy shade trees shall constitute at least fifty (50) percent of all tree plantings. Required street trees may be used to contribute to this standard.</p> <ul style="list-style-type: none"> • All sides of the buildings meet or exceed the tree stocking requirement. The total building perimeter around all sides of both buildings is approximately 533 feet, which would require 18 trees if spaced at 30-foot intervals. This standard is met with 26 proposed trees placed within 50 feet of the perimeter of the proposed buildings. • The S. College Avenue frontage is also emphasized, with 17 trees provided along the building's 173-foot College Avenue frontage. At least 6 trees are required along this portion of the building's frontage. The increase in tree planting in this area contributes to the project's compatibility with the surrounding area by enhancing the visual quality of the building frontage and helping mitigate the mass and bulk of the building form. • The additional trees along the College frontage contribute to the Midtown Subarea Plan's goal of providing high-quality, pedestrian-oriented spaces by expanding the tree canopy coverage in this area, enhancing comfort, and reinforcing an appropriate human scale along the project's main building facades. 	<p>Complies</p>

	<ul style="list-style-type: none"> A total of 32 trees are provided with the proposed project, with three of these trees being existing Bur Oaks along E. Prospect Road. Of the 32 trees, 17 are canopy shade trees which meets the requirement that at least 50% of the trees be canopy shade trees. 	
3.2.1(D)(2) Street Trees	<p>This section requires that canopy shade trees be planted along public sidewalks at thirty-foot to forty-foot spacing and to the extent reasonably feasible, be positioned at evenly spaced intervals.</p> <ul style="list-style-type: none"> Nine shade trees are provided along the College Avenue and Prospect frontage. The tree locations are spaced at 30' intervals and are positioned to comply with traffic and utility separation requirements. 	Complies
3.2.1(D)(3) Minimum Species Diversity	<p>Eight different tree species are proposed for the 32 trees provided, with a 19% maximum of any one species proposed. This meets and exceeds the diversity standard which requires that the maximum percentage of any one species be not more than 33% when 29-39 trees are on the site.</p>	Complies
3.2.1(D)(4) Tree Species and Minimum Sizes	<p>All minimum required tree and shrub sizes are met.</p>	Complies
3.2.1(E)(4) Parking Lot Perimeter Landscaping	<p>This section requires one tree per twenty-five linear feet within the parking lot setback areas along a public street and one tree per forty linear feet along a side lot line parking setback area. Trees may be spaced irregularly in informal groupings or be uniformly spaced, as consistent with larger overall planting patterns and organization. Perimeter landscaping along a street may be located in and should be integrated with the streetscape in the street right-of-way.</p> <ul style="list-style-type: none"> Along College Avenue, 3 trees are placed along the parking lot street setback area between the two proposed buildings. The tree locations work in tandem with the proposed street tree pattern and other tree plantings along the building frontage. For the side lot line along the east of the property (along the alley), five trees are provided for the 200 linear feet of parking frontage, meeting the 40' spacing requirement. <p>This section also requires screening from the street and abutting uses (walls, fences, berming, plant material, or similar) of at least thirty (30) inches in height for a minimum of seventy percent (70%) of the length of the street frontage shall be provided.</p> <ul style="list-style-type: none"> The project proposes continuous plant material coverage along the College Avenue and east parking areas within the site, meeting the standard. Plant species selected are appropriate and will achieve a minimum height of thirty inches. 	Complies; Modification Requested
3.2.1(E)(5) Parking Lot Interior Landscaping	<p>This section requires six percent of the interior space of all parking lots with less than one hundred spaces to be landscape areas:</p> <ul style="list-style-type: none"> The proposed interior parking area is 6,160 square feet, and 370 square feet, or 6% of interior landscape space is provided in accordance with the standard. 	Complies

	<p>This section also has minimum requirements for Landscaped Islands:</p> <p>In addition to any pedestrian refuge areas, each landscaped island shall include one (1) or more canopy shade trees, be of length greater than eight (8) feet in its smallest dimension, include at least eighty (80) square feet of ground area per tree to allow for root aeration, and have raised concrete curbs.</p> <ul style="list-style-type: none"> • All landscape islands exceed the 80 square-foot requirement. The minimum interior island size provides approximately 84 square feet of interior space. • All interior islands are at least 8 feet wide and provide at least one shade tree. 	
<p>3.2.1(F) Tree Protection and Replacement</p>	<p>This standard requires that the project preserve and protect existing significant trees within the Limits of Development to the extent reasonably feasible, and these trees may help satisfy the landscaping requirements of the development. Streets, buildings and lot layouts shall be designed to minimize the disturbance to significant existing trees. All required landscape plans shall accurately identify the locations, species, size and condition of all significant trees, each labeled showing the applicant’s intent to either remove, transplant or protect.</p> <p>A significant tree is defined in Article 5 as any tree that has a DBH (diameter at breast height) of six inches or more.</p> <p>Any affected tree that is removed shall be replaced with not less than one (1) or more than six (6) replacement trees sufficient to mitigate the loss of value of the removed significant tree. The rated value of the trees is determined by the City Forester in coordination with the Applicant’s certified arborist.</p> <ul style="list-style-type: none"> • In order to address the tree mitigation requirements in The Land Use Code, the applicant has submitted a tree mitigation plan, attached with this report (please refer to PDP Sheet 9 of 11). The plan describes the species, condition, and size of the existing trees and assigns a mitigation value (0 through 6) for the existing trees. Through the process of an on-site evaluation involving both the city forestry staff and the applicant, the health of the existing trees was evaluated, and a mitigation value was assigned to each tree by city staff, as required by the LUC standard. • A total of 11 significant existing trees are located within the project’s limits of development. Of this total, 3 are proposed to remain, with the remaining 8 trees proposed to be removed and mitigated. A total of 18 upsized mitigation trees are required. 15 mitigation trees are proposed on-site (as noted with the (M) on the planting legend, and 3 mitigation trees will be provided off-site through City Forestry’s cash-in-lieu program. • Staff is satisfied that all efforts have been made to retain significant trees to the extent reasonably feasible because the redevelopment of this urban site to meet current code standards requires a significant reconfiguration of the building locations and surrounding parking layout. The project satisfies staff’s recommended mitigation requirements by providing recommended mitigation trees on the site. Based on the existing tree evaluation process and aspects of the site plan configuration outlined above, staff’s opinion is that the project satisfies the tree protection and replacement standards of this section by preserving and protecting existing significant trees within the Limits of Development to the extent reasonably feasible, by providing an adequate number of new mitigation trees in locations and with species selections that are suitable to provide a long-term contribution to the City urban tree canopy, and by providing a cash-in-lieu payment for three of the mitigation trees. 	<p>Complies</p>

Section 3.2.2 Access, Circulation and Parking		
<p>3.2.2(B) General Standard</p> <p>3.2.2(C)(1) Development Standards</p> <p>Safety Considerations</p>	<p>In conformance with the Purpose, General Standard, and Development Standards described in this section, the parking and circulation system provided with the project is adequately designed with regard to safety, efficiency and convenience for vehicles, bicycles, pedestrians and transit, both within the development and to and from surrounding areas:</p> <ul style="list-style-type: none"> As required, the sidewalk system provided addresses vehicle conflicts and contributes to the attractiveness of the development. An interior sidewalk system provides convenient access from the parking areas to the building entrances. A new 10' wide detached sidewalk is proposed along the S. College Avenue frontage per staff recommendations. Along the E. Prospect Road frontage, a 10' wide sidewalk has already been constructed by the City. 	<p>Complies</p>
<p>3.2.2(C)(4) Bicycle Facilities</p>	<p>This standard requires at least 1 bicycle parking space per 4,000 square feet of commercial building space, and a minimum of four spaces. At least one of these spaces must be enclosed/covered.</p> <ul style="list-style-type: none"> The four bicycle parking spaces are provided with two exterior bicycle racks provided near each building entrance. One parking space is provided within the bank building to meet the covered parking requirement. 	<p>Complies</p>
<p>3.2.2(D) Access and Parking Lot Requirements</p>	<p>This standard requires that all vehicular use areas in any proposed development be designed to be safe, efficient, convenient and attractive, considering use by all modes of transportation that will use the system, (including, without limitation, cars, trucks, buses, bicycles and emergency vehicles). To the maximum extent feasible, pedestrians and vehicles shall be separated through provision of a sidewalk or walkway. Where complete separation of pedestrian and vehicles is not feasible, potential hazards shall be minimized by using landscaping, bollards, special paving, lighting and other means to clearly delineate pedestrian areas.</p> <ul style="list-style-type: none"> The project complies with this standard by providing sidewalk improvements along S. College Avenue as well as an east/west sidewalk connection through the parking lot to the alley. Per the <i>Pedestrian/Vehicle Separation</i> requirement in 3.2.2(D)(1), the east/west walkway is defined using cross walk striping to delineate the pedestrian route and enhance safety. Per the parking lot location standards describe in 3.2.2(D)(3), the required off-street parking spaces are located on the same lot or premises as the building. 	<p>Complies</p>
<p>3.2.2(J) Setbacks (for vehicle use areas)</p>	<p>This section requires that any vehicular use area containing six or more parking spaces or one thousand eight hundred (1,800) or more square feet shall be set back from the street right-of-way and the side and rear yard lot line (except a lot line between buildings or uses with collective parking) consistent with the provisions of this Section, according to the following table:</p>	<p>Modification Requested, and Condition of Approval</p>

	<table border="1"> <thead> <tr> <th></th> <th>Minimum Average of Entire Landscaped Setback Area (feet)</th> <th>Minimum Width of Setback at Any Point (feet)</th> </tr> </thead> <tbody> <tr> <td>Along an arterial street</td> <td>15</td> <td>5</td> </tr> <tr> <td>Along a nonarterial street</td> <td>10</td> <td>5</td> </tr> <tr> <td>Along a lot line</td> <td>5</td> <td>5</td> </tr> </tbody> </table>		Minimum Average of Entire Landscaped Setback Area (feet)	Minimum Width of Setback at Any Point (feet)	Along an arterial street	15	5	Along a nonarterial street	10	5	Along a lot line	5	5	
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<p>3.2.2(K)(2) Nonresidential Parking Requirements</p>	<ul style="list-style-type: none"> The project complies with the 5-foot minimum and 15-foot average setback requirements along the S. College Avenue arterial right-of-way -- where the average setback is approximately 16 feet, but does not comply along the east side lot line near the alley. A Modification request is provided by the applicant and staff recommends approval to allow reduce the setback along the east property line (please see Modification section of this staff report). A condition of approval is recommended to address a landscaped setback area along the south portion of the alley which is less than 5 feet (see Exhibit A, Attachment 14). <p>Section 3.2.2(K)(2)(b) Nonresidential Parking Requirements – Existing Buildings Exemption states, in part: <i>For the redevelopment of a property which includes the demolition of existing buildings, the minimum parking requirement shall be applied to the net increase in the square footage of new buildings.</i></p> <p>Existing buildings are proposed to be demolished, and one building is proposed to be moved; however, the applicant has not proposed to use this reduction.</p> <p>Parking Required:</p> <p>Section 3.2.2(K)(2)(a) outlines both minimum and maximum parking for commercial uses based on the 9,342 square feet proposed:</p> <ul style="list-style-type: none"> ➤ 2/1,000 SF minimum, which would require at least 19 parking spaces. ➤ 4/1,000 SF maximum, which would allow not more than 37 parking spaces. <p>Parking Proposed:</p> <ul style="list-style-type: none"> ➤ 28 parking spaces total, meeting the minimum and maximum requirements. <p>Parking spillover is already a consideration within the commercial corridor and residential areas to the east. Private commercial parking lots in the area typically clarify private parking restrictions and enforcement measures. Additionally, on-street parking in areas in the vicinity of the project site are already restricted through the Residential Parking Permit RP3 program. A map these existing zones is available here: https://www.fcgov.com/parking/pdf/all-zone-map.pdf?1560378056. Due to these factors, the parking ratio proposed by the Applicant is not anticipated to have unreasonable impacts to nearby businesses and residences.</p>	<p>Complies</p>												

3.3.2(K)(5) Handicap Parking	<p>This section requires at least two handicap spaces, including one van-accessible handicap space. Parking lots with 26-50 spaces require at least 2 handicap parking spaces.</p> <ul style="list-style-type: none"> Two handicap spaces are proposed in accordance with the standard. 	Complies
3.3.2(L) Parking Stall Dimensions	<p>This section describes minimum dimensions for off-street parking areas, including short-term commercial parking stalls and drive aisle widths. All parking lot metrics are met.</p>	Complies
3.2.4 Site Lighting	<ul style="list-style-type: none"> A photometric plan has been submitted and reviewed for the project. All parking lot and exterior building lighting is provided by down-directional and sharp cut-off fixtures. As proposed, the project complies with the photometric light levels and lighting design standards in Section 3.2.4. 	Complies
3.2.5 Trash and Recycling Enclosures	<ul style="list-style-type: none"> The project provides a fully screened trash enclosure with walk-in access to recycling and waste containers in accordance with the requirements of this section. Masonry walls are proposed for enclosure using a running bond brick pattern which is the same detail used as the primary masonry on the bank building. Container sizes proposed have been reviewed by staff and are adequate to meet the needs of the commercial uses. A concrete service pad is provided to allow rollout of the containers. 	Complies

B. DIVISION 3.3 – ENGINEERING STANDARDS

Applicable Code Standard	Summary of Code Requirement and Analysis	Staff Findings
<p>3.3.1(C) – Public Sites, Reservations and Dedications</p>	<p>The Applicant is required to dedicate drainage and utility easements as needed to serve the area being developed. In cases where any part of an existing road is abutting or within the tract being developed, the applicant must dedicate such additional rights-of-way as may be necessary to increase such roadway to the minimum width required by Larimer County Urban Area Street Standards and the City of Fort Collins Land Use Code.</p> <ul style="list-style-type: none"> Additional right-of-way is required with the project to meet city standards along the S. College Avenue frontage in order to accommodate a north-bound right turn lane. The project was reviewed and the PDP approved by CDOT who has jurisdiction over South College Avenue in accordance with their access control plan. The proposed plat dedicates all necessary easements as required by the City's Engineering Services department. 	Complies

C. 3.4.7 – HISTORIC AND CULTURAL RESOURCES

Applicable Code Standard	Summary of Code Requirement and Analysis	Staff Findings
<p>Section 3.4.7 Historic and Cultural Resources</p>	<p>The purpose of this Section is to ensure that proposed development compatible with and protects historic resources.</p> <p>Relevant to this code section, the proposed Alpine Bank project includes:</p> <p>1) The existing building located at 1610 S. College Avenue was constructed in 1928 and is an historic resource. This building is proposed to be moved to the south portion of the property and maintained for a commercial use. The adaptive reuse, rehabilitation, and relocation of this 1928 Craftsman building must meet the Secretary of the Interior’s Standards for Rehabilitation of Historic Properties.</p> <p>2) The construction of the new bank building must comply with the design compatibility standards in 3.4.7 (E) Table 1 due to the adjacency with the 1928 Craftsman building. These standards guide design of new construction in a manner that ensures comfortable infill alongside existing historic buildings.</p> <ul style="list-style-type: none"> • As provided in Land Use Code Section 3.4.7(F), in its consideration of the approval of plans for properties containing or adjacent to designated, eligible or potentially eligible sites, structure, objects or districts, the Decision Maker shall receive, and consider in making its decision, a written recommendation from the Landmark Preservation Commission. <p>At its February 17, 2021 meeting, with a vote of 7-0, the Landmark Preservation Commission recommended approval for this project based on the following findings:</p> <p>A. The details in the proposal to rehabilitate and move the historic 1928 Craftsman building to a new site on an improved foundation comply with the Secretary of Interior’s Standards for Rehabilitation.</p> <p>B. While the finding that relocating the building does meet the Secretary of the Interior’s Standards in this case, due to the changing site and street improvement conditions, the Commission also noted that relocation of the building is sufficiently supported by satisfaction of three of the four criteria for modification of standards in Division 2.8 of the land use code, relative to the fact that moving the building changes the historic location and setting of the residence and the typical requirements of Section 3.4.7 of the code. The Commission agreed that meeting several of the modification criteria provides further support for a decision to relocate the building.</p> <p>C. The design of the new bank building complies with all six of the design compatibility standards contained in Land Use Code section 3.4.7 (E), Table 1.</p> <p>Post-hearing, should the project be approved, Historic Preservation staff will continue to work with the applicant to prepare for and successfully execute the relocation of the 1928 Craftsman building to the new location on the site that will provide an appropriate long-term setting. Staff will also continue to assist the applicant with an interpretive signage plan that will indicate the building has been relocated, in a manner similar to the relocation and interpretation of the Butterfly Café building in front of 222 Laporte Avenue (relocated to accommodate new construction). Documentation of these final details will be captured in the Plan of Protection for the historic building.</p>	<p>Complies</p>

D. 3.5 – BUILDING STANDARDS

The purpose of this Section is to ensure that the physical and operational characteristics of proposed buildings and uses are compatible when considered within the context of the surrounding area. More specific or stringent standards are addressed with the TOD requirements.

Applicable Code Standard	Summary of Code Requirement and Analysis	Staff Findings
<p>3.5.1(B)(C)(D)(E)(F)(G)(H) – Building Project and Compatibility</p> <p>3.5.3 – Mixed-Use, Institutional and Commercial Buildings</p>	<p>These standards are designed to ensure compatibility of new buildings with the surrounding context. Nonresidential buildings must provide significant architectural interest and shall not have a single, large, dominant building mass. The street level shall be designed to comport with a pedestrian scale in order to establish attractive street fronts and walkways. Buildings shall be designed with predominant materials, elements, features, color range and activity areas tailored specifically to the site and its context.</p> <p>The building will continue to set an enhanced standard of quality as anticipated with the Midtown Subarea plan with the combination of the following features:</p> <ul style="list-style-type: none"> • Appropriate application of masonry, lap and shake siding materials on all four sides of the building; • A porch feature is provided along the east, south and north sides of the building providing a massing transition, pedestrian scale and interest; • Entrance features, window and porch detailing, material accents and storefront transparency which are appropriately detailed to a human scale. 	<p>Complies</p>
<p>3.5.3(C)(1) – Orientation to a Connecting Walkway</p>	<p>Direct walkway connections are provided per this standard from the street sidewalk to the main entries for each building.</p>	<p>Complies</p>
<p>3.5.3(C)(2) – Orientation to Build to Lines for Streetfront Buildings</p>	<p>This standard requires a build-to-range of at least 10 feet and not more than 25 feet from the street right-of-way, with no vehicle use areas between the building and the street. The proposed building is set back 14.9 feet in accordance with this standard.</p>	<p>Complies</p>

E. 3.6 TRANSPORTATION AND CIRCULATION

This Section is intended to ensure that the transportation network of streets, alleys, roadways and trails is in conformance with adopted transportation plans and policies established by the City.

Applicable Code Standard	Summary of Code Requirement and Analysis	Staff Findings
<p>3.6.4 – Transportation Level of Service Requirements</p>	<ul style="list-style-type: none"> • Traffic Operations and Engineering Departments have reviewed the plan's Transportation Impact Study (TIS) and determined pedestrian and bicycle facilities proposed are consistent with the City of Fort Collins Multi-Modal Transportation Level of Service Manual. • Per the TIS, no street intersection or lane improvements are required in the area to accommodate the vehicle traffic generated by the development. The development does allow for the new right-turn lane along the College frontage to better accommodate existing traffic volumes. 	<p>Complies</p>

<p>3.6.6 – Emergency Access</p>	<p>This section is intended to ensure that emergency vehicles can gain access to, and maneuver within, the project so that emergency personnel can provide fire protection and emergency services without delays.</p> <ul style="list-style-type: none"> A fire lane access drive is proposed across the site to accommodate emergency access as shown on the proposed plat. 	<p>Complies</p>
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F. 3.7 COMPACT URBAN GROWTH

Applicable Code Standard	Summary of Code Requirement and Analysis	Staff Findings
<p>3.7.3 – Adequate Public Facilities</p>	<p>This section requires that any approval of a development is conditioned on the provision of all services necessary to serve the new development. This includes transportation, water, wastewater, storm drainage, fire and emergency services, electrical power and any other public facilities and services as required.</p> <ul style="list-style-type: none"> The project is located in the City’s service area for water, wastewater and electric utilities. Utilities staff have commented on the project and have concluded that existing infrastructure is capable of serving the proposed project. 	<p>Complies</p>

G. 3.10 DEVELOPMENT STANDARDS FOR THE TRANSIT-ORIENTED DEVELOPMENT (TOD) OVERLAY ZONE

The purpose of this Section is to modify the underlying zone districts south of Prospect Road to encourage land uses, densities and design that enhance and support transit stations along the Mason Corridor.

Applicable Code Standard	Summary of Code Requirement and Analysis	Staff Findings
<p>3.10.3 Site Planning</p>	<p>Section 3.10.3(A) Building Orientation. This section requires that the primary commercial entrances shall face streets, connecting walkways, plazas, parks or similar outdoor spaces, but not parking lots.</p> <ul style="list-style-type: none"> The primary entrances face S. College Avenue, meeting the standard. <p>Section 3.10.3(B) Central Feature or Gathering Place. Per this requirement, at least one prominent or central location within each transit station area shall include a convenient outdoor open space or plaza with amenities such as benches, monuments, kiosks or public art. This feature and its amenities shall be placed adjacent to a transit station, to the extent reasonably feasible.</p> <ul style="list-style-type: none"> Staff’s interpretation is that this requirement would not apply to the project but is addressed with the following requirement: <p>Section 3.10.3(C) Outdoor Spaces. To the extent reasonably feasible, buildings and extensions of buildings shall be designed to form outdoor spaces such as courtyards, plazas, arcades, terraces, balconies and decks for residents' and workers' use and interaction, and to integrate the development with the adjacent physical context. To the extent reasonably feasible, a continuous walkway system linking such outdoor spaces shall be developed, and shall include coordinated linkages between separate developments.</p>	<p>Complies</p>

	<ul style="list-style-type: none"> The project complies with this standard with the plaza and walkway space provided along the south side of the Alpine Bank building. Planting areas around the plaza provide visual interest, shade, and separation from the street. Raised planters and seat walls provide vertical elements which help provide further separation and definition of the space. 	
<p>3.10.4 Streetscape and Pedestrian Connections:</p> <p>3.10.4(A) Streetscape</p>	<p>3.10.4(A) Streetscape. Developments shall provide formal streetscape improvements which shall include sidewalks having street trees in sidewalk cutouts with tree grates, planters or other appropriate treatment for the protection of pedestrians.</p> <ul style="list-style-type: none"> A 10-foot sidewalk is required along the S. College Avenue frontage. Street trees are provided within an 8-foot parkway. Tree grates are not recommended. Streetscape enhancements provided along the building face include a raised porch, a raised brick planter and building foundation landscaping. 	<p>Complies</p>
<p>3.10.4 Streetscape and Pedestrian Connections:</p> <p>3.10.4(C) On-street Parking</p>	<p>3.10.4(C) On-street Parking. This section requires that off-street parking in the TOD Overlay Zone be located behind, above, within or below street-facing buildings to the maximum extent feasible. No parking will be allowed between the street and the front or side of a building.</p> <ul style="list-style-type: none"> The parking area proposed between the buildings meets this requirement by being set back behind the face of the buildings. 	<p>Complies</p>
<p>3.10.5 Character and Image</p>	<p>3.10.5(A) Articulation. The proposed bank building complies with this standard, which requires that the building walls be subdivided and proportioned to human scale, using projections, overhangs and recesses in order to add architectural interest and variety and avoid the effect of a single, massive wall with no relation to human size.</p> <ul style="list-style-type: none"> Massing step-backs are provided at the upper floor to reduce the apparent mass of the building. The wrap-around porch feature and porch hip-roof adds visual interest and reduces the overall scale of the façade wall planes by bisecting the walls with a horizontal roof element. <p>3.10.5(B) Rooflines. A single continuous horizontal roofline shall not be used on one-story buildings. Accent roof elements or towers may be used to provide articulation of the building mass.</p> <ul style="list-style-type: none"> A combination of gable and hip roof elements are provided with the 2-story bank building, with deep overhangs and fascia brackets providing additional detail. <p>3.10.5(C) Materials and Colors. This section includes five different standards related to material quality, selection, and color. Predominant exterior materials shall be high quality materials. All facades incorporate stone, stone veneer, brick, brick veneer, stucco, corrugated metal, wood and/or equivalent accent material in a manner that highlights the articulation of the massing or the base and top of the building. Predominant or field colors for facades shall be low reflectance, subtle, neutral or earth tone colors.</p> <ul style="list-style-type: none"> All materials proposed are high quality -- with brick, siding patterns and accent features used on all four sides of the building. These materials are applied appropriately, with masonry used on portions of the lower floor and different 	<p>Complies</p>

	<p>siding patterns used on portions of first and second floor to articulate the base and top of the building.</p> <p>3.10.5 (F)(1) Building Height (increasing the allowable height). This section is not applicable to the project.</p> <p>3.10.5 (F)(2) Building Height. This section is not applicable and requires that buildings greater than two stories in height be designed so that upper portions of the building are stepped back from the base.</p> <p>3.10.5(G) Windows. This standard requires that storefront window and door systems may be used as the predominant style of fenestration for nonresidential or mixed-use buildings as long as the building facade visually establishes and defines the building stories and establishes human scale and proportion. Minimum glazing on pedestrian-oriented facades of buildings shall be sixty (60) percent on the ground floor and forty (40) percent on upper floors. Projects functionally unable to comply with this requirement shall mitigate such noncompliance with ample, enhanced architectural features such as a change in massing or materials, enhanced landscaping, trellises, arcades or shallow display window cases.</p> <ul style="list-style-type: none">• Glazing along the street facades may not meet the transparency requirement. The window pattern and detailing on the building does provide appropriate human scale and proportion for the building and relates to the historic building to the south. The porch detail provides an enhanced architectural feature which provides alternative transparency and massing mitigation, meeting the standard.	
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6. Article 4 – Applicable Standards:

A. DIVISION 4.21 – GENERAL COMMERCIAL (C-G)

The General Commercial District is intended to be a setting for development, redevelopment and infill of a wide range of community and regional retail uses, offices and personal and business services. Secondly, it can accommodate a wide range of other uses including creative forms of housing.

While some General Commercial District areas may continue to meet the need for auto-related and other auto-oriented uses, it is the City's intent that the General Commercial District emphasize safe and convenient personal mobility in many forms, with planning and design that accommodates pedestrians.

Applicable Code Standard	Summary of Code Requirement and Analysis	Staff Findings
4.21(B)(2) – Permitted Uses	The proposed “offices and financial services” land use is a permitted use subject to Type 1 review.	Complies
4.21(D) – Land Use Standards	The maximum building height permitted within this district is 4 stories, and the project proposes a maximum height of two stories.	Complies

7. Findings of Fact/Conclusion

In evaluating the request for the Alpine Bank Project Development Plan, PDP200020, staff makes the following findings of fact:

- The Project Development Plan complies with process located in Division 2.2 – Common Development Review Procedures for Development Applications of Article 2 – Administration.
- The Modification of Standard to 3.2.2(J) *Setbacks* is not detrimental to the public good and is justified by the applicable standards in 2.8.2(H)(3) and 2.8.2(H)(4) as described in the staff findings for this modification on pages 8 and 9 of this Staff Report.
- The Modification of Standard to *Section 3.4.7 Historic and Cultural Resources* to allow moving the historic resource currently located at 1610 S. College Avenue is not detrimental to the public good and is justified by the applicable standards in 2.8.2(H)(1), 2.8.2(H)(3) and 2.8.2(H)(4) as described in the staff findings for this modification on pages 10 and 11 of this Staff Report.
- At its February 17, 2021 meeting, with a vote of 7-0, the Landmark Preservation Commission recommended approval for this Project Development Plan based on their Findings of Fact included with this Staff Report.
- The Project Development Plan complies with the relevant standards located in Article 3 – General Development Standards, provided that the two Modifications of Standard are approved, and subject to one Condition of Approval.
- The Project Development Plan complies with the relevant standards located in Division 4.21, General Commercial (C-G) of Article 4.

8. Recommendation

Staff recommends approval of the Modifications of Standards to *Section 3.2.2(J) Setbacks* and *Section 3.4.7 Historic and Cultural Resources* and approval of the Alpine Bank Project Development Plan, PDP 200020 based on the Findings of Fact in this Staff Report, with the following condition:

- 1) The parking setback for the three parking spaces located in the southeast corner of the site, as depicted in Exhibit A – Attachment 14 shall be widened to provide a landscaped median that is at least 5 feet wide as measured from the back of the median curbs.

9. Attachments

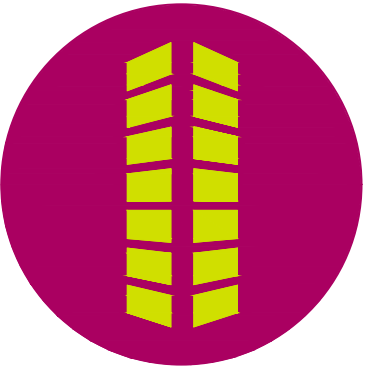
1. Zoning Map
2. Sign Posting
3. Coloradoan Notice
4. Hearing Notice Mailed Letter
5. Applicant's Planning Narrative
6. Applicant's modification request – 3.2.2(J) – Setbacks
7. Applicant's Modification request – 3.4.7 – Relocation of Historic Building at 1610 S. College Avenue
8. PDP Planning Set, 11 pages – Cover Sheet, Site Plan, Building Elevations, Landscape Plans, Lighting Plans
9. Building Material Sample Board
10. Plat
11. Utility Plans
12. Traffic Impact Study
13. Exhibit A – Condition of Approval
14. Historic Preservation Staff Report for the Landmark Preservation Commission February 17, 2021 meeting
15. Applicant's presentation for the Landmark Preservation Commission February 17, 2021 meeting

ATTACHMENT B

Condition of Approval
Alpine Bank, PDP #200020

PRELIMINARY
NOT FOR BIDDING
NOT FOR CONSTRUCTION

COPYRIGHT
THESE PLANS ARE AN INSTRUMENT OF SERVICE AND ARE THE PROPERTY OF GALLOWAY, AND MAY NOT BE DUPLICATED, DISCLOSED, OR REPRODUCED WITHOUT THE WRITTEN CONSENT OF GALLOWAY. COPYRIGHTS AND INFRINGEMENTS WILL BE ENFORCED AND PROSECUTED.



SITE PLANS FOR
ALPINE BANK
1608 SOUTH COLLEGE AVENUE
S. COLLEGE AVE. & PROSPECT ROAD
FORT COLLINS, COLORADO

#	Date	Issue / Description	Init.
1	12/22/2020	1ST PDF SUB	MAS
2	1/20/2021	2ND PDF SUB	MAS
3	02/17/2021	3RD PDF SUB	MAS

Project No:	ALB000001
Drawn By:	MRB
Checked By:	MAS
Date:	02/17/2021

SITE PLAN

SCHEDULE

- 1 EXISTING CURB AND GUTTER TO REMAIN
- 2 PROPOSED ACCESS POINT
- 3 EXISTING SIDEWALK TO REMAIN
- 4 EXISTING CONCRETE PAV TO BE REMOVED & REPLACED PER CITY STANDARDS
- 5 EXISTING PEDESTRIAN CROSSWALK TO BE MODIFIED PER CITY STANDARDS
- 6 EXISTING SITE LIGHT TO REMAIN
- 7 PROPOSED 6" CONCRETE CURB WITH 2" GUTTER; REF. SITE DETAILS
- 8 EXISTING TREE TO REMAIN (CONTRACTOR TO PROTECT IN PLACE - SEE LANDSCAPE PLANS FOR DETAILS)
- 9 EXISTING UTILITY BOX TO REMAIN AND BE RELOCATED OR ADJUSTED TO FINAL GRADE (SEE UTILITY PLAN FOR TYPE)
- 10 EXISTING MANHOLE TO REMAIN (SEE UTILITY PLAN FOR TYPE)
- 11 EXISTING INLET TO REMAIN (SEE UTILITY PLAN FOR TYPE)
- 12 EXISTING UTILITY BOX TO BE REMOVED AND RELOCATED (REF. EXISTING CONDITIONS & DEMOLITION PLAN FOR EXISTING LOCATION OF BOX)
- 13 PROPOSED FIRE HYDRANT; REF. UTILITY PLAN
- 14 PROPOSED ADA PARKING SIGN; REF. SITE DETAILS
- 15 PROPOSED 7,600 SF ALPINE BANK BUILDING; REF. ARCH. PLANS
- 16 PROPOSED BICYCLE RACK; REF. SITE DETAILS
- 17 PROPOSED OUTDOOR SEATING AREA; REF. ARCH. PLANS
- 18 PROPOSED MANHOLE; REF. UTILITY PLAN FOR TYPE
- 19 PROPOSED CONCRETE SIDEWALK
- 20 PROPOSED TRASH ENCLOSURE
- 21 PROPOSED 6" CONCRETE CURB WITH 1" GUTTER; REF. SITE DETAILS
- 22 PROPOSED FULL DEPTH ASPHALT PAVING
- 23 PROPOSED SITE LIGHT; REF. PHOTOMETRIC PLAN
- 24 PROPOSED 8'x17' 90-DEGREE "HEAD-IN" PARKING STALL (TYP.); REF. SIGNAGE & STRIPING PLAN
- 25 PROPOSED ADA ACCESSIBLE RAMP WITH DETECTABLE WARNINGS; REF. SITE DETAILS
- 26 PROPOSED ADA PARKING STALL WITH PAINTED ACCESS AISLE AND SIGNAGE; REF. SITE DETAILS
- 27 PROPOSED 8'x19' 90-DEGREE "HEAD-IN" PARKING STALL (TYP.); REF. SIGNAGE & STRIPING PLAN
- 28 PROPOSED PARKING STRIPING; REF. SIGNAGE & STRIPING PLAN
- 29 PROPOSED STOP BAR; REF. SITE DETAILS AND INTERSECTION PLAN
- 30 PROPOSED CROSSWALK; REF. SITE DETAILS AND INTERSECTION PLAN
- 31 PROPOSED R1-1 STOP SIGN WITH R3-SR "RIGHT TURN ONLY" SIGN BELOW; REF. SITE DETAILS
- 32 PROPOSED LANDSCAPE PLANTER BOX; REF. LANDSCAPE PLAN
- 33 PROPOSED MONUMENT SIGN; SEPARATE PERMIT REQUIRED
- 34 PROPOSED AREA INLET; REF. UTILITY PLAN
- 35 PROPOSED CURB INLET; REF. UTILITY PLAN
- 36 PROPOSED LANDSCAPING; REF. LANDSCAPE PLANS
- 37 EXISTING RETAINING WALL; REF. LANDSCAPE PLANS
- 38 PROPOSED SITE BENCH; REF. LANDSCAPE PLANS
- 39 PROPOSED SAN CUT LINE; REF. SITE DETAILS
- 40 PROPOSED WATER METER PIT; REF. UTILITY PLANS
- 41 EXISTING ASPHALT PAVING TO REMAIN
- 42 PROPOSED ADS UNDERGROUND WATER QUALITY SYSTEM; REF. UTILITY PLANS
- 43 PROPOSED ADA PARKING SIGN W/VAN ACCESSIBLE SIG; REF. SITE DETAILS
- 44 PROPOSED EXIT STRIPING; REF. SITE DETAILS
- 45 PROPOSED TRAFFIC DIRECTIONAL ARROW; REF. SITE DETAILS AND INTERSECTION PLAN
- 46 PROPOSED RELOCATION OF 1,100 SF HISTORIC BUILDING
- 47 PROPOSED ENCLOSED BIKE STORAGE (1 STALL)
- 48 PROPOSED CONCRETE PAVING FOR LANE WIDENING (TO MATCH EXISTING STREET PAVEMENT SECTION)
- 49 PROPOSED STAIRS; REF. ARCHITECTURAL PLANS
- 50 PROPOSED RAMP; REF. ARCHITECTURAL PLANS
- 51 PROPOSED SIDEWALK RAMP W/ HANDRAILS
- 52 PROPOSED R5-1 "DO NOT ENTER" SIGN FACING EASE WITH BACK TO BACK R6-1R/L "ONE WAY" SIGNS BELOW (ARROWS FACING PROSPECT); REF. SITE DETAILS
- 53 PROPOSED "RIGHT LANE MUST TURN RIGHT" SIGN; REF. SITE DETAILS & INTERSECTION PLAN
- 54 EXISTING CROSSWALK PUSH BUTTON PEDESTAL TO BE RELOCATED 9'; CONTRACTOR TO ADJUST TO FINAL GRADE AND CORRECT ORIENTATION PER NEW ADA RAMP LOCATION
- 55 PROPOSED CONCRETE CROSSSPAN; REF. SITE DETAILS
- 56 PROPOSED TRANSFORMER PAD; REF. UTILITY PLANS
- 57 PROPOSED TRAFFIC LIGHT TO REMAIN; REF. UTILITY PLANS
- 58 EXISTING EDGE OF ASPHALT
- 59 PROPOSED SWITCHGEAR BOX AND PAD; REF. UTILITY PLANS
- 60 PROPOSED SANITARY SEWER MANHOLE; REF. UTILITY PLAN
- 61 EXISTING "NO LEFT TURN" SIGN TO REMAIN
- 62 PROPOSED "ONE WAY" SIGN; REF. SITE DETAILS
- 63 PROPOSED ENHANCED CORNER TREATMENT; REF. BUILDING PLANS
- 64 PROPOSED SPEED LIMIT SIGN; REF. SITE DETAILS
- 65 PROPOSED HOSPITAL SIGN; REF. SITE DETAILS
- 66 PROPOSED STAMPED CONCRETE
- 67 PROPOSED HEAVY DUTY CONCRETE
- 68 PROPOSED STEPS

LEGEND

- EXISTING PROPERTY BOUNDARY LINE
- - - PROPOSED PROPERTY BOUNDARY LINE
- ADJACENT PROPERTY BOUNDARY LINE
- - - EXISTING EASEMENT BOUNDARY LINE
- - - EXISTING EASEMENT BOUNDARY LINE TO BE REMOVED
- - - PROPOSED EASEMENT BOUNDARY LINE
- - - SECTION LINE
- - - SAWCUT LINE
- - - SIGHT TRIANGLE
- - - EXISTING FENCE
- - - PEDESTRIAN/ADA PATH OF TRAVEL
- ⊙ EXISTING LIGHT POLE
- ⊞ EXISTING ELECTRICAL BOX
- ⊞ EXISTING ELECTRICAL METER/RISER
- ⊞ EXISTING ELECTRICAL VAULT
- ⊞ PROPOSED WATER VALVE
- ⊞ EXISTING FIRE HYDRANT (TO BE REMOVED)
- ⊞ EXISTING SANITARY SEWER MANHOLE
- ⊞ EXISTING GAS METER
- ⊞ EXISTING GAS METER (TO BE REMOVED)
- ⊞ EXISTING TELEPHONE MANHOLE
- ⊞ EXISTING TELEPHONE VAULT
- ⊞ EXISTING IRRIGATION METER (TO BE REMOVED)
- ⊞ EXISTING IRRIGATION COVER BOX (TO BE REMOVED)
- ⊞ EXISTING TREE TO REMAIN
- ⊞ EXISTING TREE TO BE REMOVED
- ⊞ EXISTING SIGN
- ⊞ EXISTING STORM SEWER AREA INLET
- ⊞ EXISTING STORM SEWER CURB INLET

EASEMENT SCHEDULE

- A PROPOSED UTILITY AND PUBLIC PEDESTRIAN ACCESS EASEMENT; REF. PLAT
 - B PROPOSED EMERGENCY ACCESS EASEMENT; REF. PLAT
 - C PROPOSED DRAINAGE EASEMENT; REF. PLAT
 - D PROPOSED UTILITY EASEMENT; REF. PLAT
- NOTE: UTILITY AND PUBLIC PEDESTRIAN ACCESS EASEMENT GENERALLY FOLLOWS BACK OF PROPOSED SIDEWALK

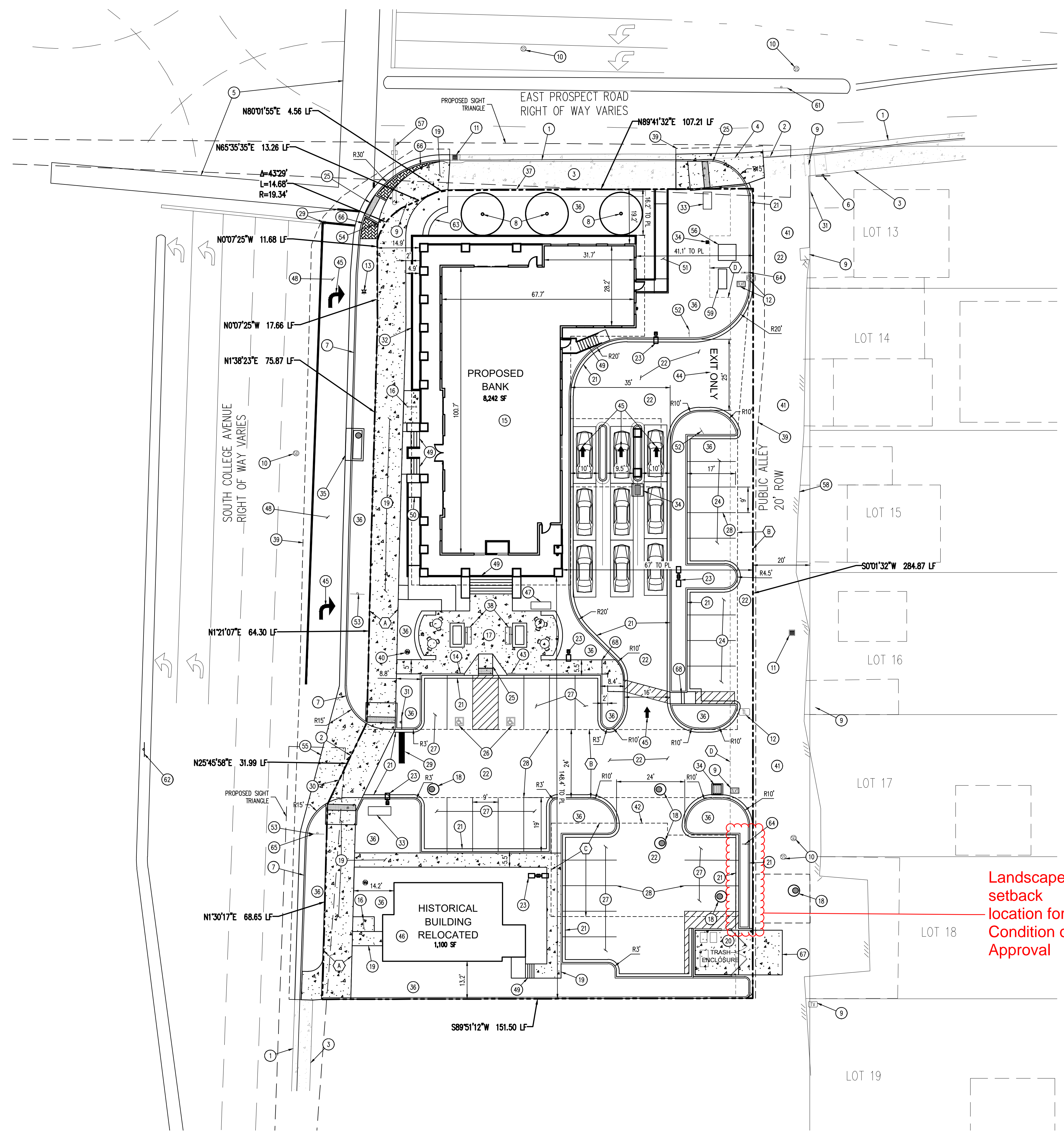
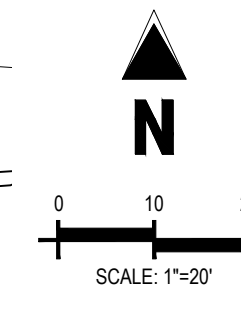
BENCHMARK

ELEVATIONS ARE BASED ON CITY OF FORT COLLINS - "WOODWARD" AND IS DESCRIBED AS FOLLOWS: NAVD 88; ELEVATION = 4992.03'

"WOODWARD" IS LOCATED & DESCRIBED AS FOLLOWS: ON THE WEST SIDE OF A CONCRETE TRAFFIC SIGNAL BASE AT THE SOUTHEAST CORNER OF COLLEGE AVE. AND PROSPECT ROAD.

BASIS OF BEARING

ALL BEARINGS ARE GRID BEARINGS OF THE COLORADO STATE PLANE COORDINATE SYSTEM, NORTH ZONE, NORTH AMERICAN DATUM 1983. THE NORTH LINE OF THE NORTHWEST QUARTER OF SECTION 24 BEARS N 89°52'03" E AND DISTANCE OF 2637.05'. MONUMENTED AT THE WEST BY NO. 6 REBAR WITH 2.5" ALUMINUM CAP IN RANGE BOX STAMPED "2017, PLS 31169 AND TO THE EAST BY NO. 6 REBAR WITH 2.5" ALUMINUM CAP IN A RANGE BOX, STAMPED "1995, PLS 17497" AS SHOWN WITH ALL OTHER BEARINGS RELATIVE THERETO.



Landscaped setback location for Condition of Approval