

# STAFF REPORT

Hearing Officer

July 10, 2017

## PROJECT NAME

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**Choice Center Filing 3 Project Development Plan – PDP 160042**

## STAFF

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Jason Holland, City Planner

## PROJECT INFORMATION

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**PROJECT DESCRIPTION:** This is a Project Development Plan (PDP) for a multi-family development at 1751 South College Avenue. The site is 0.35 acres and was formerly occupied by Discount Tire. The Discount Tire building has been demolished and the site is presently used as surface parking for the State on Campus (formerly known as the Summit) mixed use residential development adjacent to the west. The applicant for this PDP, Core Spaces, also owns and manages the State on Campus development.

The project site is located in the General Commercial (C-G) zone district and is within the Transit-Oriented Development (TOD) Overlay zone. Ten multi-family dwellings are proposed in two townhouse-style buildings. All of the units include garage parking spaces that satisfy the required off-street parking requirement. Additionally, ten guest spaces are available in the existing State on Campus parking garage to the west of the development. A Modification request is also required with the PDP to reduce the interior drive aisle width between garages from 28 feet to 24 feet. Additionally, one of the proposed units contains 5 bedrooms and an occupancy increase request is provided for this unit.

**APPLICANT:** Mark Goehausen  
Sr. Development Manager  
Core Spaces  
1643 N Milwaukee Ave, 5th Floor  
Chicago, IL 60647

**OWNER:** Core Fort Collins LLC  
2234 W North Ave.  
Chicago, IL 60647

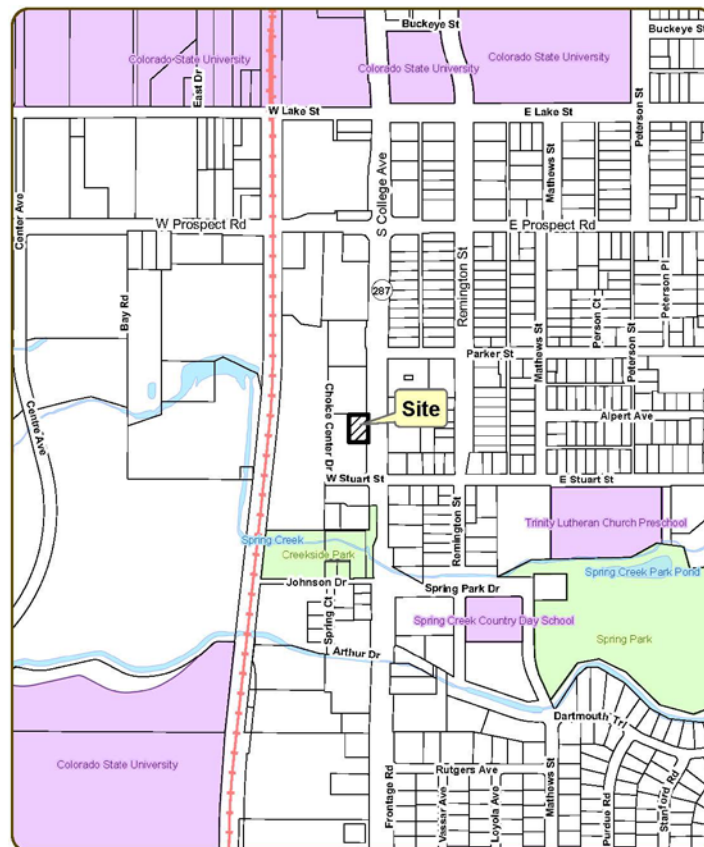
**RECOMMENDATION:** Approval

**EXECUTIVE SUMMARY**

Choice Center Filing 3 Project Development Plan complies with the applicable requirements of the City of Fort Collins Land Use Code (LUC), more specifically:

- The PDP complies with process located in Division 2.2 – Common Development Review Procedures for Development Applications of Article 2 – Administration.
- The PDP complies with relevant standards located in Division 4.21, General Commercial (C-G) of Article 4 – Districts.
- The Modification of Standard to Section 3.2.2(L)(1) to reduce the garage drive aisle width meets the applicable requirements of Section 2.8.2(H), and the granting of this Modification would not be detrimental to the public good.
- The PDP complies with relevant standards located in Article 3 – General Development Standards, provided that the Modification of Standard is approved.

**VICINITY MAP:**



**Choice Center Filing 3 Multi-family Project Development Plan**

500 250 0 500 Feet



**COMMENTS:**

**1. Background:**

The surrounding zoning and land uses are as follows:

Direction	Zone District	Existing Land Uses
North	General Commercial (C-G)	Choice Center Building 3 – commercial/retail
South	General Commercial (C-G)	Choice Center Building 2 – mixed use building with multi-family residential and commercial/retail (This is a student-oriented housing development formerly known as “The Summit”, now called “The State on Campus”)
East	General Commercial (C-G)	Commercial uses
West	General Commercial (C-G)	Choice Center Building 1 – The State on Campus multi-family residential

Land Use History:

The property was annexed into the City of Fort Collins with the South College Consolidated Annexation in December 1957.

The property is currently addressed as 1751 South College Avenue and had operated as a *retail store with vehicle servicing* owned by Discount Tire. In April of 2013 the roof of the Discount Tire collapsed due to snow, and the building was later demolished. The property was sold to the owners of the Choice Center multi-family student housing development adjacent to the property to the south and west, formerly known as “The Summit”, now called “The State on Campus.”

**2. Compliance with Applicable General Commercial (C-G) District Standards:**

The project is in compliance with all applicable General Commercial District standards with the following relevant comments provided. Language in this staff report that is taken directly from the Land Use Code (LUC) is shown in *italics*, with certain relevant elements underlined for emphasis.

**A. Section 4.21(B) – Permitted Uses**

The proposed multi-family residential land use is permitted in the General Commercial District. Multi-family developments with less than 50 units or 75 bedrooms are subject to a Type 1 administrative review and public hearing.

**B. Section 4.21(D) Land Use Standards**

This section states that *the maximum building height shall be four (4) stories*. The project proposes two story buildings in compliance with this standard.

**C. Section 4.21(E)(2) Development Standards –Site Design**

This Section states that:

*(a) Pedestrian-oriented outdoor spaces shall be placed next to activity areas that generate the users (such as street corners, shops, stores, offices, day care and dwellings). Because liveliness created by the presence of people is the main key to the attractiveness of such spaces, to the maximum extent feasible, the development shall link outdoor spaces to and make them visible from streets and sidewalks. Sculpture, kiosks or shelters are encouraged to be prominently placed in outdoor spaces.*

*(b) In multiple-building developments, outdoor spaces and landscaped areas shall be integral to an open space system in conjunction with streets and connections, and not merely residual areas left over after buildings and parking lots are sited.*

**Staff Analysis:**

The PDP provides a new widened 10' sidewalk and 7' tree lawn along the College Avenue frontage that links the development to surrounding activity areas. A pedestrian connection is also provided to the north of the site which links the development to the State on Campus amenities and parking garage to the west. The development also provides attractive dwelling entrances with brick planters and 10' of landscape depth along the College Avenue frontage. This configuration is an adequate contribution to satisfy the site design standards of Section 4.21(E)(2). Given the small infill nature of the project, a requirement for additional outdoor public space is not recommended. Additional outdoor space would be recommended with larger developments or developments that envision uses that would support adjacent activity areas and outdoor spaces.

**3. Compliance with Article 3 of the Land Use Code – General Development Standards**

The project remains in compliance with all applicable General Development Standards with the following relevant comments provided:

**A. Section – 3.2.1 Landscaping**

Street trees are provided at approximately 40-foot intervals along College Avenue and the connecting walkway to the west of Building B in accordance with the standards of this section.

All building frontages, sidewalks and parking areas are adequately landscaped in accordance with the minimum requirements, with deciduous shrubs, evergreen shrubs and perennials provided with appropriate plant spacing. The PDP shows off-site landscaping to the north and west which is required along with the reconfiguration of drive aisles, curb/gutter and walkways in these areas. The developer is required to submit Minor Amendments to these areas to obtain approval for the reconfiguration of these areas prior to the final approval of this PDP.

**B. Section 3.2.2 – Access, Circulation and Parking**

Parking quantities for the proposed multi-family use are provided in excess of what is required in the TOD zone. Parking is provided on a per-bedroom basis. Multi-family dwellings and mixed-use dwellings within the Transit-Oriented Development (TOD) Overlay Zone shall provide a minimum number of parking spaces as shown in the following table:

Number of Bedrooms/Dwelling Unit	Number of dwellings proposed	Required TOD Parking Ratio Per Dwelling Unit	Required Parking Spaces Per Dwelling Unit / required per unit totals proposed	Parking Provided per unit
One or less	1	0.75	0.75	1
Two	1	1	1	2
Three	7	1.25	8.75	14
Four and above	1	1.5	1.5	3
<b>Total</b>	<b>10</b>		<b>12</b>	<b>18</b>

Additionally, because all of the parking provided is in enclosed garage spaces and cannot be easily shared between tenants or accessed by guests, the applicant has agreed to provide ten additional guest parking spaces within the State on Campus parking garage to the west.

Bicycle parking is provided in accordance with the standards that require one bicycle space per bed. A total of 29 beds are proposed and 30 bicycle spaces are provided. 60% of the spaces are required to be interior spaces and the plans provide alcoves within the garages of each unit with bicycle racks to satisfy this requirement. Sheet 7 of 9 of the PDP plans show a detail of the garage spaces and bike accommodations. 12 additional spaces are located outside on the south side of Building A for guests and occasional use.

In conformance with the *Purpose, General Standard and Development Standards* described in this section, staff finds that the parking and circulation system provided with the project is adequately designed with regard to safety, efficiency and convenience for vehicles, bicycles, pedestrians and transit. A ten foot wide sidewalk is provided along the College frontage and an east/west sidewalk and enhanced crossing is provided to the north of the development which links the development and College Avenue sidewalk to the State on Campus and other nearby destinations. The drive aisle configuration and site access has been reviewed and accepted by Poudre Fire Authority and the proposed plat provides the necessary emergency access easements.

A Modification request is required with the PDP to reduce the drive aisle width between the proposed buildings from 28 feet to 24 feet. This is discussed later in the staff report.

**C. Section 3.2.4 – Site Lighting**

A photometric plan is provided with the project that complies with the lighting levels and design standards of this section. Building and drive aisle lighting is provided by down-directional and sharp cut-off fixtures.

***D. Section 3.2.5 – Trash and Recycling Enclosures***

The project provides a fully screened trash enclosure with walk-in access to recycling and waste containers in accordance with the requirements of this section. The enclosure is finished with brick which provides a high quality appearance consistent with the building design.

***E. Section 3.5.1 Building and Project Compatibility; 3.5.2 Residential Building Standards; 3.8.30 Multi-family Dwelling Development Standards; Division 3.10 Development Standards for the TOD Overlay Zone***

The PDP proposes building elevations for the two multi-family buildings that meet or exceed the design and articulation requirements of these sections. The building design provides variation in massing, juxtaposed materials and forms, and varied patterns of recesses and projections that provide vertical and horizontal interest, breaking down the overall scale of the buildings.

Unit entrances face streets and walkways, with entrance canopies used to define the individual unit entrances. Unique, varied door designs are incorporated into the unit entrances to provide additional visual interest and building variation. The use of materials and patterns is balanced with an appropriate proportion of brick used on the facades that have prominent public views. Colors and textures used are appealing and help emphasize and articulate overall building forms.

Garage doors include windows to provide more detail. This is intended to help enhance the appearance of the garage areas where portions of the facade may be seen from College Avenue.

In compliance with 3.10.5(C) TOD Character and Image – Materials and Colors, brick masonry and cast stone accents provide a high quality appearance around all four sides of the buildings. The north and south elevations provide different material patterns, with additional masonry used with the north elevation which will be visible from South College Avenue. A base course of brick and cast stone is used along the interior facing building facades adjacent to the garage doors, with masonry wrapping the ends of the interior facades to provide a cohesive, four-sided masonry design.

A fiber cement panel system supplements the masonry pattern used on the building facades that are visible from the street, with the cement panels used more extensively on the interior-facing garage facades. An appropriately scaled reveal pattern is incorporated into the cement panels with a ½ inch spacing to provide a noticeable shadow line with the reveals.

In compliance with 3.10.5(E) TOD Character and Image – Walls Fences and Planters, brick and cast stone planters are used along the College Avenue frontage to help provide appropriately-scaled detail consistent with the TOD standards that encourage the use of planters that match the building façade.

In compliance with 3.5.2(B) Residential Building Standards – General Standard, staff finds that the proposed single-family detached building design continues to meet the standards of this section by providing adequate architectural articulation and variation, with distinctive

building entrances that are oriented towards the public streets in accordance with the general standard.

In compliance with 3.5.2(D) Residential Building Standards – Relationship of Dwellings to Streets and Parking, all of the dwellings are oriented towards a street or *connecting walkway* in accordance with the standard.

In compliance with Section 3.8.30(E)(3) – Minimum building setbacks for multi-family dwellings, the PDP proposes a reduced arterial building setback along South College Avenue from 15 feet to 10 feet. This is permitted provided that one of the five following criteria is met.

Staff recommends the approval of the reduced setback based on the plan's compliance with exception #2 below. The project provides extensive landscaping and tree quantities beyond the minimum requirements, with low brick planter walls and appropriate design details to provide pedestrian interest, comfort and visual continuity in compliance with the standard. Additionally, the reduced setback from 15 to 10 feet is reasonable given the lower, two-story height of the proposed buildings and the wide 10' detached sidewalk provided along the College Avenue frontage, which provides outdoor public space and additional setback depth from the street to the building face. Additionally, the proposed building, with the 10 foot building setback behind a proposed 10' sidewalk, is set back farther than the 4 story State on Campus building directly to the south, in which the building face is located approximately 3.5' behind a 7' sidewalk.

*Minimum setback from the right-of-way along an arterial street shall be fifteen (15) feet and along a non-arterial street shall be nine (9) feet.*

*(a) Exceptions to the setback standards are permitted if one (1) of the following is met:*

*1. Each unit side that faces the street has a porch and/or balcony that has a minimum depth of six (6) feet (as measured from the building facade to the far side posts, railings/spindles) and a minimum length of eight (8) feet. If more than one (1) side of a unit faces the street, then only one (1) side is required to comply.*

*2. An outdoor space such as a plaza, courtyard, patio or garden is located between a building and the sidewalk, provided that such space shall have landscaping, low walls, fencing or railings, a tree canopy and/or other similar site improvements along the sidewalk designed for pedestrian interest, comfort and visual continuity.*

*3. All ground units that face a street are ADA compliant units that have street-facing porches that are directly and individually accessed from the public sidewalk by a connecting walkway that is at least six (6) feet in width.*

*4. All ground units that face a street with a transit stop that fronts the building are affordable housing units, each having a street-facing stoop that directly accesses the public sidewalk by a connecting walkway.*

*5. A project is within an area in the Downtown that is designated in the Downtown Plan as allowing "main street storefront" buildings with zero or minimal setback.*

**F. Section 3.8.16 – Occupancy Limits; Increasing the Number of Persons Allowed**

This requirement is triggered by the 5-bedroom unit proposed.

This section limits the maximum occupancy allowed per dwelling unit in a single-family, two-family or multi-family dwelling to either:

- (1) one (1) family as defined in Section 5.1.2 and not more than one (1) additional person; or
- (2) two (2) adults and their dependents, if any, and not more than one (1) additional person.

The above limitation requires that any units with more than 3 bedrooms address an increase in occupancy based on the following code provision (underlined for emphasis):

*Increasing the Occupancy Limit.*

*With respect to multiple-family dwellings, the decision maker (depending on the type of review, Type 1 or Type 2) may, upon receipt of a written request from the applicant and upon a finding that all applicable criteria of this Code have been satisfied, increase the number of unrelated persons who may reside in individual dwelling units. The decision maker shall not increase said number unless satisfied that the applicant has provided sufficient additional amenities, either public or private, to sustain the activities associated with multi-family residential development, to adequately serve the occupants of the development and to protect the adjacent neighborhood. Such amenities may include, without limitation, passive open space, buffer yards, on-site management, recreational areas, plazas, courtyards, outdoor cafes, limited mixed-use restaurants, parking areas, sidewalks, bikeways, bus shelters, shuttle services or other facilities and services.*

The applicant's justification for increasing the occupancy of the residential unit includes the following summary. The applicant has provided a formal request for increased occupancy, with the full narrative and justification attached to this staff report.

On site manager:

The adjacent State on Campus student housing development operated by the Applicant features 24-hour, on-site management services which will extend to the Phase 2 development. This allows residents to have noise, maintenance, or safety concerns addressed at any time of day.

Safe Pedestrian Spaces:

The streetscape along South College Avenue will add to the pedestrian experience by providing street trees, built-up planters, grass parkway, lighted bollards, and convenient bike parking.

Recreation & Contextual Amenities:

Located immediately southeast of Colorado State University's campus, residents can easily access CSU's numerous recreation facilities. The proposed PDP is adjacent to numerous restaurants and retail stores along South College Avenue as well as Spring Park to the south east of the project site.

Recreational facilities will also be made available to residents of the PDP at the adjacent State on Campus student housing development. These include the use of exterior landscape courtyards, pool, hot tub, fire pit seating areas, climbing boulders, and volleyball courts.



Interior study lounges, media lounge, fitness rooms, and game rooms will also be made available to the PDP residents.

**Staff Recommendation:**

In order to increase the number of unrelated persons who may reside in individual dwelling units, the applicant must provide sufficient additional amenities to sustain the activities associated with the project, adequately serve the occupants, and protect the adjacent neighborhood.

Staff is supportive of the mitigation efforts outlined in the applicant's request. In this case there is only one 5-bedroom unit and the size of the project is relatively small. The standards of this section are addressed through the provision of nearby amenities and management available at the State on Campus development and through the design of the 5-bedroom unit, which provides rooftop amenity space above the 5-bedroom unit in addition to adequate living space and bicycle storage facilities within the unit.

**4. Modification of Standard Request to LUC Section 3.2.2(L)(1) -- Driveway Width**

**Land Use Code Standard proposed to be modified:**

Driveway widths for all two-way drive aisles are required to be at least 24 feet in width in accordance with *Table A* of this code section, and in cases where garages are located along both sides of a drive aisle, this code section states:

*When garages are located along a driveway and are opposite other garages or buildings, the driveway width must be increased to 28 feet.*

**Request for Modification.** The applicant requests a modification to allow the drive aisle between the garages to be reduced from 28 feet to 24 feet.

**Land Use Code Modification Criteria:**

*"The decision maker may grant a modification of standards only if it finds that the granting of the modification would not be detrimental to the public good, and that:*

*(1) the plan as submitted will promote the general purpose of the standard for which the modification is requested equally well or better than would a plan which complies with the standard for which a modification is requested; or*

*(2) the granting of a modification from the strict application of any standard would, without impairing the intent and purpose of this Land Use Code, substantially alleviate an existing, defined and described problem of city-wide concern or would result in a substantial benefit to the city by reason of the fact that the proposed project would substantially address an important community need specifically and expressly defined and described in the city's Comprehensive Plan or in an adopted policy, ordinance or resolution of the City Council, and the strict application of such a standard would render the project practically infeasible; or*

*(3) by reason of exceptional physical conditions or other extraordinary and exceptional situations, unique to such property, including, but not limited to, physical conditions such as exceptional narrowness, shallowness or topography, or physical conditions which hinder the owner's ability to install a solar energy system, the strict application of the standard sought to be modified would result in unusual and exceptional practical difficulties, or exceptional or undue hardship upon the owner of such property, provided that such difficulties or hardship are not caused by the act or omission of the applicant; or*

*(4) the plan as submitted will not diverge from the standards of the Land Use Code that are authorized by this Division to be modified except in a nominal, inconsequential way when considered from the perspective of the entire development plan, and will continue to advance the purposes of the Land Use Code as contained in Section 1.2.2.*

*Any finding made under subparagraph (1), (2), (3) or (4) above shall be supported by specific findings showing how the plan, as submitted, meets the requirements and criteria of said subparagraph (1), (2), (3) or (4).*

**Applicant's Justification:**

The applicant has provided a modification request, attached to this staff report. The Applicant requests that the modification be approved and provides the following justification for Criteria 1, 3 and 4:

*Criteria (1): the plan as submitted will promote the general purpose of the standard for which the modification is requested equally well or better than would a plan which complies with the standard for which a modification is requested.*

**Applicant's Justification for Criteria 1:**

"The Applicant has undertaken a geometric study of typical car turning radiuses to verify that the proposed 24' driveway width provides adequate vehicle maneuvering clearances between the proposed facing garages. These exhibits are attached. The results of this study indicate that a 24'-wide driveway sufficiently allows the 90 degree head-in and back-in movements of a vehicle into each garage as well as the outbound 90 degree back-out and head out geometries. Therefore, the proposed modification request will serve the residents as well as a driveway of the standard compliant width."

*Criteria (3) by reason of exceptional physical conditions or other extraordinary and exceptional situations, unique to such property, including, but not limited to, physical conditions such as exceptional narrowness, shallowness or topography, or physical conditions which hinder the owner's ability to install a solar energy system, the strict application of the standard sought to be modified would result in unusual and exceptional practical difficulties, or exceptional or undue hardship upon the owner of such property, provided that such difficulties or hardship are not caused by the act or omission of the applicant;*

**Applicant's Justification for Criteria 3:**

"As previously mentioned, the proposed site layout maintains a connection between the northern retail parking lot and College Avenue via a shared driveway, though this connection severely limits the other uses on the site. The site of the proposed residential development is

123-feet wide. The parkway buffer, multi-use path, and utility easement along College Avenue are a total width of 27-feet wide while the proposed utility easement and sidewalk at the west edge of the property are 8' wide, thus leaving 88' in the center of the property for (2) residential buildings and drive aisle. By providing a 24' wide driveway, the Applicant can then allocate 32' of site width to each residential building. The Applicant has found that a residential building depth of 32' is the minimum dimension that provides adequate interior living space and enclosed garage space for student housing residences such as this. Therefore, granting the modification request for a 24'-wide drive aisle facilitates the development of an exceptionally constrained building site.

*Criteria (4): The plan as submitted will not diverge from the standards of the Land Use Code that are authorized by this Division to be modified except in a nominal, inconsequential way when considered from the perspective of the entire development plan, and will continue to advance the purposes of the Land Use Code as contained in Section 1.2.2.*

**Applicant's Justification for Criteria 4:**

"The applicant is requesting a 15% reduction from the 28'-wide drive aisle to a proposed width of 24 feet. This 15% reduction is a nominal and inconsequential reduction to the width of the drive surface because it does not require drivers to alter their driving patterns when traveling through the development site nor does it hinder the turning geometries of residents maneuvering into or out of the garages."

"In summary, the Applicant asks that the proposed modification request be granted due to the fact that it is not detrimental to the public good, it provides a driveway that functions equally as well as the standard, it facilitates development on a tightly constrained building site, and it grants a nominal and inconsequential reduction to the drive way width."

"Finally, the proposed site layout, which is possible through utilization of the Modification Request for reduced driveway width, produces a building design that is supported by several City Plan Policies. These policies are: targeted infill and redevelopment within transit served locations (LIV 5.1), reduced visual impact of parking from primary pedestrian streets and public spaces (LIV 30.4), the creation of a more pedestrian-friendly environment through parkway buffers and generous sidewalk widths (LIV 30.4), the creation of parking structures that do not dominate the street frontage (LIV 30.5), reduced land devoted to surface parking lots (LIV 30.6), and transform through infill and Redevelopment (LIV 35.4)."

**Staff Findings for the Modification**

Staff finds that the request for a Modification of Standard to Section 3.5.2(E)(2) is justified by the applicable standards in 2.8.2(H). This is because:

- A. The granting of the Modification would not be detrimental to the public good. While the drive aisle will be narrower and potentially less convenient for some residents, the functional use of the garages spaces remains viable and guest spaces are provided in the State on Campus parking garage for visitors. Additionally the size of the project is relatively small, urban in character and will be marketed to students. The potential impacts to residents of the project do not appear to be significant, as compared to a larger project where a similar garage configuration could be more impactful to the viability of the project.

- B. Criteria 1 (2.8.2(H)(1): *The plan as submitted will promote the general purpose of the standard for which the modification is requested equally well or better than would a plan which complies with the standard for which a modification is requested.*

While staff is supportive of this modification, staff finds that the reduced drive aisle width does not satisfy this criterion. The 28 foot standard drive aisle is intended to provide more maneuverability so that there is less margin of error when entering and exiting the garage spaces, allowing the configuration of garages to be safe and convenient. With this purpose in mind, staff's opinion is that 24 feet cannot be found to be equal to or better than 28 feet in terms of convenience.

- C. The project design satisfies Criteria 4 (2.8.2(H)(4): *The plan as submitted will not diverge from the standards of the Land Use Code that are authorized by this Division to be modified except in a nominal, inconsequential way when considered from the perspective of the entire development plan, and will continue to advance the purposes of the Land Use Code as contained in Section 1.2.2.*

Staff finds that the Modification of Standard to allow a drive aisle width is nominal and inconsequential because the drive aisle reduction is limited to 18 garage spaces on a constrained site and the reduced drive aisle allows an overall better plan with a townhome style building configuration that enhances the neighborhood scale and provides an appropriate building transition from the taller buildings in the vicinity. The narrow drive aisle, as evidenced by the turning radius templates provided by the application, will allow ingress/egress, and the potential inconvenience of the narrower drive aisle is nominal. Because of the project's urban infill location, limited scope of the modification, and nominal reduction in the function of the parking area, staff finds that the modification is nominal and inconsequential and that the plan continues to advance the purposes of the Land Use Code as contained in Section 1.2.2 including: targeted infill and redevelopment within transit served locations (LIV 5.1), reduced visual impact of parking from primary pedestrian streets and public spaces (LIV 30.4), the creation of a more pedestrian-friendly environment through parkway buffers and generous sidewalk widths (LIV 30.4), the creation of parking structures that do not dominate the street frontage (LIV 30.5), reduced land devoted to surface parking lots (LIV 30.6), and transform through infill and Redevelopment (LIV 35.4).

- D. The project design satisfies Criteria 4 (2.8.2(H)(3): *By reason of exceptional physical conditions or other extraordinary and exceptional situations, unique to such property, including, but not limited to, physical conditions such as exceptional narrowness, shallowness or topography, or physical conditions which hinder the owner's ability to install a solar energy system, the strict application of the standard sought to be modified would result in unusual and exceptional practical difficulties, or exceptional or undue hardship upon the owner of such property, provided that such difficulties or hardship are not caused by the act or omission of the applicant.*

Staff finds that the depth and size of the site is constrained, and that the relatively small size of the site is suitable for the townhome style buildings proposed. Staff concurs that due to the depth constraints and College Avenue improvements required, including the

10' sidewalk, 7' tree lawn and right-of-way dedication, this condition does represent a hardship not caused by the applicant that would justify the granting of the modification.

**5. Neighborhood Meeting**

A City neighborhood meeting was not required for this project and a meeting was not held.

**6. Findings of Fact/Conclusion**

In evaluating the Choice Center Filing 3 Project Development Plan (PDP), staff makes the following findings of fact:

- A. The PDP complies with process located in Division 2.2 – Common Development Review Procedures for Development Applications of Article 2 – Administration.
- B. The PDP complies with relevant standards located in Division 4.21, General Commercial (C-G) of Article 4 – Districts.
- C. The Modification of Standard to Section 3.5.2(E)(2) meets the applicable requirements of Section 2.8.2(H) -- Criteria (3) and (4), and the granting of this Modification would not be detrimental to the public good, because the reduced 24' drive aisle will function adequately for the intended purpose and use. As evidenced by the turning radius templates provided by the application, the reduced drive aisle will allow ingress/egress, and the potential inconvenience of the narrower drive aisle is nominal and inconsequential in accordance with Criterion 4. Additionally, the depth and size of the site is constrained and that due to the depth constraints and College Avenue improvements required, including the 10' sidewalk, 7' tree lawn and right-of-way dedication, these conditions represent a hardship not caused by the developer that would justify the granting of the modification in accordance with Criterion 3.
- D. The PDP complies with the relevant standards located in Article 3 – General Development Standards, provided that the Modification of Standard to Section 3.2.2(L)(1) is approved.

**RECOMMENDATION:**

Approval of Choice Center Filing 3 Project Development Plan – PDP160042 including the Modification of Standard to Section 3.2.2(L)(1) to reduce the garage drive aisle from 28 feet to 24 feet and request for Increase in Occupancy

**ATTACHMENTS**

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- 1. Applicant's Narrative and Planning Objectives
- 2. Applicant's Modification of Standard Request
- 3. Applicant's Increase of Occupancy Request

4. PDP plan set:
  - a. Site Plan
  - b. Landscape Plan
  - c. Building Elevations
  - d. Lighting Plan
  - e. Unit Details
5. Plat
6. Utility Plans
7. Traffic Memorandum



December 7, 2016

## **College Avenue Townhouses 1751 S. College Avenue**

### **Project Description (1.d.v)**

The Applicant, Core Spaces, is a full-service real estate development and management company who builds industry-leading assets on prime urban sites across the country. Core Spaces is proposing a townhouse development on South College Ave on an infill site presently covered by surface parking. The site is located in the General Commercial (CG) zone district, is within the Transit-Oriented Development (TOD) zone, and is contained within the Upper District of the Midtown in Motion plan for South College Ave.

The 15,447 sf (+/-) [0.35 acres] parcel is currently undeveloped and is partially surfaced with asphalt, concrete paving and gravel. The parking lot contains twenty-six (26) surface parking spaces that serve the State on Campus (formerly the Summit on College) residential development to the south and west. To the north of the site is a shopping center surface parking lot. Directly to the south is a service driveway for a garbage dumpster serving Building 2 of the State on Campus development.

The proposed infill development places ten (10) town houses on this parcel, which will be expanded to 17,332 sf (+/-) [0.40 acres]. Two blocks of five (5) townhouses are separated by an internal drive, which connects to the retail parking to the north and to South College Ave. to the south. Each townhouse is two stories tall with a rooftop terrace and contains an enclosed garage. There are nine (9) three-bedroom townhouse units with a two-car garage and one (1) two-bedroom townhouse with a one-car garage for a total of (29) dwelling units. The project is 30 ft. tall to the top of roof access hut and 27 ft. to the top of the building parapet.

The development is designed to provide student-oriented housing and site improvements in relation to the following components within Ft. Collins:

- Several blocks south of the Colorado State University campus
- Near the Spring Creek and Prospect Stations of Mason Corridor Transitway;
- Adjacent to the existing State on Campus residential development;
- Adjacent to existing commercial developments along South College Ave.

The project has been designed to tackle the small size and access constraints of this infill lot while improving the Stuart Ave. to Prospect Rd. section of South College Ave. The townhome building acts to bridge the streetwall between Building 2 of the State on Campus development and the shopping center to the north. The paved multi-use path and the parkway buffer will link and improve pedestrian movement along the west side of South College Ave. The utility easement will be landscaped to further improve the pedestrian experience along the roadway.

The buildings are sited parallel to South College Avenue (north to south) and are separated by an internal drive. This drive provides access to the private garages on each townhouse ground floor and provides connection to the retail parking to the north of the development.

Paved walkways are located on the north, west and south of the townhome buildings provide linkage to South College Ave. as well as to Choice Center Drive. Individual units are accessed from the east (from South College Ave.) and from the west (from a concrete walkway). The garage doors at each unit face the internal drive.

The building facades have been designed to express a contemporary statement merging architectural composition with attractive materials. The primary facades – facing east and west – combine dynamic planar zones of modular brick, cast stone trim pieces, prefinished fiber-cement panels with architectural composite wood siding accents at the unit entry bays. Each unit entry is further called out by a painted steel canopy. The north, south and internal drive façades feature the same material palette, but in a simplified expression. The project utilizes thermally-broken, insulated glazed windows for the residential units and panelized overhead garage doors. The roofline is activated by glass and prefinished metal-frame guardrails at the private rooftop terraces.





February 7, 2017

## **Choice Center Filing 3 Modification Request**

### **3.2.2 Access, Circulation and Parking**

#### **(L) Parking Stall Dimensions**

##### **(1) – Standard Spaces – Table A – Driveway Width**

Parking spaces for standard vehicles shall conform with the standard car dimensions shown on Table A.

#### **Reason for the Request**

The Applicant, CORE Spaces, is proposing a vertical multi-family development on South College Ave. The site is located in the General Commercial (CG) zone district, and within the Transit-Oriented Development (TOD) zone along South College Ave., just south of Prospect Road.

The proposed infill development includes ten (10) apartment units within two buildings at the east and west portions of the property. The residential buildings contain a total of (18) garage parking spaces for an average ratio of 1.8 spaces per apartment. Each of the parking spaces will be provided in individual attached garages at the ground floor of each apartment unit. Garages will be accessed from a single drive aisle located between the two apartment buildings.

The Fort Collins Land-Use Code typically requires a drive aisle of 24' for 90-degree standard parking spaces, but makes special note that "when garages are located along a driveway and are opposite other garages or buildings, the driveway width must be increased to 28 feet."

The proposed development site directly abuts the retail parking lot to the adjacent north property, and traffic from this northern parking lot currently connects to South College Avenue via a driveway on the proposed development site. If the entry and exit to College Avenue is moved to the south east corner of the development site, the proposed building layout can be arranged to maintain the connection of the northern retail parking lot to College Avenue via a shared driveway with the residential units. However, this maintained traffic connection, in combination with the relatively narrow width of the site, severely limits the potential for marketable development on the proposed site. By reducing the width of the shared driveway, the Applicant believes that a successful residential development is feasible. The Applicant is therefore proposing to reduce the driveway to the standard width of 24'.

#### **Justifications**

*The Land Use Code states that the decision-maker may grant a modification of standards only if it finds that the granting of the modification would not be detrimental to the public*

*good; and the decision-maker must also find that the Modification meets one of the following four criteria described in the LUC.*

Granting this modification reduces the driveway between the apartment buildings from 28-foot wide to 24-foot wide. Vehicles passing through the driveway from either College Avenue or to the adjacent retail complex are given a 12-foot drive aisle in either direction. This is as wide, or wider than, many typical drive lanes on major roadways (11 feet). The applicant has also undergone study to prove that the provided driveway width is sufficient for residents to enter and exit the garages (see attached). For these reasons, the modification is not detrimental to drivers utilizing the driveway to access or pass through the development site. Furthermore, the proposed development will also benefit the economic viability of the proposed neighborhood by providing student rental apartments in the immediate vicinity to multiple retail and dining establishments. This project utilizes the constrained property efficiently, and is contextually compatible with existing and proposed buildings heights in the area. Therefore, we believe granting the Modification would not be detrimental to the public good.

*(1) the plan as submitted will promote the general purpose of the standard for which the modification is requested equally well or better than would a plan which complies with the standard for which a modification is requested:*

The Applicant has undertaken a geometric study of typical car turning radiuses to verify that the proposed 24' driveway width provides adequate vehicle maneuvering clearances between the proposed facing garages. These exhibits are attached. The results of this study indicate that a 24'-wide driveway sufficiently allows the 90 degree head-in and back-in movements of a vehicle into each garage as well as the outbound 90 degree back-out and head out geometries. Therefore, the proposed modification request will serve the residents as well as a driveway of the standard compliant width.

*(3) by reason of exceptional physical conditions or other extraordinary and exceptional situations, unique to such property, including, but not limited to, physical conditions which hinder the owner's ability to install a solar energy system, the strict application of the standard sought to be modified would result in unusual and exceptional practical difficulties, or exceptional or undue hardship upon the owner of such property, provided that such difficulties or hardship are not caused by the act or omission of the applicant;*

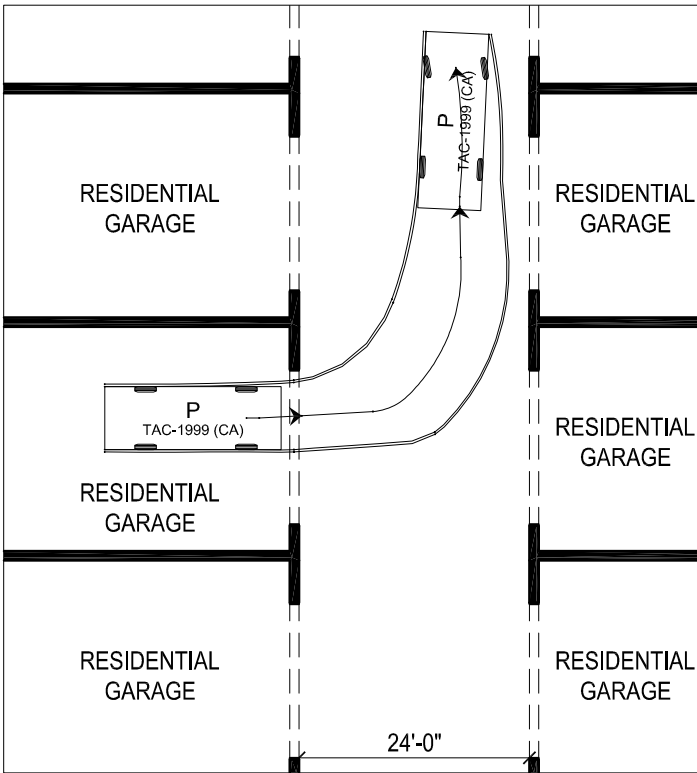
As previously mentioned, the proposed site layout maintains a connection between the northern retail parking lot and College Avenue via a shared driveway, though this connection severely limits the other uses on the site. The site of the proposed residential development is 123-foot wide. The parkway buffer, multi-use path, and utility easement along College Avenue are a total width of 27-foot wide while the proposed utility easement and sidewalk at the west edge of the property are 8' wide, thus leaving 88' in the center of the property for (2) residential buildings and drive aisle. By providing a 24' wide driveway, the Applicant can then allocate 32' of site width to each residential building. The Applicant has found that a residential building depth of 32' is the minimum dimension that provides adequate interior living space and enclosed garage space for student housing residences such as this. Therefore, granting the modification request for a 24'-wide drive aisle facilitates the development of an exceptionally constrained building site.

*(4) the plan as submitted will not diverge from the standards of the Land Use Code that are authorized by this Division to be modified except in a nominal, inconsequential way when considered from the perspective of the entire development plan, and will continue to advance the purposes of the Land Use Code as contained in Section 1.2.2.*

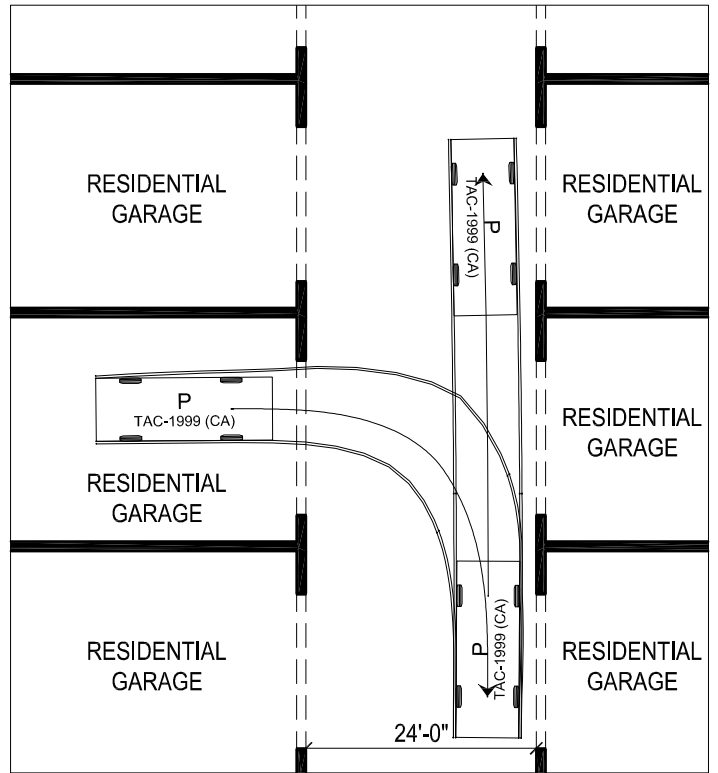
The applicant is requesting a 15% reduction from the 28'-wide drive aisle to a proposed width of 24 feet. This 15% reduction is a nominal and inconsequential reduction to the width of the drive surface because it does not require drivers to alter their driving patterns when traveling through the development site nor does it hinder the turning geometries of residents maneuvering into or out of the garages.

In summary, the Applicant asks that the proposed modification request be granted due to the fact that it is not detrimental to the public good, it provides a driveway that functions equally as well as the standard, it facilitates development on a tightly constrained building site, and it grants a nominal and inconsequential reduction to the drive way width.

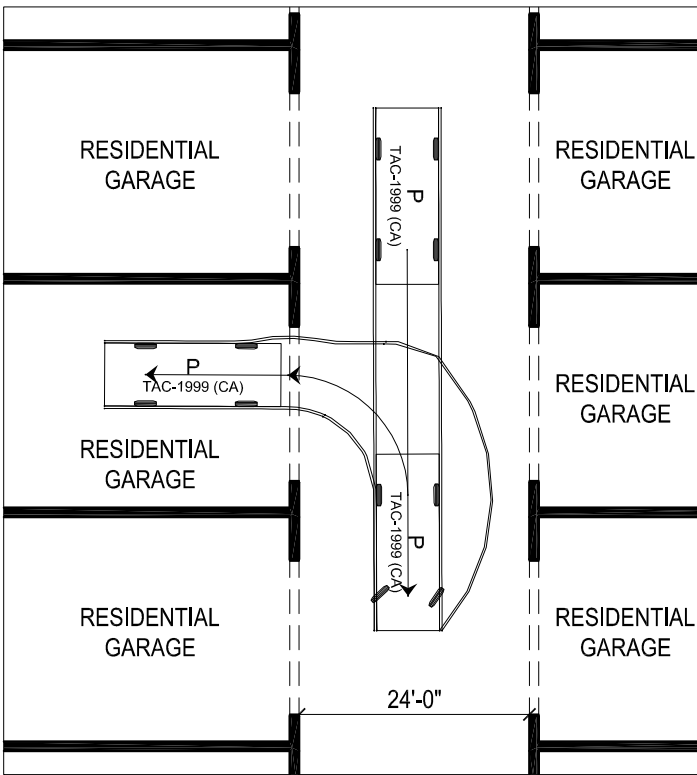
Finally, the proposed site layout, which is possible through utilization of the Modification Request for reduced driveway width, produces a building design that is supported by several City Plan Policies. These policies are: targeted infill and redevelopment within transit served locations (**LIV 5.1**), reduced visual impact of parking from primary pedestrian streets and public spaces (**LIV 30.4**), the creation of a more pedestrian-friendly environment through parkway buffers and generous sidewalk widths (**LIV 30.4**), the creation of parking structures that do not dominate the street frontage (**LIV 30.5**), reduced land devoted to surface parking lots (**LIV 30.6**), and transform through infill and Redevelopment (**LIV 35.4**).



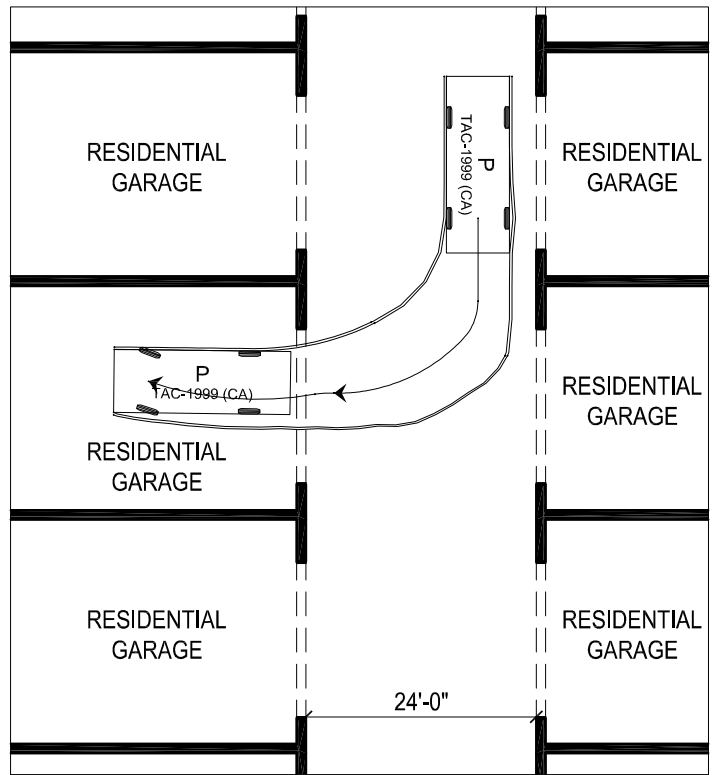
**4** OUT-BOUND: HEAD-OUT EXITING  
SCALE: 1:20



**2** OUT-BOUND: BACK-OUT EXITING  
SCALE: 1:20



**3** IN-BOUND: BACK-IN PARKING  
SCALE: 1:20



**1** IN-BOUND: HEAD-IN PARKING  
SCALE: 1:20

# CHOICE CENTER FILING 3

## VEHICLE TURNING GEOMETRY STUDY - 24' DRIVEWAY

Ft. Collins, Colorado

Core Spaces - Developer | Antunovich Associates - Architects & Planners

02.07.2017



February 8, 2017

## **Choice Center Filing 3 Request for Increased Occupancy Limit**

Core Spaces is proposing a new residential development on South College Ave on an infill site presently covered by surface parking. The 15,447 sf (+/-) [0.35 acres] parcel is currently undeveloped and is partially surfaced with asphalt, concrete paving and gravel. The proposed infill development places ten (10) apartment units on this parcel, which will be expanded to 17,332 sf (+/-) [0.40 acres]. Two blocks of five (5) residences are separated by an internal drive, which connects to the retail parking to the north and to South College Ave. to the south. Each building is two stories tall with rooftop terraces and contains an enclosed garage. The State on Campus - Phase 2 development is located directly adjacent to the State on Campus student housing facilities which are also owned and operated by the Applicant. State on Campus features many amenities (exterior landscaped courtyards, fitness facilities, study rooms, covered parking structure, etc.) which will all be made available to residents of State on Campus – Phase 2.

In the City of Fort Collins, the maximum occupancy allowed per multi-family dwelling unit is three unrelated persons, unless the decision maker increases the number of individuals allowed to reside together. In order to provide 5-bedroom units intended to be occupied by five (5) unrelated persons, the City's Land Use Code requires the Applicant to provide a written request as follows:

### **3.8.16 Occupancy Limits; Increasing the Number of Persons Allowed (E) Increasing the Occupancy Limit.**

*(2) With respect to multiple-family dwellings, the decision maker (depending on the type of review, Type 1 or Type 2) may, upon receipt of a written request from the applicant and upon a finding that all applicable criteria of this Code have been satisfied, increase the number of unrelated persons who may reside in individual dwelling units. The decision maker shall not increase said number unless satisfied that the applicant has provided sufficient additional amenities, either public or private, to sustain the activities associated with multi-family residential development, to adequately serve the occupants of the development and to protect the adjacent neighborhood. Such amenities may include, without limitation, passive open space, buffer yards, on-site management, recreational areas, plazas, courtyards, outdoor cafes, limited mixed-use restaurants, parking areas, sidewalks, bikeways, bus shelters, shuttle services or other facilities and services.*

### **Justification**

From its prior experience building student housing communities across the county, the Applicant has found that 5-bedroom units are a popular lifestyle alternative for many students. It allows the students residing in these units to share an apartment in a well-managed environment. It is more secure than most single-family home rentals and, because higher occupancy units typically rent for less per bedroom, they offer a more affordable alternative to students on a budget.

In order to increase the number of unrelated persons who may reside in individual dwelling units, the Applicant must provide sufficient additional amenities to sustain the activities associated with the project, adequately serve the occupants, and protect the adjacent neighborhood.

Justification for increasing the occupancy of certain residential units includes the following:

### **Open Space and Amenities:**

#### Security

- Replacing existing surface parking lots with new residential developments creates more pedestrian activity along the College Avenue and removes dark, inactive 'dead zones' that reduce the perceived safety of an area. New, vibrant residences attracts new residents and creates more 'eyes on the street' that strengthens the surrounding community.
- The adjacent State on Campus student housing development operated by the Applicant features 24-hour, on-site management services which will extend to the Phase 2 development. This allows residents to have noise, maintenance, or safety concerns addressed at any time of day.

#### Safe Pedestrian Spaces

- The streetscape along South College Avenue will add to the pedestrian experience by providing street trees, built-up planters, grass parkway, lighted bollards, and convenient bike parking.

#### Recreation & Contextual Amenities

- Located immediately south east of Colorado State University's campus, residents can easily access CSU's numerous recreation facilities. The phase 2 development is adjacent to numerous restaurants and retail stores along South College Avenue as well as Spring Park to the south east of the project site.
- Recreational facilities will also be made available to residents at the adjacent State on Campus student housing development. These include the use of exterior landscape courtyards, pool, hot tub, fire pit seating areas, climbing boulders, and volleyball courts. Interior study lounges, media lounge, fitness rooms, and game rooms will also be made available to Phase 2 residents.

#### Transportation

- The project is located within the Transit-Oriented Development (TOD) Overlay Zone and walking distance from the MAX Rapid Transit's Prospect Station. Parking for each residence is located within internal enclosed garages. Bicycle parking is also located within each residence as well as an exterior bike rack within the project boundary.

#### Services

- The proposed site is adequately served with standard public infrastructure including water, wastewater, police and fire facilities. The existing street network has adequate capacity to absorb the additional traffic within the level of service standards.

#### Architectural Design

- High occupancy residences have positive effects related to architectural design and sustainability. Five bedroom dwelling units are more efficient in the utilization of space, allowing more flexibility for residents.

#### Sustainability

- Locating higher-density housing for students where they can access the campus, shopping and recreational opportunities without using an automobile is a key component of being an environmentally responsible community. The proposed development will house 29 students in a location where having a vehicle is not necessary to have access to necessary amenities. This is highly desirable from an environmental perspective and makes attending college more affordable to students on a budget. Units with increased occupancy are an essential component of the proposed development that allows higher density to work in this location. Higher density housing protects the adjacent neighborhood by providing appropriate housing for students in one location, discouraging them from living in single family neighborhoods.
- Units with increased occupancy are also more efficient in terms of material usage and are also more energy efficient. Data obtained from Conservice, a nationwide utility billing service that analyzes information from many properties and thousands of units, indicates that electricity usage is an average of 21.25% more efficient when comparing a 5-bedroom unit to two 2-bedroom units. In other words, the same five people use less electricity when housed in a 4-bedroom unit vs. being split up into two 2-bedroom units. Likewise, natural gas is estimated to be 55.12% more efficient. Additionally, less construction materials translates into less environmental impact from a construction standpoint.

In summary, the Applicant requests the Hearing Officer to approve occupancy by five (5) unrelated individuals within the State on Campus – Phase 2 apartment units. We believe the 5-bedroom units will serve the students by providing an attractive, secure and affordable lifestyle alternative with sufficient amenities to sustain the activities of the student population without impacting adjacent neighbors. Adequate parking for both cars and bicycles is conveniently provided within each residence and a wide variety of shopping, dining, recreation and entertainment opportunities are located within walking distance to satisfy the student living and recreational needs.

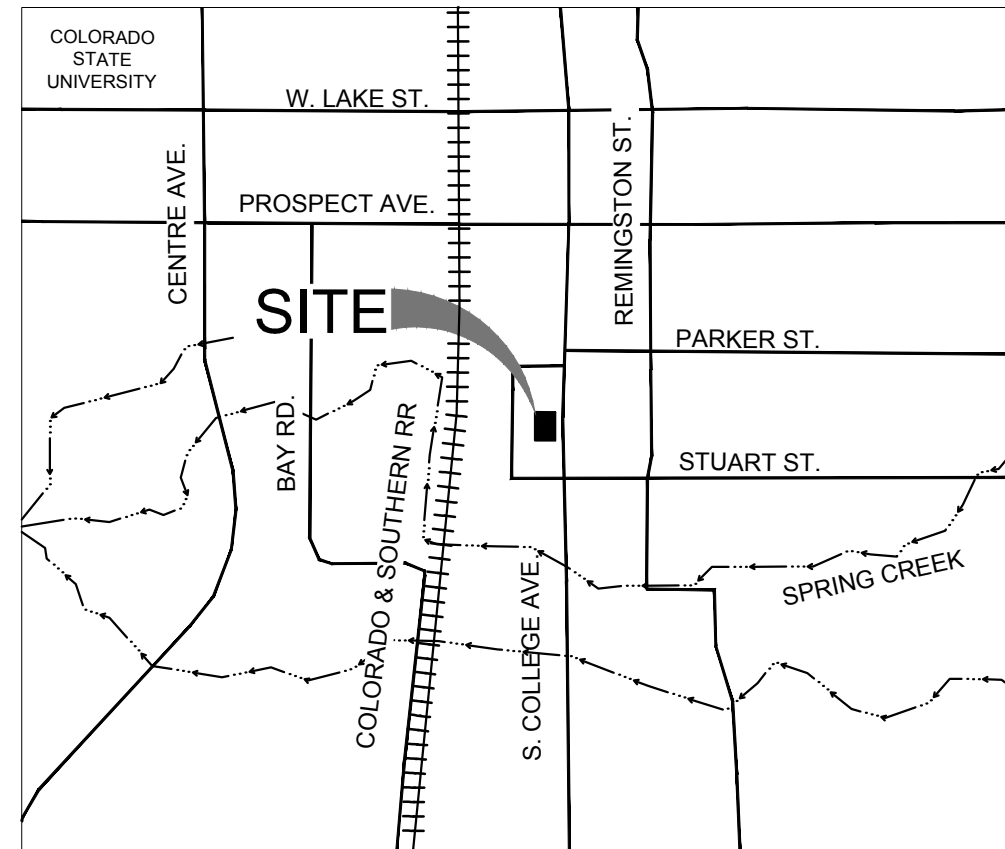
# CHOICE CENTER THIRD FILING

BEING A REPLAT OF LOT 2, CHOICE CENTER SUBDIVISION AND A PORTION OF LOT 3, FOX SHOPPING CENTER SUBDIVISION  
 LOCATED WITHIN THE NORTHEAST QUARTER (NE 1/4) OF SECTION 23, TOWNSHIP 7 NORTH, RANGE 69 WEST OF THE SIXTH PRINCIPAL MERIDIAN  
 CITY OF FORT COLLINS AND COUNTY OF LARIMER, STATE OF COLORADO.



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JUNE 2017



**VICINITY MAP**  
 1" = 1000'

**ORIGINAL FIELD SURVEY:**

RICHARD B. GABRIEL  
 POWER SURVEYING COMPANY, INC.  
 150 W. 84TH AVE.  
 THORNTON, CO. 80260  
 DATE: 11/11/2016  
 REVISED: 11/15/2016  
 REVISED: 12/14/2016  
 REVISED: 01/30/2017  
 REVISED: 02/07/2017  
 REVISED: 03/06/2017

**SUBSURFACE EXPLORATION BY:**

DAVID RICHER  
 EARTH ENGINEERING CONSULTANTS, LLC  
 4396 GREENFIELD DRIVE  
 WINDSOR, CO 80550

**DISCLAIMER STATEMENT:**

THESE PLANS HAVE BEEN REVIEWED BY THE CITY OF FORT COLLINS FOR CONCEPT ONLY. THE REVIEW DOES NOT IMPLY RESPONSIBILITY BY THE REVIEWING DEPARTMENT, THE CITY OF FORT COLLINS ENGINEER, OR THE CITY OF FORT COLLINS FOR ACCURACY AND CORRECTNESS OF THE CALCULATIONS. FURTHERMORE, THE REVIEW DOES NOT IMPLY THAT QUANTITIES OF ITEMS ON THE PLANS ARE THE FINAL QUANTITIES REQUIRED. THE REVIEW SHALL NOT BE CONSTRUED FOR ANY REASON AS ACCEPTANCE OF FINANCIAL RESPONSIBILITY BY THE CITY OF FORT COLLINS FOR ADDITIONAL QUANTITIES OF ITEMS SHOWN THAT MAY BE REQUIRED DURING THE CONSTRUCTION PHASE.

**CERTIFICATION STATEMENT:**

I HEREBY AFFIRM THAT THESE FINAL CONSTRUCTION PLANS WERE PREPARED UNDER MY DIRECT SUPERVISION, IN ACCORDANCE WITH ALL APPLICABLE CITY OF FORT COLLINS AND STATE OF COLORADO STANDARDS AND STATUTES, RESPECTIVELY, AND THAT I AM FULLY RESPONSIBLE FOR THE ACCURACY OF ALL DESIGN, REVISIONS, AND RECORD CONDITIONS THAT I HAVE NOTED ON THESE PLANS.

## CONTACT INFORMATION

**PROJECT TEAM:**

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 TERRY STENCEL  
 XCEL ENERGY  
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**ELECTRIC**  
 TODD VEDDER  
 CITY OF FORT COLLINS LIGHT & POWER  
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**CABLE**  
 DON KAPPERMAN  
 COMCAST  
 TEL: (970) 567-0425

**TELECOMMUNICATIONS**  
 WILLIAM JOHNSON  
 CENTURYLINK  
 TEL: (970) 377-6401

**WATER**  
 WES LAMARUE  
 CITY OF FORT COLLINS  
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**WASTEWATER**  
 WES LAMARUE  
 CITY OF FORT COLLINS  
 TEL: (970) 416-2418

**STORMWATER**  
 WES LAMARUE  
 CITY OF FORT COLLINS  
 TEL: (970) 416-2418

## SHEET INDEX

- 1 OF 9 COVER SHEET
- 2 OF 9 DETAILED SITE PLAN
- 3 OF 9 OVERALL SITE PLAN
- 4 OF 9 LANDSCAPE PLAN
- 5 OF 9 LANDSCAPE NOTES AND DETAILS
- 6 OF 9 LIGHTING PLAN
- 7 OF 9 UNIT SUMMARY & ENLARGED PARKING PLANS
- 8 OF 9 ELECTRICAL SITE PLAN
- 9 OF 9 ELECTRICAL ONE LINE
  - A1 EASTERN ARCHITECTURAL ELEVATIONS
  - A2 WESTERN ARCHITECTURAL ELEVATIONS

## OWNER'S CERTIFICATION OF APPROVAL

THE UNDERSIGNED DOES/DO HEREBY CERTIFY THAT I/WE ARE THE LAWFUL OWNERS OF REAL PROPERTY DESCRIBED ON THIS SITE PLAN AND DO HEREBY CERTIFY THAT I/WE ACCEPT THE CONDITIONS AND RESTRICTIONS SET FORTH ON SAID SITE PLAN.

IN WITNESS WHEREOF, WE HAVE HEREUNTO SET OUR HANDS AND SEALS THIS THE DAY OF \_\_\_\_\_, 20\_\_\_\_.

**NOTARIAL CERTIFICATE**

STATE OF COLORADO  
 COUNTY OF \_\_\_\_\_  
 THE FOREGOING INSTRUMENT WAS ACKNOWLEDGED BEFORE ME BY  
 (PRINTED NAME)  
 THIS \_\_\_\_ DAY OF \_\_\_\_\_, 20\_\_\_\_  
 MY COMMISSION EXPIRES: \_\_\_\_\_ NOTARY PUBLIC

(SEAL)

## PLANNING APPROVAL

BY THE DIRECTOR OF COMMUNITY DEVELOPMENT AND NEIGHBORHOOD SERVICES OF THE CITY OF FORT COLLINS, COLORADO THIS \_\_\_\_\_ DAY OF \_\_\_\_\_ A.D., 20\_\_\_\_.

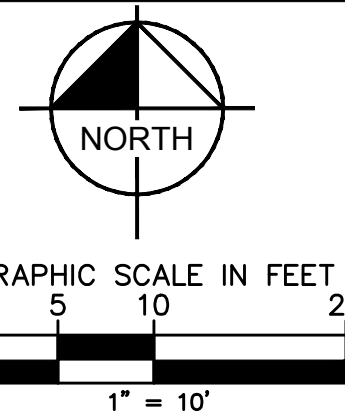
DIRECTOR OF COMMUNITY DEVELOPMENT AND NEIGHBORHOOD SERVICES

1	PDP COMMENTS	02/07/17	LEN
2	PDP COMMENTS/SITE PLAN REVISION	05/02/17	LEN
3	PDP COMMENTS	06/09/17	LEN
REVISIONS			
NO.	REVISIONS	DATE	BY
<p>© 2017 KIMLEY-HORN AND ASSOCIATES, INC.                  1001 WARRENVILLE ROAD, SUITE 350,                  Lisle, IL 60532-4550                  WWW.KIMLEY-HORN.COM</p>			
SCALE:	AS NOTED		
DESIGNED BY:	JPM		
DRAWN BY:	JPM		
CHECKED BY:	LEN		
COVER SHEET			
CHOICE CENTER THIRD FILING FORT COLLINS, CO 80525			
ORIGINAL ISSUE: 12/07/2016			
KHA PROJECT NO. 168524002			
SHEET NUMBER			
1 OF 9			

Drawing name: K:\GIS\_LIVE\16846002\_CORE\_FortCollins\_CO\_College\2\_Design\CAD\PlanSheets\SITE PLAN\1 OF 9 - COVER SHEET.dwg  
 COVER SHEET Jun 09, 2017 11:51am by: steven.pelino  
 This document, together with the concepts and designs presented herein, is intended only for the specific purpose and client for which it was prepared. Reuse of and improper reliance on this document without written authorization and adaptation by Kimley-Horn and Associates, Inc. shall be without liability to Kimley-Horn and Associates, Inc.



# CHOICE CENTER THIRD FILING



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## SITE PLAN NOTES

- THE PROJECT SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE FINAL PLANS. AMENDMENTS TO THE PLANS MUST BE REVIEWED AND APPROVED BY THE CITY PRIOR TO THE IMPLEMENTATION OF ANY CHANGES TO THE PLANS.
- REFER TO FINAL UTILITY PLANS FOR EXACT LOCATIONS AND CONSTRUCTION INFORMATION FOR STORM DRAINAGE STRUCTURES, UTILITY MAINS AND SERVICES, PROPOSED TOPOGRAPHY, STREET IMPROVEMENTS.
- REFER TO THE SUBDIVISION PLAT AND UTILITY PLANS FOR EXACT LOCATIONS, AREAS AND DIMENSIONS OF ALL EASEMENTS, LOTS, TRACTS, STREETS, WALKS AND OTHER SURVEY INFORMATION.
- ALL ROOF TOP AND GROUND MOUNTED MECHANICAL EQUIPMENT MUST BE SCREENED FROM VIEW FROM ADJACENT PROPERTY AND PUBLIC STREETS. IN CASES WHERE BUILDING PARAPETS DO NOT ACCOMPLISH SUFFICIENT SCREENING, THEN FREE-STANDING SCREEN WALLS MATCHING THE PREDOMINANT COLOR OF THE BUILDING SHALL BE CONSTRUCTED. OTHER MINOR EQUIPMENT SUCH AS CONDUIT, METERS AND PLUMBING VENTS SHALL BE SCREENED OR PAINTED TO MATCH SURROUNDING BUILDING SURFACES.
- ALL CONSTRUCTION WITH THIS DEVELOPMENT PLAN MUST BE COMPLETED IN ONE PHASE UNLESS A PHASING PLAN IS SHOWN WITH THESE PLANS.
- ALL EXTERIOR LIGHTING PROVIDED SHALL COMPLY WITH THE FOOT-CANDLE REQUIREMENTS IN SECTION 3.2.4 OF THE LAND USE CODE AND SHALL USE A CONCEALED, FULLY SHIELDED LIGHT SOURCE WITH SHARP CUT-OFF CAPABILITY SO AS TO MINIMIZE UP-LIGHT, SPILL LIGHT, GLARE AND UNNECESSARY DIFFUSION.
- FIRE HYDRANTS MUST MEET OR EXCEED POUDRE FIRE AUTHORITY STANDARDS. ALL BUILDINGS MUST PROVIDE AN APPROVED FIRE EXTINGUISHING SYSTEM.
- ALL BIKE RACKS PROVIDED MUST BE PERMANENTLY ANCHORED.
- ALL SIDEWALKS AND RAMPS MUST CONFORM TO CITY STANDARDS. ACCESSIBLE RAMPS MUST BE PROVIDED AT ALL STREET AND DRIVE INTERSECTIONS AND AT ALL DESIGNATED ACCESSIBLE PARKING SPACES. ACCESSIBLE PARKING SPACES MUST SLOPE NO MORE THAN 1:48 IN ANY DIRECTION. ALL ACCESSIBLE ROUTES MUST SLOPE NO MORE THAN 1:20 IN DIRECTIONS OF TRAVEL AND WITH NO MORE THAN 1:48 CROSS SLOPE.
- COMMON OPEN SPACE AREAS AND LANDSCAPING WITHIN RIGHT OF WAYS, STREET MEDIANS, AND TRAFFIC CIRCLES ADJACENT TO COMMON OPEN SPACE AREAS ARE REQUIRED TO BE MAINTAINED BY A PROPERTY MANAGER. THE PROPERTY MANAGER IS RESPONSIBLE FOR SNOW REMOVAL ON ALL ADJACENT STREET SIDEWALKS AND SIDEWALKS IN COMMON OPEN SPACE AREAS.
- ANY DAMAGED CURB, GUTTER AND SIDEWALK EXISTING PRIOR TO CONSTRUCTION, AS WELL AS STREETS, SIDEWALKS, CURBS AND GUTTERS, DESTROYED, DAMAGED OR REMOVED DUE TO CONSTRUCTION OF THIS PROJECT, SHALL BE REPLACED OR RESTORED TO CITY OF FORT COLLINS STANDARDS AT THE DEVELOPER'S EXPENSE PRIOR TO THE ACCEPTANCE OF COMPLETED IMPROVEMENTS AND/OR PRIOR TO THE ISSUANCE OF THE FIRST CERTIFICATE OF OCCUPANCY.
- FIRE LANE MARKING: A FIRE LANE MARKING PLAN MUST BE REVIEWED AND APPROVED BY THE FIRE OFFICIAL PRIOR TO THE ISSUANCE OF ANY CERTIFICATE OF OCCUPANCY. WHERE REQUIRED BY THE FIRE CODE OFFICIAL, APPROVED SIGNS OR OTHER APPROVED NOTICES THAT INCLUDE THE WORDS NO PARKING FIRE LANE SHALL BE PROVIDED FOR FIRE APPARATUS ACCESS ROADS TO IDENTIFY SUCH ROADS OR PROHIBIT THE OBSTRUCTION THEREOF. THE MEANS BY WHICH FIRE LANES ARE DESIGNATED SHALL BE MAINTAINED IN A CLEAN AND LEGIBLE CONDITION AT ALL TIMES AND BE REPLACED OR REPAIRED WHEN NECESSARY TO PROVIDE ADEQUATE VISIBILITY.
- SIGNAGE AND ADDRESSING ARE NOT PERMITTED WITH THIS PLANNING DOCUMENT AND MUST BE APPROVED BY SEPARATE CITY PERMIT PRIOR TO CONSTRUCTION. SIGNS MUST COMPLY WITH CITY SIGN CODE UNLESS A SPECIFIC VARIANCE IS GRANTED BY THE CITY.
- LIMITS OF STREET CUTS ARE APPROXIMATE. FINAL LIMITS ARE TO BE DETERMINED IN THE FIELD BY THE CITY ENGINEERING INSPECTOR. ALL REPAIRS TO BE IN ACCORDANCE WITH CITY STREET REPAIR STANDARDS.

## LEGEND

- HEAVY DUTY CONCRETE PAVEMENT  
SEE CONSTRUCTION DETAILS FOR PAVEMENT SECTION
- STANDARD DUTY ASPHALT PAVEMENT  
SEE CONSTRUCTION DETAILS FOR PAVEMENT SECTION
- CONCRETE SIDEWALK  
SEE CONSTRUCTION DETAILS FOR PAVEMENT SECTION
- STAMPED CONCRETE (NAVAJO RED)
- PREVIOUS PAVERS  
SEE CONSTRUCTION DETAILS FOR PAVEMENT SECTION
- STANDARD PITCH CONCRETE CURB AND GUTTER
- PROPERTY LINE
- EXISTING EASEMENT LINE
- PROPOSED EASEMENT LINE
- NEW EDGE OF PAVEMENT LINE
- EMERGENCY ACCESS EASEMENT
- ACCESS EASEMENT
- UTILITY EASEMENT
- DRAINAGE EASEMENT
- PUBLIC ACCESS EASEMENT
- SIDEWALK EASEMENT
- "NO PARKING FIRE LANE" SIGN PER CITY OF FORT COLLINS DRAWING 1418

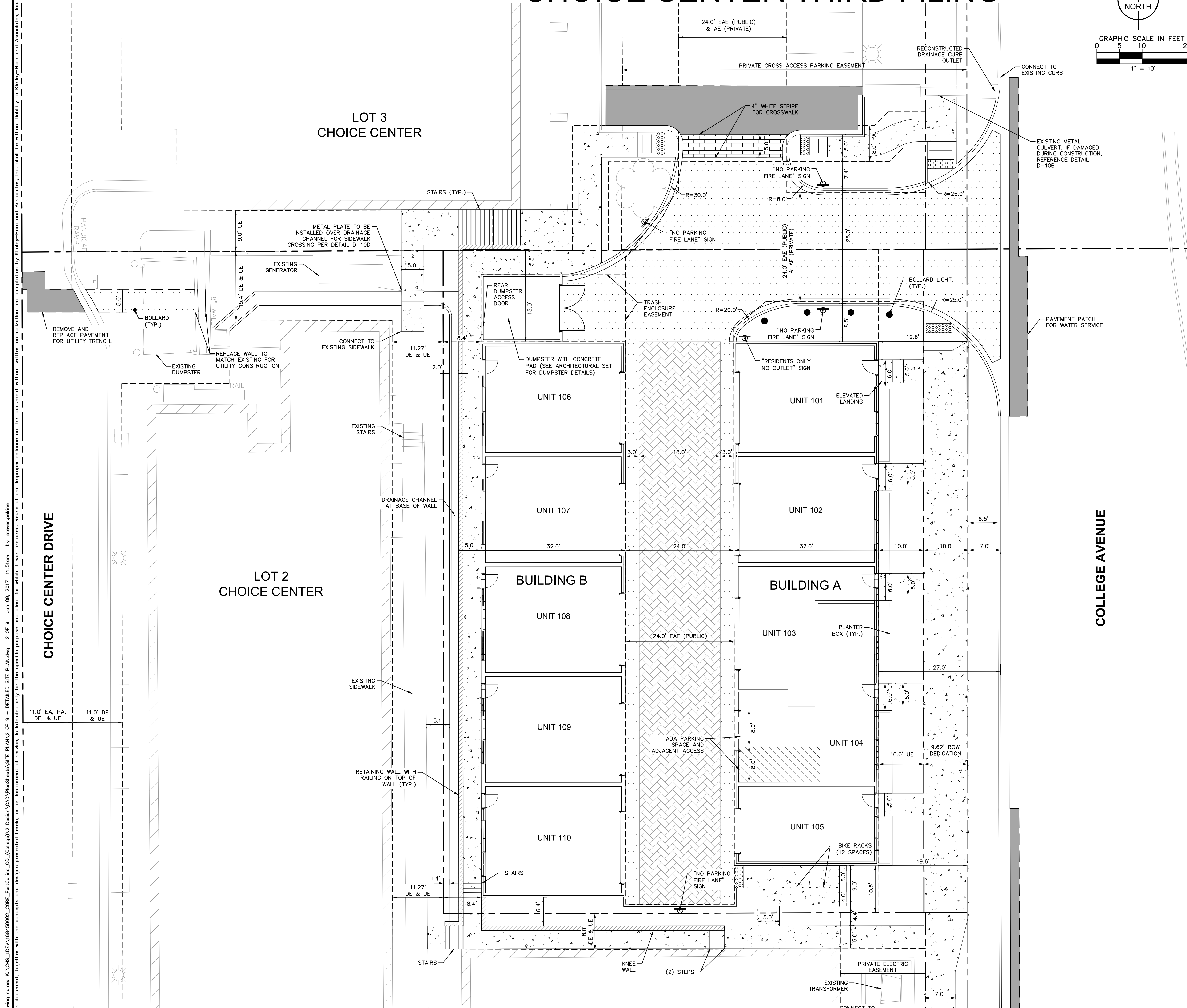
LEN	02/07/17	PDP COMMENTS	1	AS NOTED	SCALE:
LEN	05/02/17	PDP REVISION	2	DESIGNED BY: JPM	 © 2017 KIMLEY-HORN AND ASSOCIATES, INC. 1001 WARRENVILLE ROAD, SUITE 350, Lisle, IL 60532 WWW.KIMLEY-HORN.COM
LEN	06/09/17	PDP COMMENTS	3	DRAWN BY: JPM	
LEN				CHECKED BY: LEN	
BY		REVISIONS	NO	DATE	

## DETAILED SITE PLAN

## CHOICE CENTER THIRD FILING

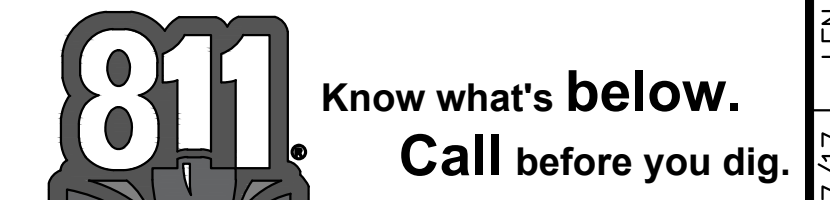
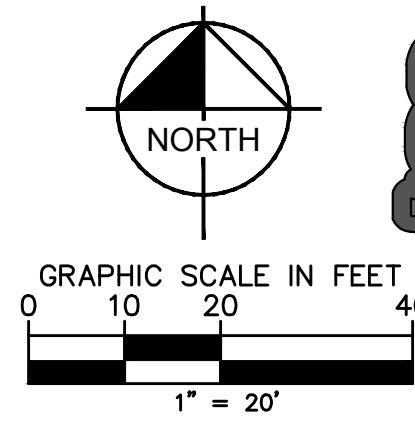
ORIGINAL ISSUE:  
12/07/2016  
KHA PROJECT NO.  
168524002

SHEET NUMBER  
2 OF 9

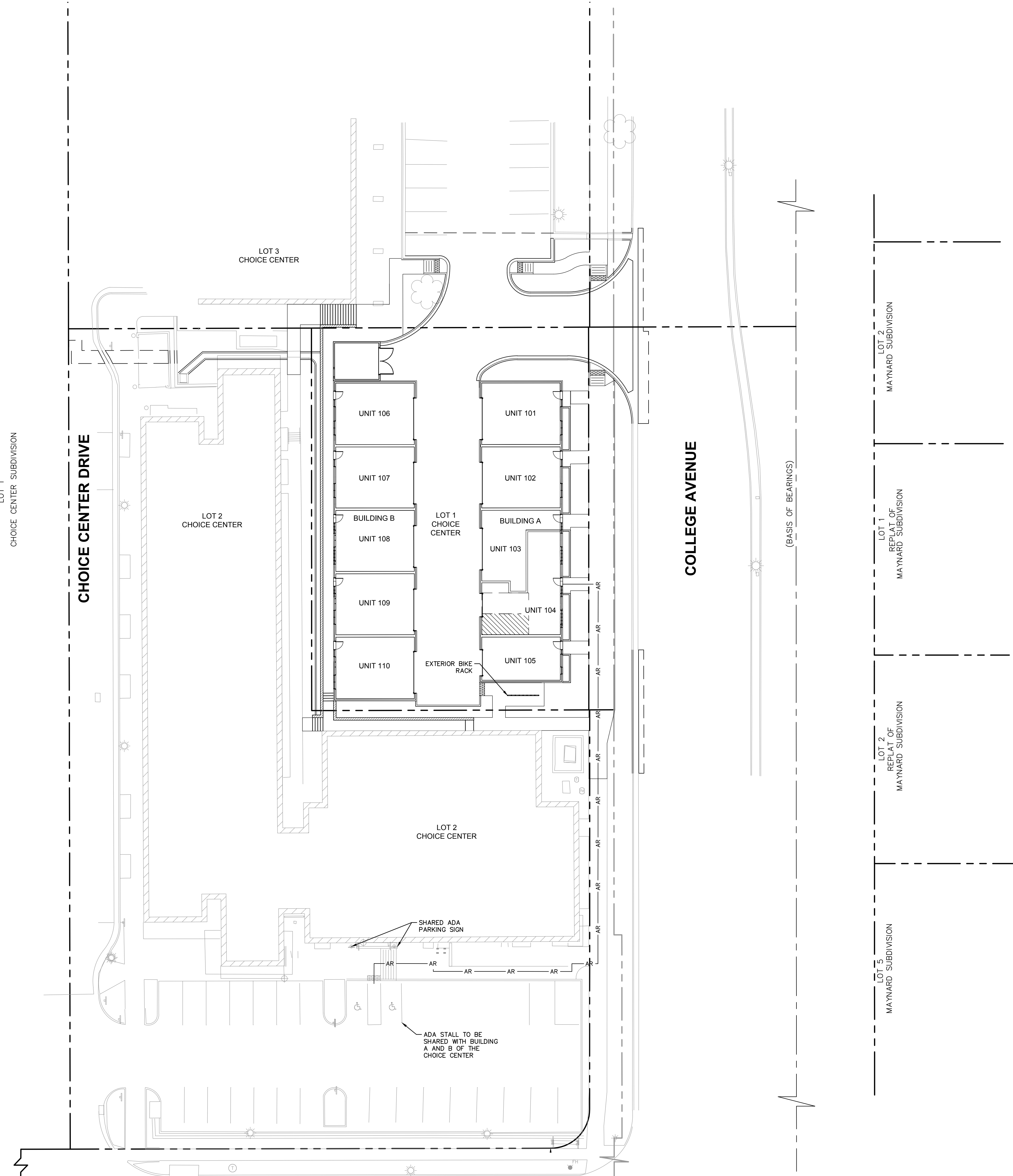


Drawing name: K:\CHS\_LBE\168460002\_CORE\_FortCollins\_CO\_College\2 Design\CAD\PlanSheets\SITE PLAN\2 OF 9 - DETAILED SITE PLAN.dwg 2 OF 9 Jun 09, 2017 11:51am By: steven.pelrine  
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# CHOICE CENTER THIRD FILING



10 GUEST PARKING SPACES TO BE PROVIDED WITHIN THE CHOICE CENTER PARKING GARAGE FOR THE MULTI FAMILY DEVELOPMENT ON LOT 1 OF CHOICE CENTER.



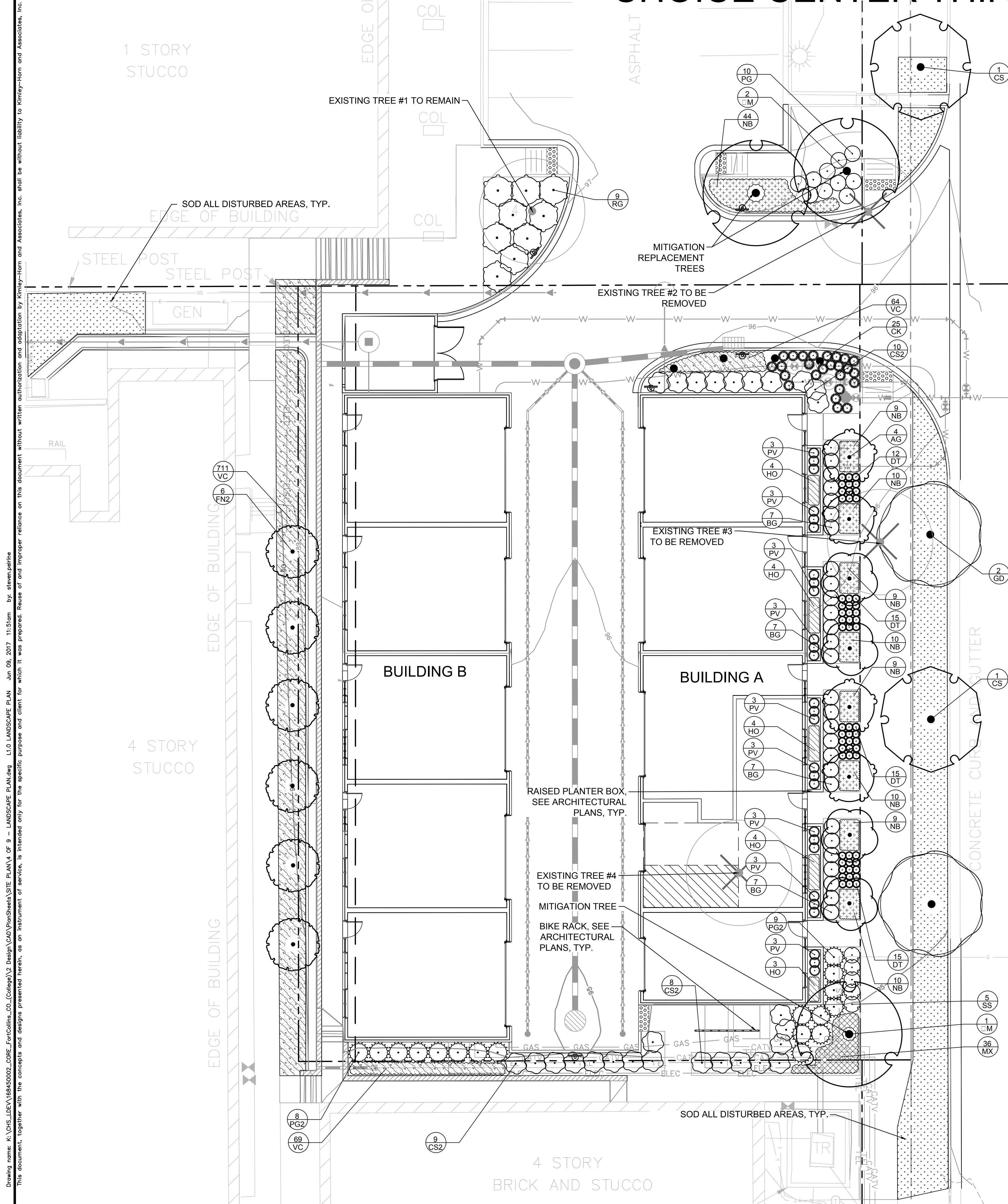
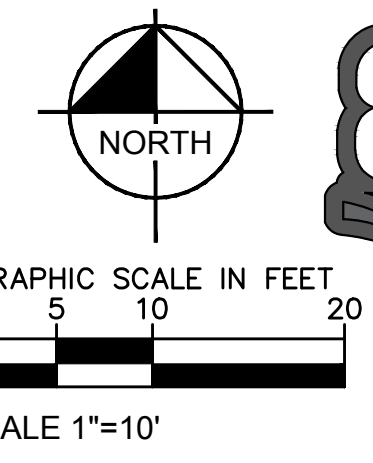
ZONING INFORMATION	
ZONING	GCD - GENERAL COMMERCIAL DISTRICT TOD - TRANSIT ORIENTED DEVELOPMENT OVERLAY ZONE
EXISTING LOT SIZE	0.35 AC (15,447 SF)
PROPOSED LOT SIZE	0.36 AC (15,625 SF)
ROW DEDICATION	1,413 SF (0.03 AC)
BUILDING INFORMATION	
BUILDING TYPE	MULTI-FAMILY RESIDENTIAL
TOTAL UNITS/BEDROOMS	1 UNIT / 5 BEDROOM 7 UNITS / 3 BEDROOM 1 UNIT / 2 BEDROOM 1 UNIT / 1 BEDROOM
BUILDING HEIGHT	30 FT
RESIDENTIAL DENSITY	24.7 DU/ACRE
GROSS FLOOR AREA	15,292 SF
GARAGE FLOOR AREA	4,170 SF
NET FLOOR AREA	11,122 SF
FLOOR AREA RATIO	0.7118
LOT COVERAGE	
BUILDING	48.9% (7,646 SF)
DRIVEWAY/PARKING	39.5% (6,164 SF)
OPEN SPACE/LANDSCAPE	11.6% (1,815 SF)
ACTIVE RECREATIONAL USE	0.0% (0 SF)
PARKING INFORMATION	
PARKING REQUIRED (TOD)	0.75/1 BEDROOM UNIT * 1 UNIT = 0.75 SPACES 1.00/2 BEDROOM UNIT * 1 UNIT = 1 SPACE 1.25/3 BEDROOM UNIT * 7 UNITS = 8.75 SPACES 1.5/5 BEDROOM UNIT * 1 UNIT = 1.5 SPACES
TOTAL REQUIRED (TOD)	12 SPACES
PARKING PROVIDED	18 SPACES (GARAGE UNITS)
GUEST PARKING PROVIDED	10 SPACES (PARKING GARAGE)
ADA SPACES REQUIRED	1 ADA GARAGE SPACE; 1 ADA GUEST SPACE
ADA SPACES PROVIDED	1 GARAGE SPACE (UNIT 104); 1 GUEST SPACE (CHOICE CENTER PARKING SHARED SPACE)
BICYCLE SPACES REQUIRED	1/2 BED * 29 BED = 29 SPACES (60%/40% INTERIOR/EXTERIOR = 17/12)
BICYCLE SPACES PROVIDED	18 SPACES INTERIOR/12 SPACES EXTERIOR TOTAL BICYCLE PARKING = 30 SPACES

LEGEND	
	STANDARD PITCH CONCRETE CURB AND GUTTER
	PROPERTY LINE
	NEW EDGE OF PAVEMENT LINE
	ACCESSIBLE ROUTE
	PARKING SIGN
	ACCESSIBLE RAMP
	TRUNCATED DOME TRACK PAD

LEN	02/07/17	PDP COMMENTS	NO	REVISIONS	DATE	BY
LEN	05/02/17	PDP COMMENTS/SITE PLAN REVISION				
LEN	06/09/17	PDP COMMENTS				
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<p>OVERALL SITE PLAN</p>						
<p>CHOICE CENTER THIRD FILING FORT COLLINS, CO 80525</p>						
<p>ORIGINAL ISSUE: 12/07/2016 KHA PROJECT NO. 168524002</p>						
<p>SHEET NUMBER <b>3 OF 9</b></p>						

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# CHOICE CENTER THIRD FILING



## PLANT SCHEDULE

TREES	CODE	TY	BOTANICAL NAME	COMMON NAME	CONT	CAL	SI	E	DIVERSITY
	CS	2	CATALPA SPECIOSA	NORTHERN CATALPA	B & B	2" CAL MIN			9.5
	FN2	6	FORESTIERA NEOMEXICANA	NEW MEXICAN PRIVET	10 GAL		6-8 HT		28
	GD	2	GYMNOCLADUS DIOICA ESPRESSO	KENTUCKY COFFEETREE	B & B	2" CAL MIN			9.5
	M	3	QUERCUS MACROCARPA	BURR OAK	B & B	3" CAL MIN			14
EXISTING TREES	CODE	TY	BOTANICAL NAME	COMMON NAME	CONT	CAL	SI	E	
	EX	4	--	EXISTING DECIDUOUS TREE					
ORNAMENTAL TREES	CODE	TY	BOTANICAL NAME	COMMON NAME	CONT	CAL	SI	E	
	AG	4	AMELANCHIER X GRANDIFLORA AUTUMN BRILLIANCE	AUTUMN BRILLIANCE SERVICEBERRY	B & B	2" CAL MIN			19.5
	PY	4	PYRUS CALLERYANA CAPITAL	CAPITAL CALLERY PEAR	B & B	2" CAL MIN			19.5
SHRUBS	CODE	TY	BOTANICAL NAME	COMMON NAME	CONT	SPACING	SI	E	
	BG	28	BUXUS X GREEN VELVET	BOXWOOD	5 GAL	SEE PLAN			
	CS2	27	CORNUS STOLONIFERA RED OSIER	DOGWOOD	5 GAL				
	PG	10	POTENTILLA FRUTICOSA GOLD STAR	GOLD STAR POTENTILLA	3 GAL	SEE PLAN			
	PG2	17	PICEA PUNGENS GLAUCA GLOBOSA	COLORADO SPRUCE	1 GAL	SEE PLAN			
	RG	9	RHUS AROMATICA GRO-LOW	GRO-LOW FRAGRANT SUMAC	1 GAL	SEE PLAN			
	SS	5	SPIRAEA JAPONICA LITTLE PRINCESS	LITTLE PRINCESS JAPANESE SPIREA	5 GAL	SEE PLAN			
GRASSES	CODE	TY	BOTANICAL NAME	COMMON NAME	CONT	SPACING	SI	E	
	CK	25	CALAMAGROSTIS X ACUTIFLORA KARL FOERSTER	FEATHER REED GRASS	1 GAL	SEE PLAN			
	DT	57	DESCHAMPSIA CESPITOSA	TUFTED HAIR GRASS	1 GAL				
	PV	27	PANICUM VIRGATUM	SWITCH GRASS	1 GAL	SEE PLAN			
GROUND COVERS	CODE	TY	BOTANICAL NAME	COMMON NAME	SPACING				
			1,330 SFPOA PRATENSIS	KENTUCKY BLUEGRASS					
	HO	19	HEMEROCALLIS X STELLA DE ORO	STELLA DE ORO DAYLILY	24" o.c.				
	MX	36	PERENNIAL MIX	60" ALLIUM, 40" LESSER CALAMINT	18" o.c.				
	NB	118	NEPETA RACEMOSA BLUE WONDER	BLUE WONDER CATMINT	18" o.c.				
	VC	927	VINCA MINOR	COMMON PERIWINKLE	12" o.c.				

- NOTE:**
- ALL PLANTS SPECIFIED FALL IN ACCORDANCE WITH THE FORT COLLINS APPROVED PLANT LIST
  - ALL PLANT SPECIFICATIONS IN THE PLANT SCHEDULE SHALL BE CONSIDERED THE MINIMUM ALLOWABLE SPECIFICATIONS. CONTRACTOR SHALL PROCURE PLANT MATERIALS AND UPSIZE AS NECESSARY TO MEET THE MOST STRINGENT SPECIFICATION.
  - LANDSCAPE ARCHITECT RESERVES THE RIGHT TO FIELD ADJUST ALL PLANT MATERIAL.
  - ALL QUANTITIES PROVIDED FOR CONVENIENCE ONLY. CONTRACTOR SHALL CONFIRM ALL QUANTITIES PER PLAN PRIOR TO BIDDING AND CONSTRUCTION.
  - NO TREES SHALL BE REMOVED DURING THE SONGBIRD NESTING SEASON (FEBRUARY 1 TO JULY 31) WITHOUT FIRST HAVING A PROFESSIONAL ECOLOGIST OR WILDLIFE BIOLOGIST COMPLETE A NESTING SURVEY TO IDENTIFY ANY ACTIVE NESTS EXISTING ON THE PROJECT SITE. THE SURVEY SHALL BE SENT TO THE CITY ENVIRONMENTAL PLANNER. IF ACTIVE NESTS ARE FOUND THE CITY WILL COORDINATE WITH RELEVANT STATE AND FEDERAL REPRESENTATIVES TO DETERMINE WHETHER ADDITIONAL RESTRICTIONS ON TREE REMOVAL AND CONSTRUCTION APPLY.

OVERALL SITE NORTH AND SOUTH			
HYDROZONE	AREA (SF)	WATER NEED (GALLONS/SF)	ANNUAL WATER USE (GALLONS)
HIGH	59	18	1,062
MODERATE	1,350	10	13,500
LOW	1,502	3	4,506
VERY LOW	0	0	0
<b>TOTAL</b>	<b>2,911</b>	<b>Avg: 6.55</b>	<b>19,068</b>

Tree Mitigation				
TREE NUMBER	CALIPER	TYPE	CONDITION	MITIGATION REQUIREMENTS
1	4"	HONEY LOCUST	GOOD	0
2	17.5"	HONEY LOCUST	FAIR	2
3	13"	HONEY LOCUST	POOR	1
	MULTI- 3"-4" STEMS, 5 TOTAL	SIBERIAN ELM	FAIR	0

A PERMIT MUST BE OBTAINED FROM THE CITY FORESTER BEFORE ANY TREES OR SHRUBS AS NOTED ON THIS PLAN ARE PLANTED, PRUNED OR REMOVED IN THE PUBLIC RIGHT-OF-WAY. THIS INCLUDES TREES BETWEEN THE SIDEWALK AND CURB, MEDIANS AND OTHER CITY PROPERTY. THIS PERMIT SHALL APPROVE THE LOCATION AND SPECIES TO BE PLANTED. FAILURE TO OBTAIN THIS PERMIT IS A VIOLATION OF THE CITY OF FORT COLLINS CODE SUBJECT TO CITATION (SECTION 27-31) AND MAY ALSO RESULT IN REPLACING OR RELOCATING TREES AND A HOLD ON CERTIFICATE OF OCCUPANCY.

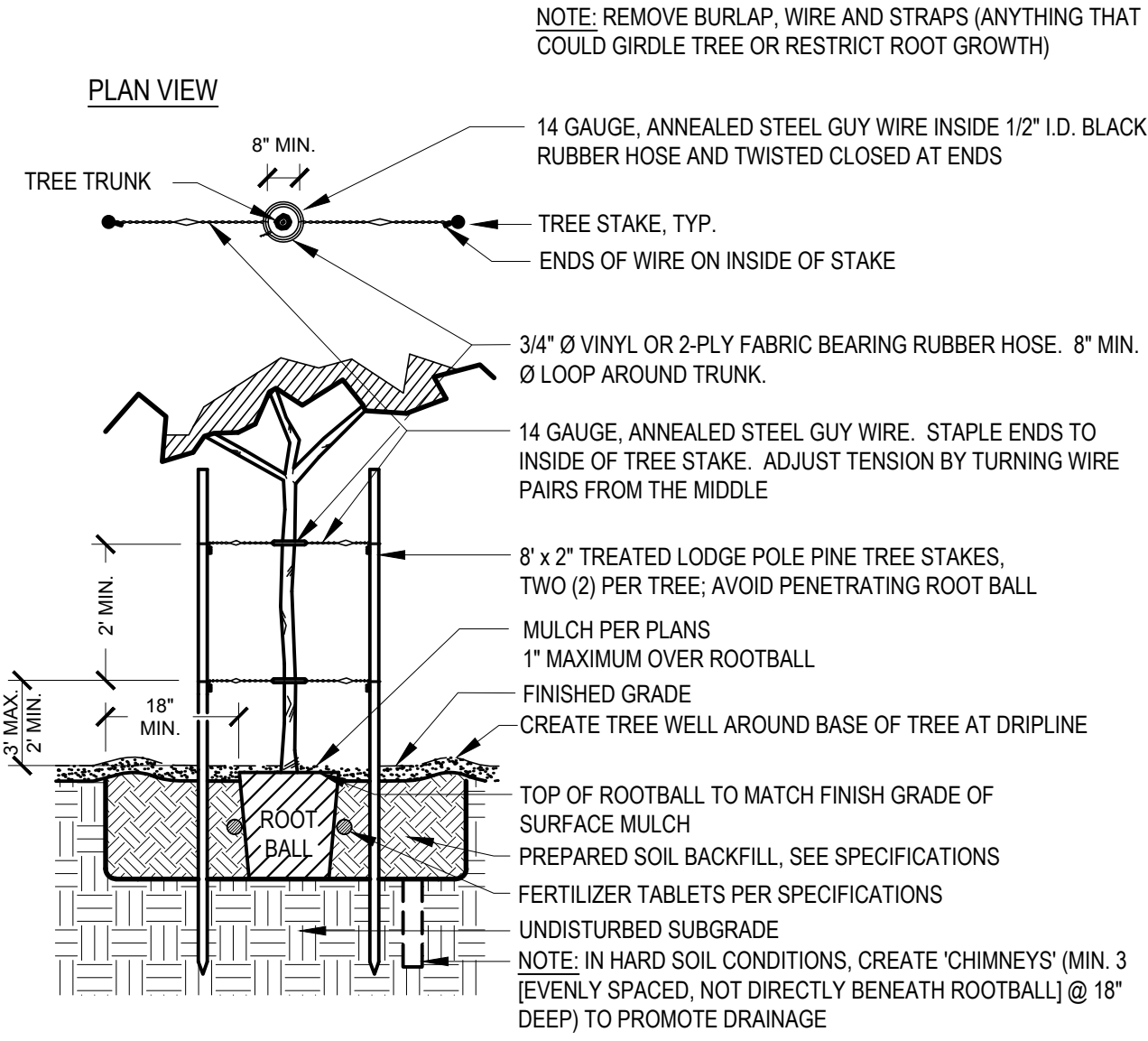
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1	PDP COMMENTS	02/07/17	LEN	BY
2	PDP COMMENTS/SITE PLAN REVISION	05/02/17	LEN	
3	PDP COMMENTS	06/09/17	LEN	
<b>LANDSCAPE PLAN</b>				
<b>CHOICE CENTER THIRD FILING</b> FORT COLLINS, CO 80525				
ORIGINAL ISSUE: 12/07/2016				
KHA PROJECT NO. 168524002				
SHEET NUMBER				
4 OF 9				

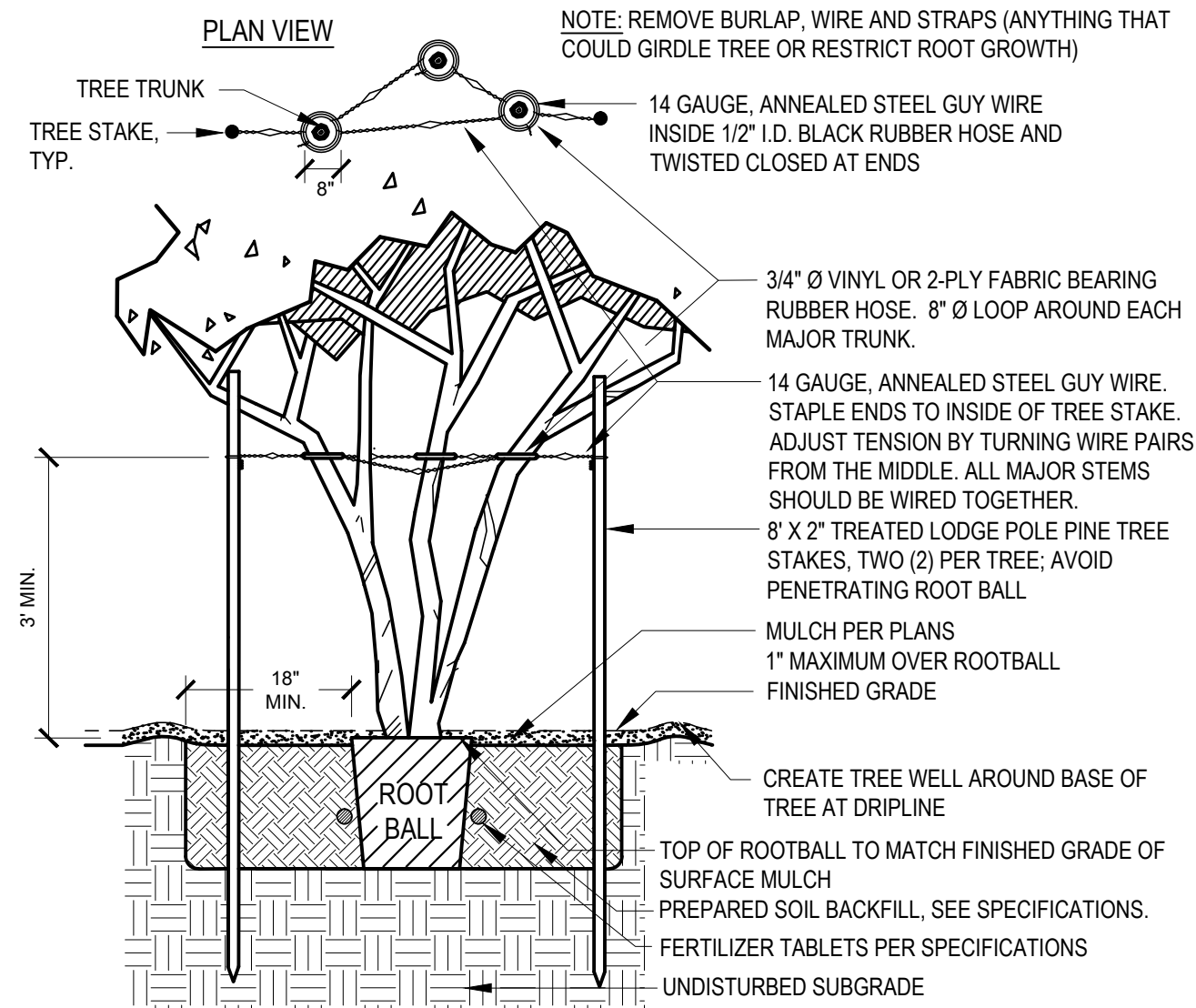


Know what's below. Call before you dig.

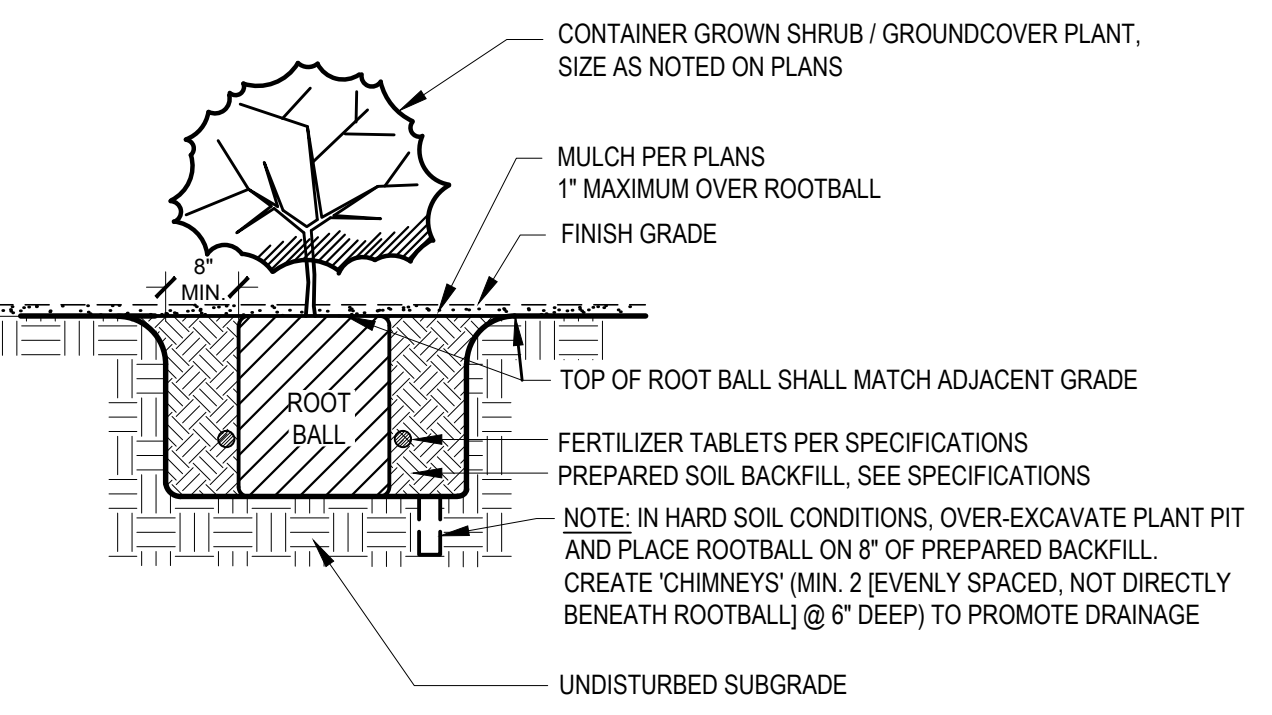
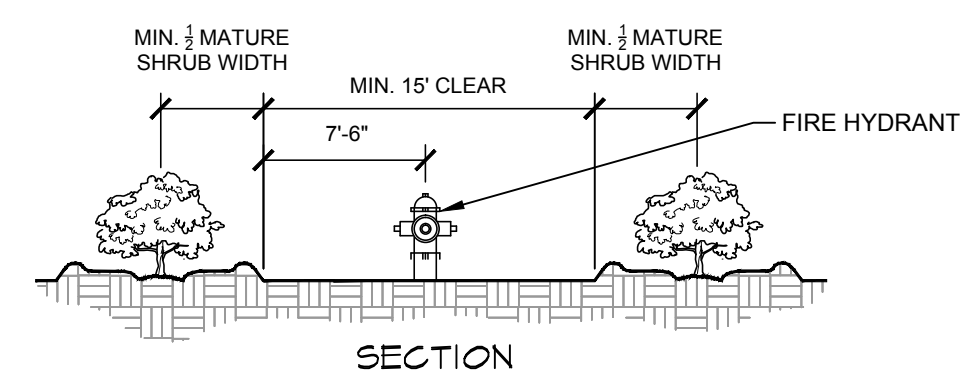
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**1 TREE PLANTING AND STAKING**  
SCALE: N.T.S.

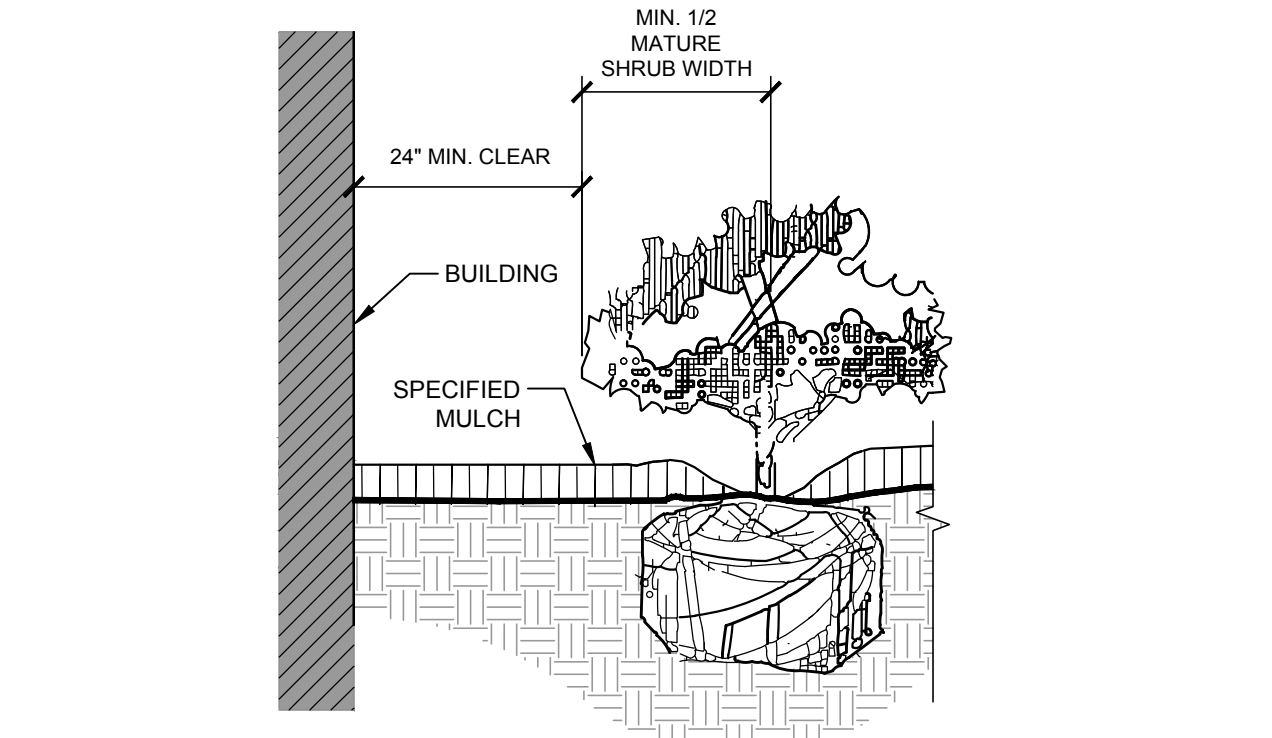


**2 TREE STAKING - MULTI-TRUNK**  
SCALE: N.T.S.



**3 SHRUB / GROUND COVER PLANTING**  
SCALE: N.T.S.

**4 SHRUB PLANTING AT FIRE HYDRANT**  
SCALE: N.T.S.



**NOTES:**  
1. CLEARANCE: 36" MIN. FROM BUILDING TO CENTER OF NEAREST SHRUB.  
2. INSTALL SPECIFIED MULCH: 24" MIN. FROM BUILDING. SPECIFIED MULCH TO BE INSTALLED AT A DEPTH OF 3" (MIN.)

**5 PLANTING ADJACENT TO BUILDINGS**  
SCALE: N.T.S.

**CITY OF FORT COLLINS GENERAL LANDSCAPE NOTES:**

- 1. PLANT QUALITY: ALL PLANT MATERIAL SHALL BE A-GRADE OR NO. 1 GRADE - FREE OF ANY DEFECTS, OF NORMAL HEALTH, HEIGHT, LEAF DENSITY AND SPREAD APPROPRIATE TO THE SPECIES AS DEFINED BY THE AMERICAN ASSOCIATION OF NURSERYMEN (AAN) STANDARDS. ALL TREES SHALL BE BALL AND BURLAP OR EQUIVALENT.
- 2. IRRIGATION: ALL LANDSCAPE AREAS WITHIN THE SITE INCLUDING TURF, SHRUB BEDS AND TREE AREAS SHALL BE IRRIGATED WITH AN AUTOMATIC IRRIGATION SYSTEM. THE IRRIGATION PLAN MUST BE REVIEWED AND APPROVED BY THE CITY OF FORT COLLINS WATER UTILITIES DEPARTMENT PRIOR TO THE ISSUANCE OF A BUILDING PERMIT. ALL TURF AREAS SHALL BE IRRIGATED WITH AN AUTOMATIC POP-UP IRRIGATION SYSTEM. ALL SHRUB BEDS AND TREES, INCLUDING IN NATIVE SEED AREAS, SHALL BE IRRIGATED WITH AN AUTOMATIC DRIP (TRICKLE) IRRIGATION SYSTEM, OR WITH AN ACCEPTABLE ALTERNATIVE APPROVED BY THE CITY WITH THE IRRIGATION PLANS. THE IRRIGATION SYSTEM SHALL BE ADJUSTED TO MEET THE WATER REQUIREMENTS OF THE INDIVIDUAL PLANT MATERIAL.
- 3. TOPSOIL: TO THE MAXIMUM EXTENT FEASIBLE, TOPSOIL THAT IS REMOVED DURING CONSTRUCTION ACTIVITY SHALL BE CONSERVED FOR LATER USE ON AREAS REQUIRING REVEGETATION AND LANDSCAPING.
- 4. SOIL AMENDMENTS: SOIL AMENDMENTS SHALL BE PROVIDED AND DOCUMENTED IN ACCORDANCE WITH CITY CODE SECTION 12-132. THE SOIL IN ALL LANDSCAPE AREAS, INCLUDING PARKWAYS AND MEDIANS, SHALL BE THOROUGHLY LOOSENED TO A DEPTH OF NOT LESS THAN EIGHT(8) INCHES AND SOIL AMENDMENT SHALL BE THOROUGHLY INCORPORATED INTO THE SOIL OF ALL LANDSCAPE AREAS TO A DEPTH OF AT LEAST SIX(6) INCHES BY TILLING, DICING OR OTHER SUITABLE METHOD, AT A RATE OF AT LEAST THREE (3) CUBIC YARDS OF SOIL AMENDMENT PER ONE THOUSAND (1,000) SQUARE FEET OF LANDSCAPE AREA, PRIOR TO THE ISSUANCE OF ANY CERTIFICATE OF OCCUPANCY. A WRITTEN CERTIFICATION MUST BE SUBMITTED TO THE CITY THAT ALL PLANTED AREAS, OR AREAS TO BE PLANTED, HAVE BEEN THOROUGHLY LOOSENED AND THE SOIL AMENDED, CONSISTENT WITH THE REQUIREMENTS SET FORTH IN SECTION 12-132.
- 5. INSTALLATION AND GUARANTEE: ALL LANDSCAPING SHALL BE INSTALLED ACCORDING TO SOUND HORTICULTURAL PRACTICES IN A MANNER DESIGNED TO ENCOURAGE QUICK ESTABLISHMENT AND HEALTHY GROWTH. ALL LANDSCAPING FOR EACH PHASE MUST BE EITHER INSTALLED OR THE INSTALLATION MUST BE SECURED WITH AN IRREVOCABLE LETTER OF CREDIT, PERFORMANCE BOND, OR ESCROW ACCOUNT FOR 125% OF THE VALUATION OF THE MATERIALS AND LABOR PRIOR TO ISSUANCE OF A CERTIFICATE OF OCCUPANCY FOR ANY BUILDING IN SUCH PHASE.
- 6. MAINTENANCE: TREES AND VEGETATION, IRRIGATION SYSTEMS, FENCES, WALLS AND OTHER LANDSCAPE ELEMENTS WITH THESE FINAL PLANS SHALL BE CONSIDERED AS ELEMENTS OF THE PROJECT IN THE SAME MANNER AS PARKING, BUILDING MATERIALS AND OTHER SITE DETAILS. THE APPLICANT, LANDOWNER OR SUCCESSORS IN INTEREST SHALL BE JOINTLY AND SEVERALLY RESPONSIBLE FOR THE REGULAR MAINTENANCE OF ALL LANDSCAPING ELEMENTS IN GOOD CONDITION. ALL LANDSCAPING SHALL BE MAINTAINED FREE FROM DISEASE, PESTS, WEEDS AND LITTER, AND ALL LANDSCAPE STRUCTURES SUCH AS FENCES AND WALLS SHALL BE REPAIRED AND REPLACED PERIODICALLY TO MAINTAIN A STRUCTURALLY SOUND CONDITION.
- 7. REPLACEMENT: ANY LANDSCAPE ELEMENT THAT DIES, OR IS OTHERWISE REMOVED, SHALL BE PROMPTLY REPLACED IN ACCORDANCE WITH THE REQUIREMENTS OF THESE PLANS.
- 8. THE FOLLOWING SEPARATIONS SHALL BE PROVIDED BETWEEN TREES/SHRUBS AND UTILITIES:  
40 FEET BETWEEN CANOPY TREES AND STREET LIGHTS  
15 FEET BETWEEN ORNAMENTAL TREES AND STREETLIGHTS  
10 FEET BETWEEN TREES AND PUBLIC WATER, SANITARY AND STORM SEWER MAIN LINES  
6 FEET BETWEEN TREES AND PUBLIC WATER, SANITARY AND STORM SEWER SERVICE LINES  
4 FEET BETWEEN SHRUBS AND PUBLIC WATER AND SANITARY AND STORM SEWER LINES  
4 FEET BETWEEN TREES AND GAS LINES
- 9. ALL STREET TREES SHALL BE PLACED A MINIMUM EIGHT (8) FEET AWAY FROM THE EDGES OF DRIVEWAYS AND ALLEYS PER LUC 3.2.1(D)(2)(a).
- 10. PLACEMENT OF ALL LANDSCAPING SHALL BE IN ACCORDANCE WITH THE SIGHT DISTANCE CRITERIA AS SPECIFIED BY THE CITY OF FORT COLLINS. NO STRUCTURES OR LANDSCAPE ELEMENTS GREATER THAN 24" SHALL BE ALLOWED WITHIN THE SIGHT DISTANCE TRIANGLE OR EASEMENTS WITH THE EXCEPTION OF DECIDUOUS TREES PROVIDED THAT THE LOWEST BRANCH IS AT LEAST 6' FROM GRADE, ANY FENCES WITHIN THE SIGHT DISTANCE TRIANGLE OR EASEMENT MUST BE NOT MORE THAN 42" IN HEIGHT AND OF AN OPEN DESIGN.
- 11. THE FINAL LANDSCAPE PLAN SHALL BE COORDINATED WITH ALL OTHER FINAL PLAN ELEMENTS SO THAT THE PROPOSED GRADING, STORM DRAINAGE, AND OTHER DEVELOPMENT IMPROVEMENTS DO NOT CONFLICT WITH NOR PRECLUDE INSTALLATION AND MAINTENANCE OF LANDSCAPE ELEMENTS ON THIS PLAN.
- 12. MINOR CHANGES IN SPECIES AND PLANT LOCATIONS MAY BE MADE DURING CONSTRUCTION - AS REQUIRED BY SITE CONDITIONS OR PLANT AVAILABILITY. OVERALL QUANTITY, QUALITY, AND DESIGN CONCEPT MUST BE CONSISTENT WITH THE APPROVED PLANS. IN THE EVENT OF CONFLICT WITH THE QUANTITIES INCLUDED IN THE PLANT LIST, SPECIES AND QUANTITIES ILLUSTRATED SHALL BE PROVIDED. ALL CHANGES OF PLANT SPECIES AND LOCATION MUST HAVE WRITTEN APPROVAL BY THE CITY PRIOR TO INSTALLATION.
- 13. ALL PLANTING BEDS SHALL BE MULCHED TO A MINIMUM DEPTH OF THREE INCHES WITH SHREDDED HARDWOOD MULCH. ALL BEDLINES ADJACENT TO TURF GRASS AREAS TO RECEIVE A SPADED EDGE.

**CITY OF FORT COLLINS STREET TREE NOTES:**

- 1. A PERMIT MUST BE OBTAINED FROM THE CITY FORESTER BEFORE ANY TREES OR SHRUBS AS NOTED ON THIS PLAN ARE PLANTED, PRUNED OR REMOVED ON THE PUBLIC RIGHT-OF-WAY. THIS INCLUDES ZONES BETWEEN THE SIDEWALK AND CURB, MEDIANS AND OTHER CITY PROPERTY. THIS PERMIT SHALL APPROVE THE LOCATION AND SPECIES TO BE PLANTED. FAILURE TO OBTAIN THIS PERMIT MAY RESULT IN REPLACING OR RELOCATING TREES AND A HOLD ON CERTIFICATE OF OCCUPANCY.
- 2. CONTACT THE CITY FORESTER TO INSPECT ALL STREET TREE PLANTINGS AT THE COMPLETION OF EACH PHASE OF THE DEVELOPMENT. ALL TREES NEED TO HAVE BEEN INSTALLED AS SHOWN ON THE LANDSCAPE PLAN. APPROVAL OF STREET TREE PLANTING IS REQUIRED BEFORE FINAL APPROVAL OF EACH PHASE.
- 3. STREET TREE SHALL BE SUPPLIED AND PLANTED BY THE DEVELOPER USING A QUALIFIED LANDSCAPE CONTRACTOR.
- 4. THE DEVELOPER SHALL REPLACE ALL DEAD AND DYING STREET TREES AFTER PLANTING UNTIL FINAL MAINTENANCE INSPECTION AND ACCEPTANCE BY THE CITY OF FORT COLLINS FORESTRY DIVISION. ALL STREET TREES IN THE PROJECT MUST BE ESTABLISHED OF AN APPROVED SPECIES AND OF ACCEPTABLE LEAD CONDITION PRIOR TO ACCEPTANCE.
- 5. STREET TREE LOCATIONS AND NUMBERS MAY BE ADJUSTED TO ACCOMMODATE DRIVEWAY.
- 6. LOCATIONS, UTILITY STANDARDS, SEPARATION BETWEEN TREES, STREET SIGNS AND STREET LIGHTS. STREET TREES SHALL BE CENTERED IN THE MIDDLE OF THE PARKWAY. QUANTITIES SHOWN ON PLAN MUST BE INSTALLED UNLESS A REDUCTION OCCURS TO MEET SEPARATION STANDARDS.

**TREE PROTECTION NOTES:**

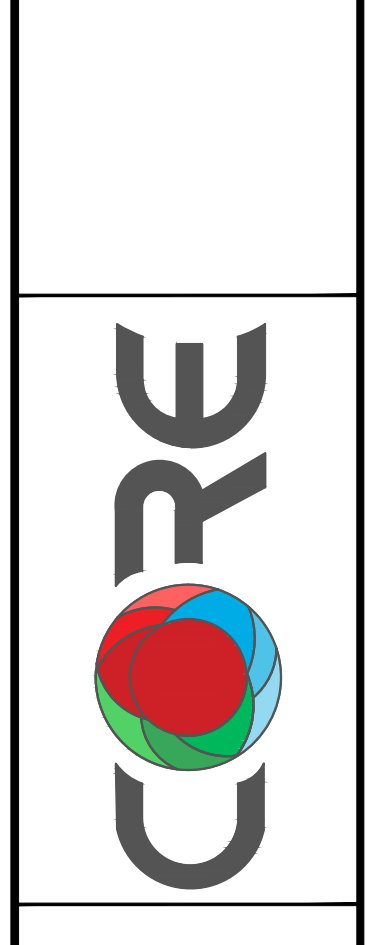
- 1. THE FOLLOWING TREE PROTECTION SPECIFICATIONS SHOULD BE FOLLOWED TO THE MAXIMUM EXTENT FEASIBLE FOR ALL PROJECTS WITH PROTECTED EXISTING TREES.
- 2. ALL EXISTING TREES WITHIN THE LIMITS OF THE DEVELOPMENT AND WITHIN ANY NATURAL AREA BUFFER ZONES SHALL REMAIN AND BE PROTECTED UNLESS NOTED ON THESE PLANS FOR REMOVAL.
- 3. WITHIN THE DRIP LINE OF ANY PROTECTED EXISTING TREE, THERE SHALL BE NO CUT OR FILL OVER A FOUR-INCH DEPTH UNLESS A QUALIFIED ARBORIST OR FORESTER HAS EVALUATED AND APPROVED THE DISTURBANCE STANDARDS PRIOR TO AND DURING CONSTRUCTION. BARRIERS SHALL BE ERRECTED AROUND ALL PROTECTED EXISTING TREES WITH SUCH BARRIERS TO BE OF ORANGE FENCING A MINIMUM OF FOUR (4) FEET IN HEIGHT, SECURED WITH METAL T-POSTS, NO CLOSER THAN SIX (6) FEET FROM THE TRUNK OR ONE-HALF (1/2) OF THE DRIP LINE, WHICHEVER IS GREATER. THERE SHALL BE NO STORAGE OR MOVEMENT OF EQUIPMENT, MATERIAL, DEBRIS OR FILL WITHIN THE FENCED TREE PROTECTION ZONE.
- 4. DURING THE CONSTRUCTION STAGE OF DEVELOPMENT, THE APPLICANT SHALL PREVENT THE CLEANING OF EQUIPMENT OR MATERIAL OR THE STORAGE AND DISPOSAL OF WASTE MATERIAL SUCH AS PAINTS, OILS, SOLVENTS, ASPHALT, CONCRETE, MOTOR OIL OR ANY OTHER MATERIAL HARMFUL TO THE LIFE OF A TREE WITHIN THE DRIP LINE OF ANY PROTECTED TREE OR GROUP OF TREES.
- 5. NO DAMAGING ATTACHMENT, WIRES, SIGNS OR PERMITS MAY BE FASTENED TO ANY PROTECTED TREE.  
(B) ORNAMENTAL TREES 2.5" CALIPER BALLED AND BURLAP OR EQUIVALENT  
(C) EVERGREEN TREES 8" HEIGHT BALLED AND BURLAP OR EQUIVALENT COLORADO CODE PUBLISHING COMPANY - FORT COLLINS LAND USE CODE - ARTICLE 3 GENERAL - PAGE 15 OF 174: <http://www.colorado.com/fortcollins/landuse/article3.htm> 12/2/10
- 6. LARGE PROPERTY AREAS CONTAINING PROTECTED TREES AND SEPARATED FROM CONSTRUCTION OR LAND CLEARING AREAS, ROAD RIGHTS-OF-WAY AND UTILITY EASEMENTS MAY BE "RIBBONED OFF" RATHER THAN ERRECTING PROTECTIVE FENCING AROUND EACH TREE AS REQUIRED IN SUBSECTION (G)(3) ABOVE. THIS MAY BE ACCOMPLISHED BY PLACING METAL T-POST STAKES A MAXIMUM OF FIFTY (50) FEET APART AND TYING RIBBON OR ROPE FROM STAKE-TO-STAKE ALONG THE OUTSIDE PERIMETERS OF SUCH AREAS BEING CLEARED.
- 7. THE INSTALLATION OF UTILITIES, IRRIGATION LINES OR ANY UNDERGROUND FIXTURE REQUIRING EXCAVATION DEEPER THAN SIX (6) INCHES SHALL BE ACCOMPLISHED BY BORING UNDER THE ROOT SYSTEM OF PROTECTED EXISTING TREES AT A MINIMUM DEPTH OF TWENTY-FOUR (24) INCHES. THE AUGER DISTANCE IS ESTABLISHED FROM THE FACE OF THE TREE (OUTER BARK) AND IS SCALED FROM TREE DIAMETER AT BREAST HEIGHT AS DESCRIBED IN THE CHART BELOW.
- 8. ALL TREE REMOVAL SHOWN SHALL BE COMPLETED OUTSIDE OF THE SONGBIRD NESTING SEASON (FEB 1 - JULY 31) OR CONDUCT A SURVEY OF TREES ENSURING NO ACTIVE NESTS IN THE AREA.

Tree Diameter at Breast Height (inches)	Auger Distance From Face of Tree (feet)
0-2	1
3-4	2
5-9	3
10-14	10
15-19	12
Over 19	15

NO.	PDP COMMENTS/SITE PLAN REVISION	DATE	BY
1		02/07/17	LEN
2		05/02/17	LEN
3		06/09/17	LEN

**Kimley-Horn**  
© 2017 KIMLEY-HORN AND ASSOCIATES, INC.  
1001 WARRENVILLE ROAD, SUITE 350,  
Lisle, IL 60532-4650  
WWW.KIMLEY-HORN.COM

SCALE: AS NOTED  
DESIGNED BY: JPM  
DRAWN BY: JPM  
CHECKED BY: LEN

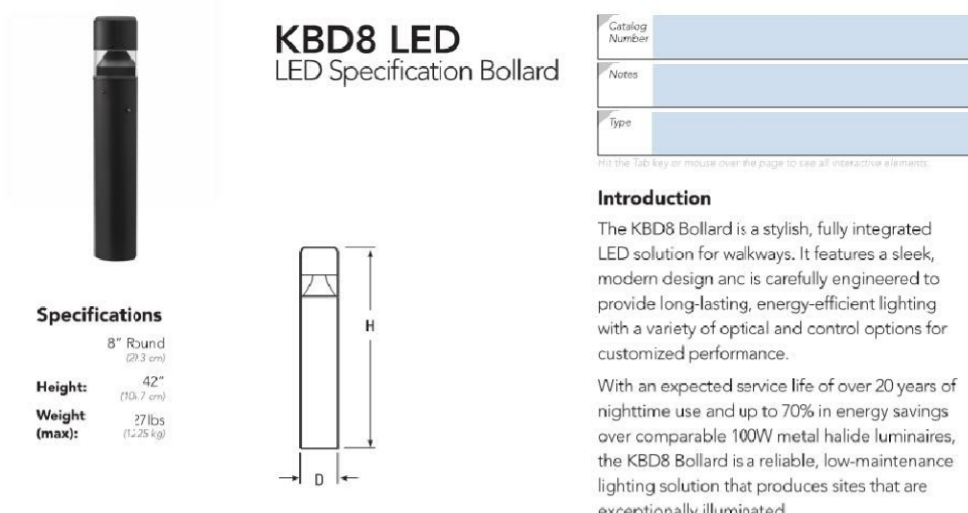


**LANDSCAPE NOTES AND DETAILS**

CHOICE CENTER  
THIRD FILING  
FORT COLLINS, CO 80525

ORIGINAL ISSUE:  
12/07/2016  
KHA PROJECT NO.  
168524002  
SHEET NUMBER  
**5 OF 9**

**LABEL: BL-01**



**Ordering Information** EXAMPLE: KBD8 LED 16C 700 40K SYM MVOLT DBBXD

Code	Qty	Manufacturer	Color Temperature	Beam Spread	Height	Material	Finish	Notes
KBD8 LED	10	16C	700	40K	SYM	MVOLT	DBBXD	

**Accessories**

WST LED	WST LED
---------	---------

One Lithonia Way • Gwynn, Georgia 30221 • Phone: 800.279.8021 • Fax: 770.918.1231 • www.lithonia.com

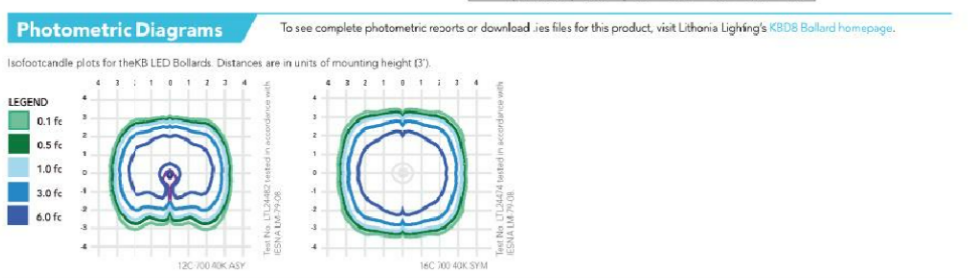
**Performance Data**

Lumen output and efficacy data for the KBD8 LED bollard.

Height	Beam Spread	Beam Diameter	Beam Area	Beam Area Multiplier	Beam Area Multiplier	Beam Area Multiplier	Beam Area Multiplier	Beam Area Multiplier	Beam Area Multiplier	Beam Area Multiplier
1.0	120°	1.0	0.785	1.0	1.0	1.0	1.0	1.0	1.0	1.0

**Projected LED Lumen Maintenance**

Hours	100%	90%	80%	70%	60%	50%	40%	30%	20%	10%
1000	100	95	90	85	80	75	70	65	60	55



**FEATURES & SPECIFICATIONS**

**INTENDED USE:** The KBD8 LED bollard is ideal for illuminating walkways, parking areas, and outdoor spaces. It is designed for use in areas where low-voltage lighting is required.

**CONSTRUCTION:** The KBD8 LED bollard is constructed from high-quality materials and is designed for long-term use. It features a weather-resistant finish and is suitable for outdoor use.

**POWER:** The KBD8 LED bollard is powered by a low-voltage power source. It is designed to be used with a 12VDC power source and is suitable for use in areas where low-voltage lighting is required.

One Lithonia Way • Gwynn, Georgia 30221 • Phone: 800.279.8021 • Fax: 770.918.1231 • www.lithonia.com

**LABEL: SC-01 & SC-02**



**Ordering Information** EXAMPLE: WST LED P1 40K VF MVOLT DBBXD

Code	Qty	Manufacturer	Color Temperature	Beam Spread	Height	Material	Finish	Notes
WST LED	10	P1	40K	VF	MVOLT	DBBXD		

**Accessories**

WST LED	WST LED
---------	---------

One Lithonia Way • Gwynn, Georgia 30221 • Phone: 800.279.8021 • Fax: 770.918.1231 • www.lithonia.com

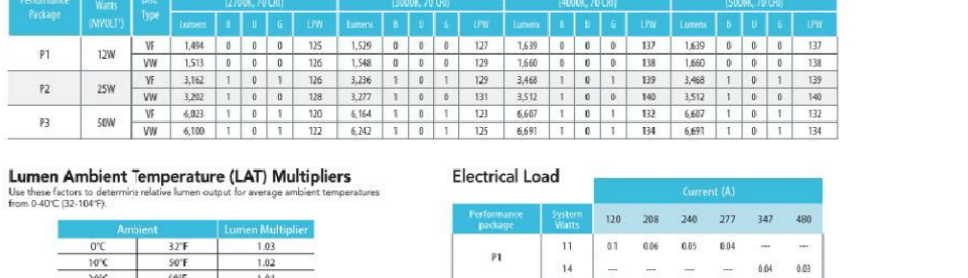
**Performance Data**

Lumen output and efficacy data for the WST LED sconce.

Height	Beam Spread	Beam Diameter	Beam Area	Beam Area Multiplier	Beam Area Multiplier	Beam Area Multiplier	Beam Area Multiplier	Beam Area Multiplier	Beam Area Multiplier	Beam Area Multiplier
1.0	120°	1.0	0.785	1.0	1.0	1.0	1.0	1.0	1.0	1.0

**Projected LED Lumen Maintenance**

Hours	100%	90%	80%	70%	60%	50%	40%	30%	20%	10%
1000	100	95	90	85	80	75	70	65	60	55



**FEATURES & SPECIFICATIONS**

**INTENDED USE:** The WST LED sconce is ideal for illuminating walkways, parking areas, and outdoor spaces. It is designed for use in areas where low-voltage lighting is required.

**CONSTRUCTION:** The WST LED sconce is constructed from high-quality materials and is designed for long-term use. It features a weather-resistant finish and is suitable for outdoor use.

**POWER:** The WST LED sconce is powered by a low-voltage power source. It is designed to be used with a 12VDC power source and is suitable for use in areas where low-voltage lighting is required.

One Lithonia Way • Gwynn, Georgia 30221 • Phone: 800.279.8021 • Fax: 770.918.1231 • www.lithonia.com

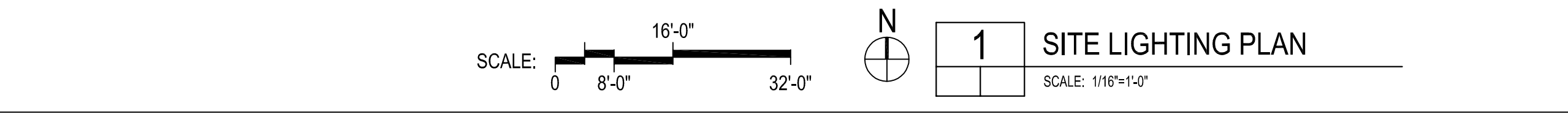
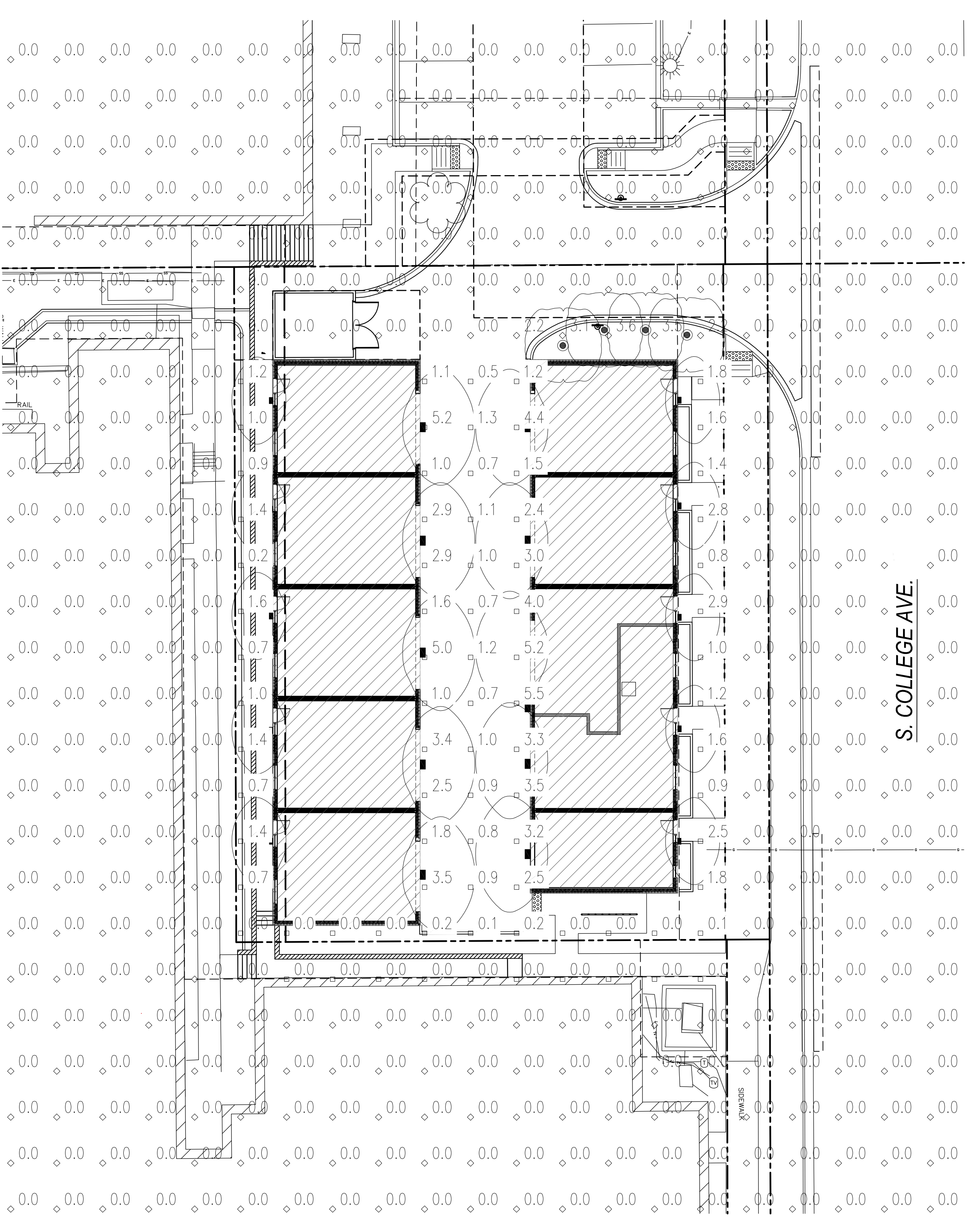
**LUMINAIRE SCHEDULE**

SYMBOL	LABEL	QTY	CATALOG NUMBER	LAMP	LUMENS	WATTS	MOUNTING HEIGHT
■	SC-01	10	WST-LED-P1-30K-Y2-MVOLT-DBLBXD	LED	1548	12	~6'-0"
■	SC-02	10	WST-LED-P2-30K-Y2-MVOLT-DBLBXD	LED	3277	25	~8'-0"
●	BL-01	4	KBD8-LED-16C-350-30K-SYM-MVOLT-DBLBXD	LED	3277	25	~8'-0"

**STATISTICS**

DESCRIPTION	SYMBOL	AVG	MAX	MIN
BLDG. SURROUNDS	□	.5 FC	5.2 FC	0.0 FC
BOUNDARY SPILL	◇	.0 FC	0.2 FC	0.0 FC

**NOTE: ALL LUMINAIRES TO BE 3000K COLOR TEMPERATURE (OR AS CLOSE TO 3000K BASED ON AVAILABILITY)**



**General Notes:**

**PDP SUBMITTAL**

03	05/03/17	PDP SUBMITTAL #3
02	02/08/17	PDP SUBMITTAL #2
01	12/07/16	PDP SUBMITTAL

**Submissions & Revisions**

No.	Date	Description
-----	------	-------------

**Owner:** CORE SPACES  
1643 N. MILWAUKEE AVE, 5TH FLR.  
CHICAGO, IL 60647  
P. 512-584-8601

**General Contractor:**

**Architect:** ANTUNOVICH ASSOCIATES  
ARCHITECTURE - PLANNING - INTERIOR DESIGN  
224 W Huron Street Main: 312.266.1126  
Chicago, Illinois 60654 Fax: 312.266.7123

**Landscape Architect:** KIMLEY-HORN  
1001 WARRENVILLE ROAD,  
SUITE 350  
LISLE, IL 60523  
P. 630-487-5555

**Structural Engineer:**

**M.E.P. & F.P. Engineers:** AE DESIGN  
1900 WAZEE STREET #350  
DENVER, CO 80202  
P. 720-862-3697

**Civil Engineer:** KIMLEY-HORN  
1001 WARRENVILLE ROAD,  
SUITE 350  
LISLE, IL 60523  
P. 630-487-5555

**Project Location:** CHOICE CENTER THIRD FILING  
FORT COLLINS, CO 80525

**Drawing Title:** LIGHTING PLAN

**Seal:** ENTITLEMENT DRAWINGS NOT FOR CONSTRUCTION

**Date:**

**Drawn By:**

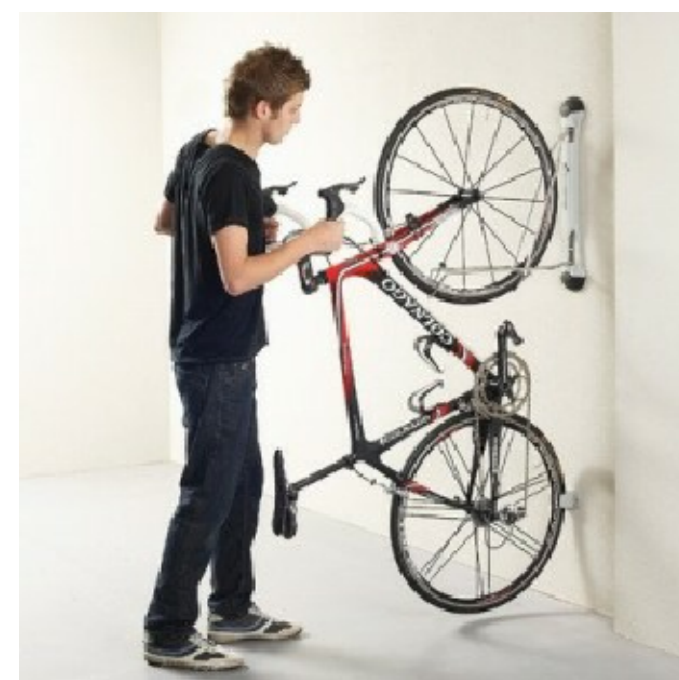
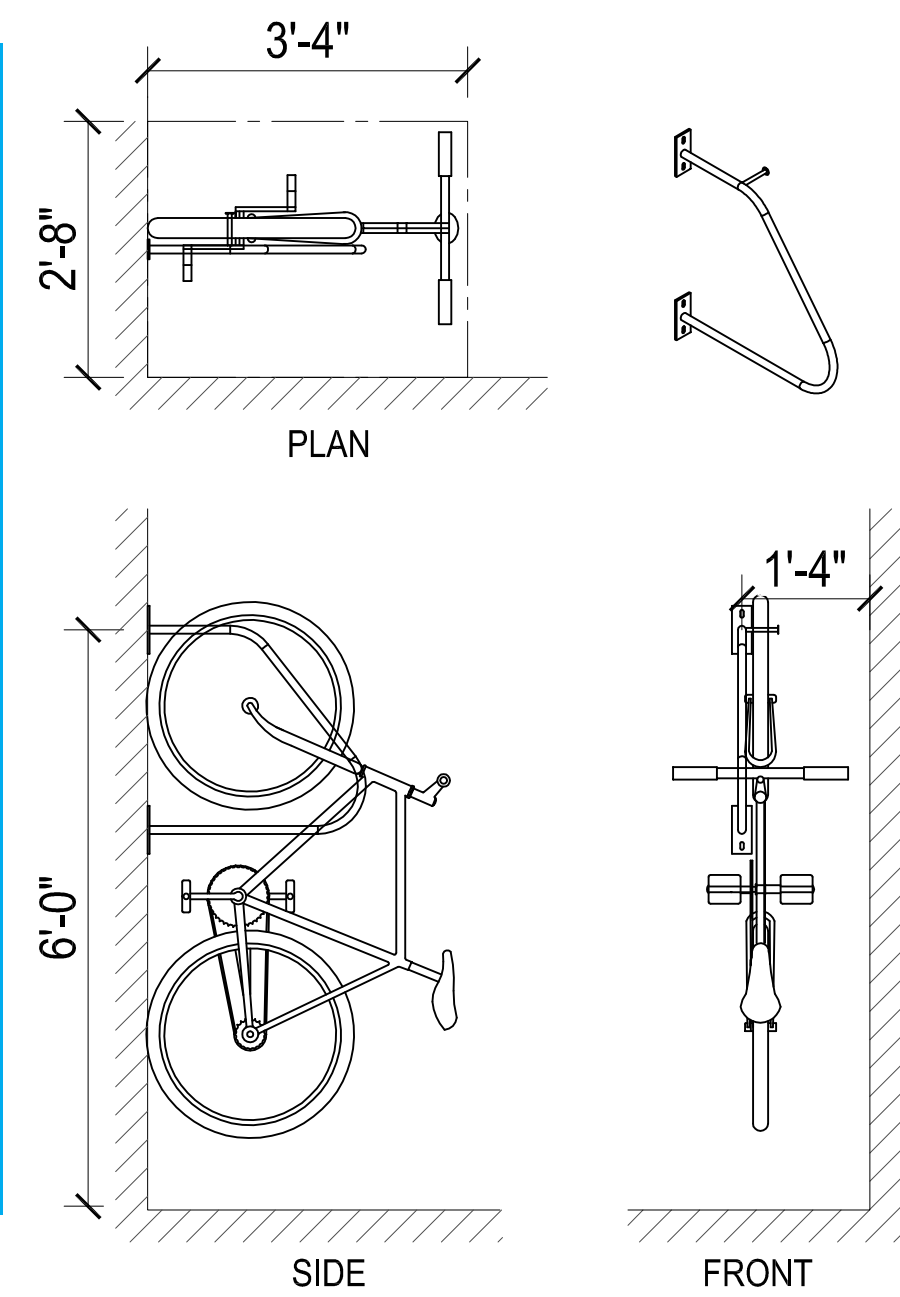
**Checked By:**

**Project No:**

**Drawing No.:** 6 OF 9

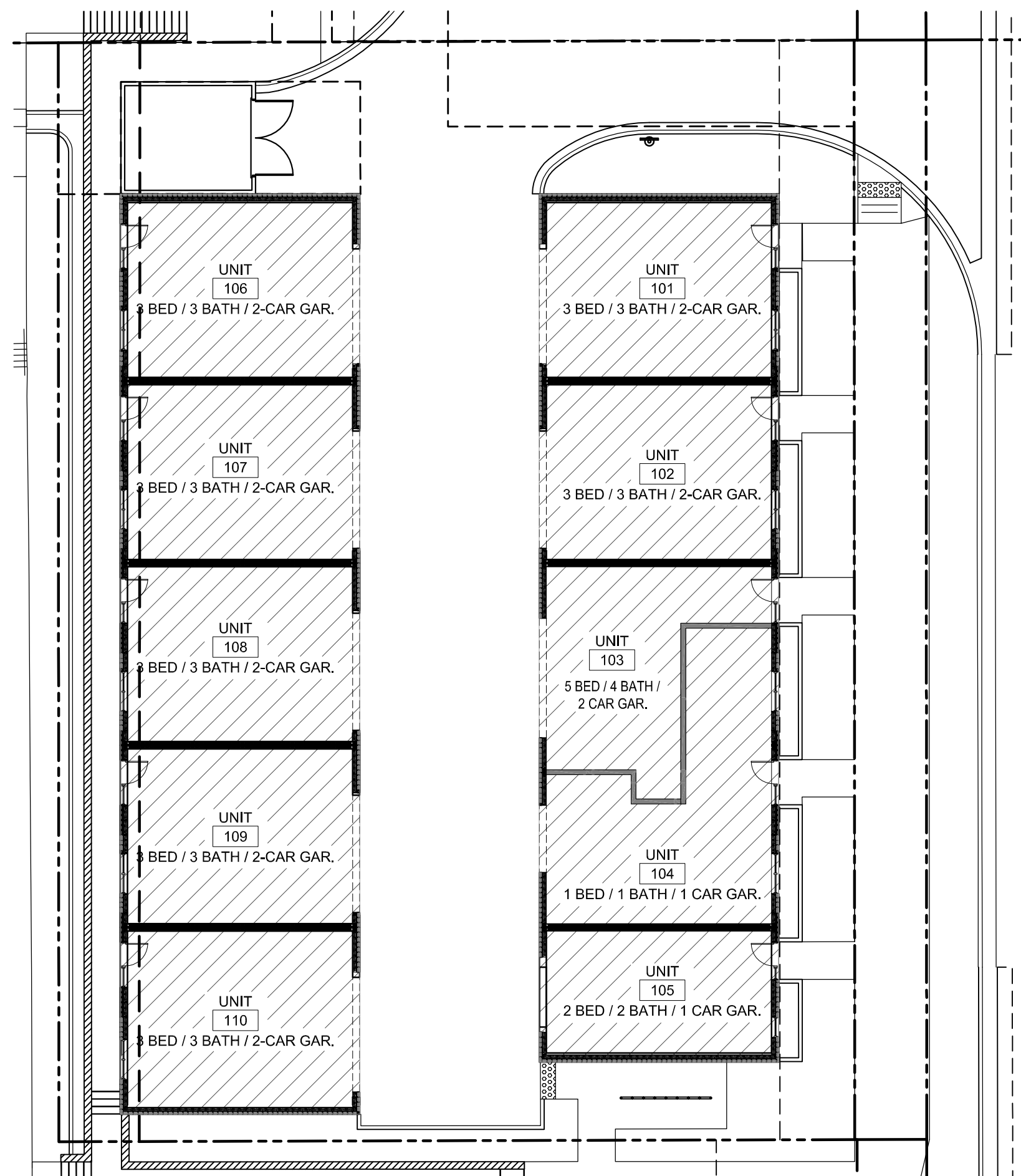


TYPICAL SPACING DIMENSIONS



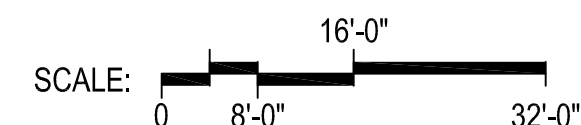
6 ENCLOSED BIKE PARKING EQUIPMENT

SCALE: 1/2"=1'-0"



5 TOWNHOUSE SITE PLAN

SCALE: 1/16"=1'-0"



UNIT TYPE	UNIT COUNT	UNIT MIX %	# BED- ROOMS PER UNIT	TOTAL BEDROOMS	# PARKING SPACES REQUIRED PER UNIT	TOTAL PARKING SPACES REQUIRED	# LONG-TERM GARAGE PARKING SPACES PROVIDED PER UNIT	TOTAL LONG-TERM GARAGE SPACES PROVIDED	ENCLOSED BIKE PARKING PROVIDED PER UNIT	TOTAL BIKE PARKING *
Type-A' 1-Bedroom **	1	10%	1	1	0.75	0.75	1 (van access)	1	1	1
2-Bedroom	1	10%	2	2	1	1	1 (compact)	1	1	1
3-Bedroom	7	70%	3	21	1.25	8.75	2	14	2	14
5-Bedroom	1	10%	5	5	1.5	1.5	2	2	2	2
<b>TOTAL</b>	<b>10</b>	<b>100%</b>	-	<b>29</b>	-	<b>12</b>	-	<b>18</b>	-	<b>18</b>

\*NOTE: 12-Bike Exterior bike rack located at northeast corner of property (see site plan)

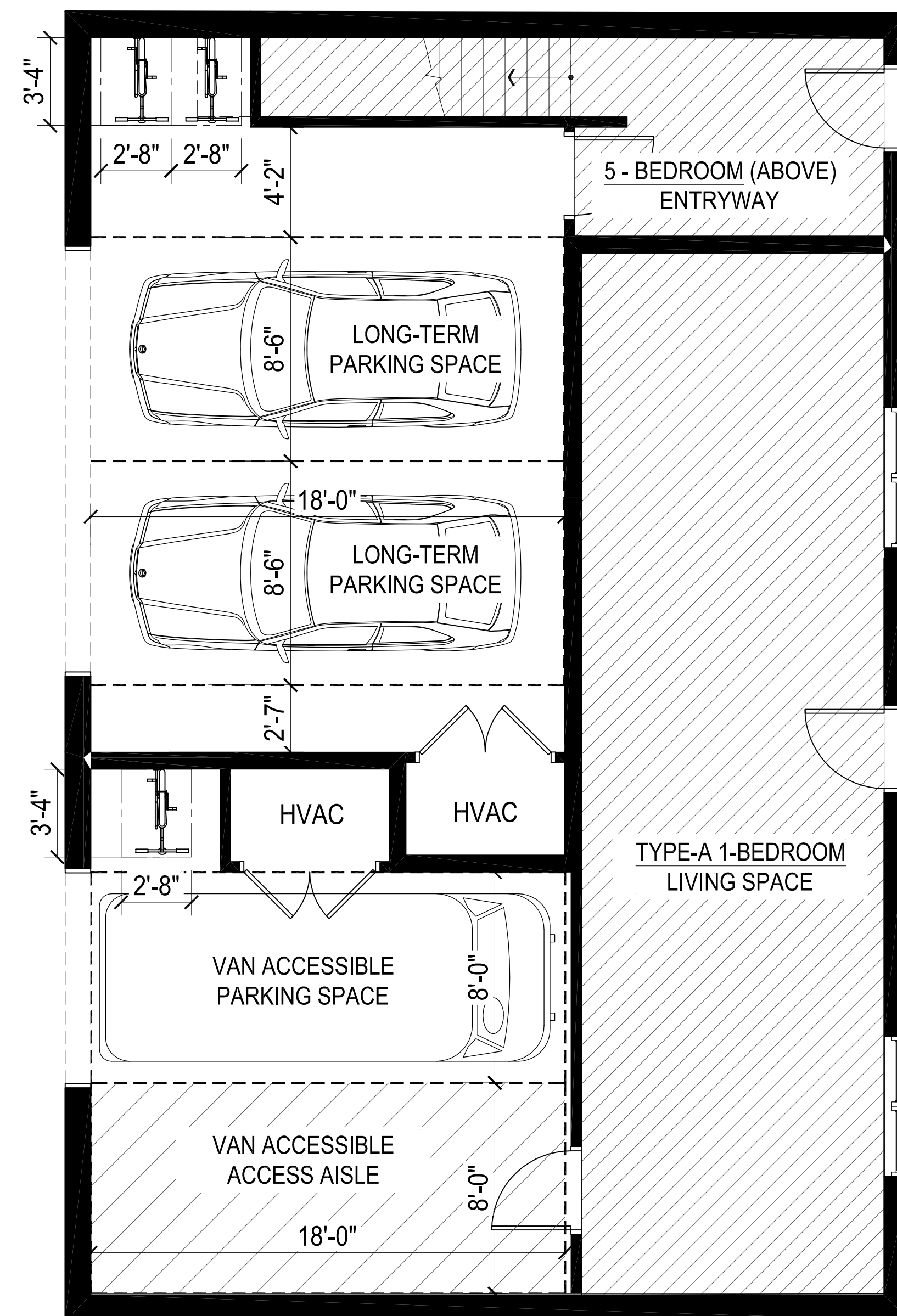
- Total Bike Parking (Interior + Exterior): 30

\*\*NOTE: Per "Colorado Revised Statutes 2013 - Title 9" - A development of 10 units requires 6 'accessibility points'.

- Town home development will achieve required 6 accessibility points by providing (1) Type-A dwelling unit.

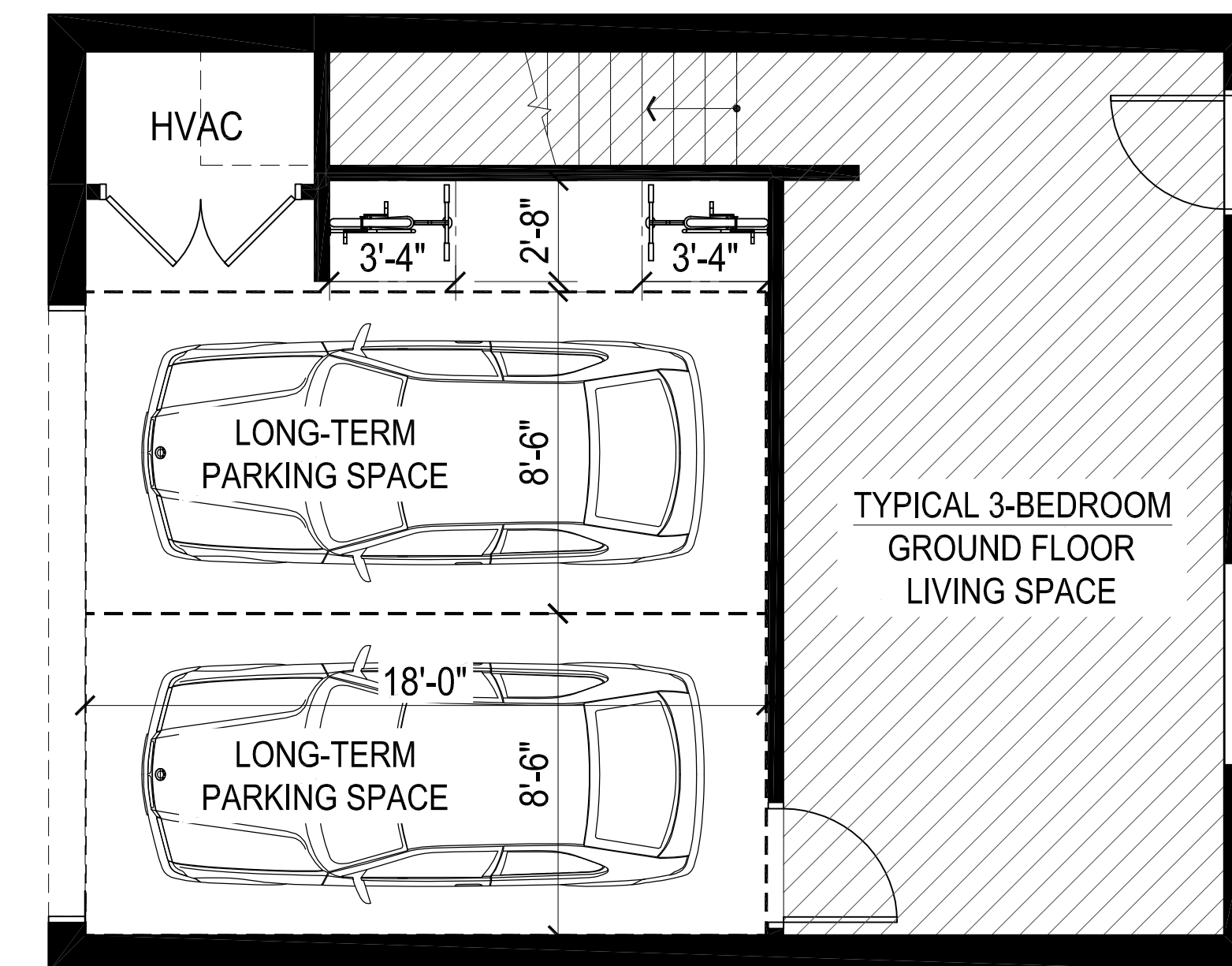
3 APARTMENT MATRIX SUMMARY

SCALE: NTS



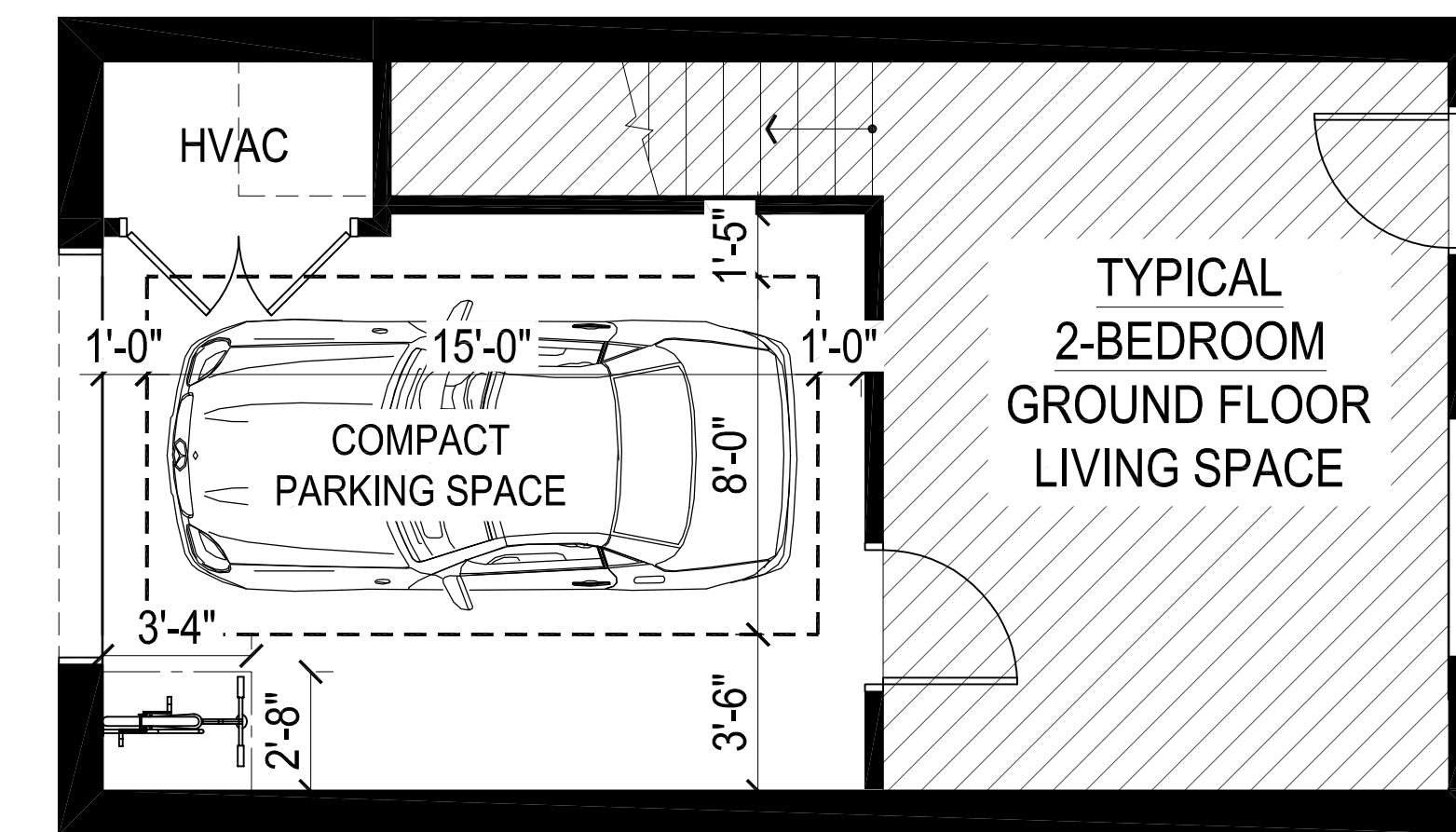
4 UPPER 5-BED APARTMENT & GROUND FLOOR TYPE-A 1-BED APARTMENT - GARAGE LAYOUT

SCALE: 1/4"=1'-0"



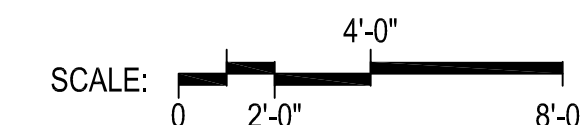
2 TYPICAL 3-BED UNIT - GARAGE LAYOUT

SCALE: 1/4"=1'-0"



1 2-BED UNIT - GARAGE LAYOUT

SCALE: 1/4"=1'-0"



General Notes:

PDP SUBMITTAL

No.	Date	Description
04	08/02/17	PDP SUBMITTAL FINAL UPDATE
03	05/03/17	PDP SUBMITTAL #3
02	02/08/17	PDP SUBMITTAL #2
01	12/07/16	PDP SUBMITTAL

Submissions & Revisions

Owner  
**CORE SPACES**  
1643 N. MILWAUKEE AVE, 5TH FLR.  
CHICAGO, IL 60647  
P. 512-584-8601

General Contractor

Architect  
**ANTUNOVICH ASSOCIATES**  
ARCHITECTURE - PLANNING - INTERIOR DESIGN

224 W Huron Street Main: 312.266.1126  
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Landscape Architect

**KIMLEY-HORN**  
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SUITE 350  
LISLE, IL 60523  
P. 630-487-5555

Structural Engineer

M.E.P. & F.P. Engineers

**AE DESIGN**  
1900 WAZEE STREET #350  
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P. 720-862-3697

Civil Engineer

**KIMLEY-HORN**  
1001 WARRENVILLE ROAD,  
SUITE 350  
LISLE, IL 60523  
P. 630-487-5555

Project Location

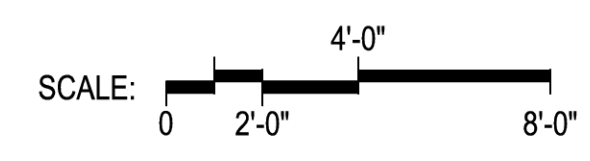
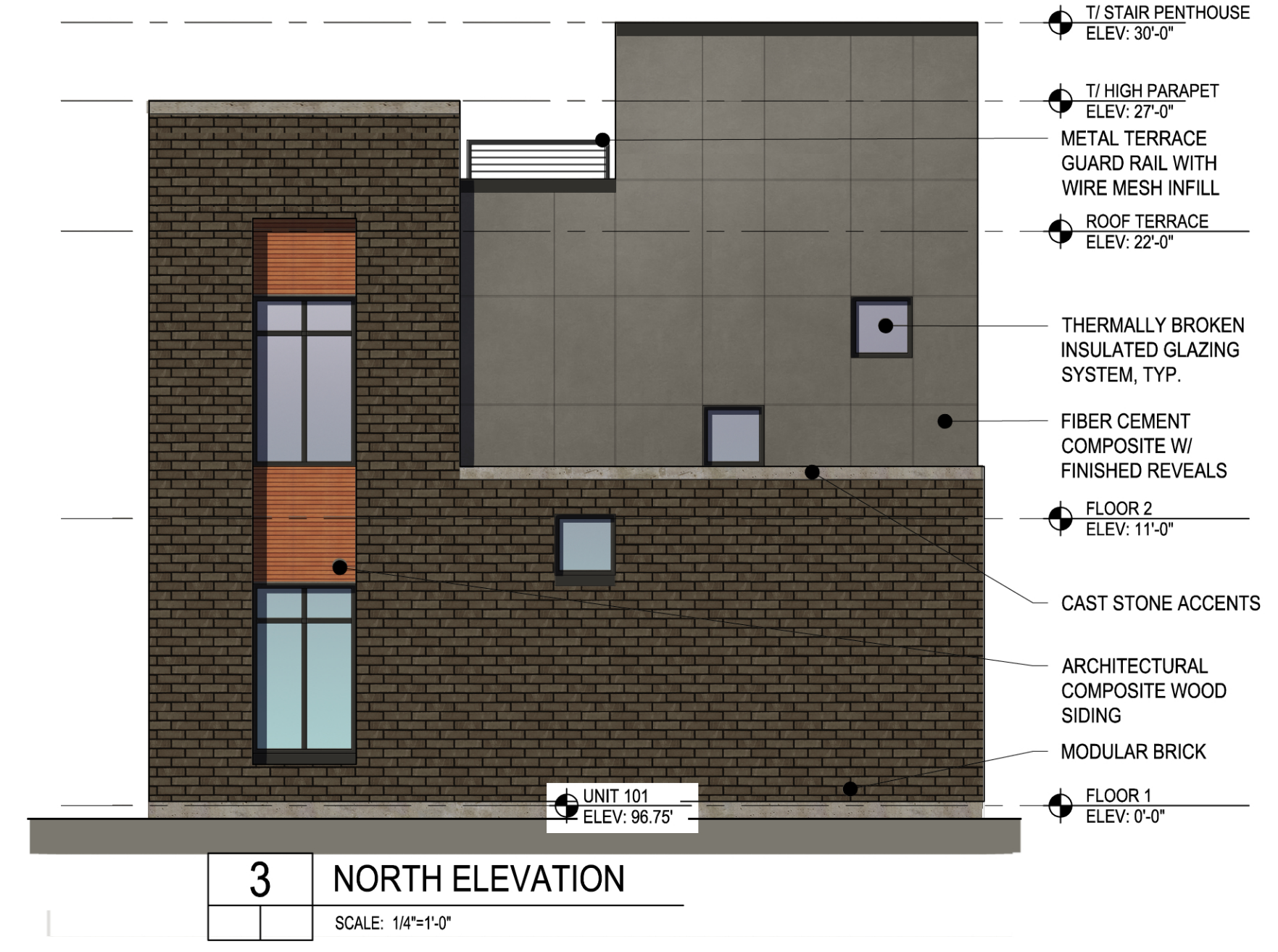
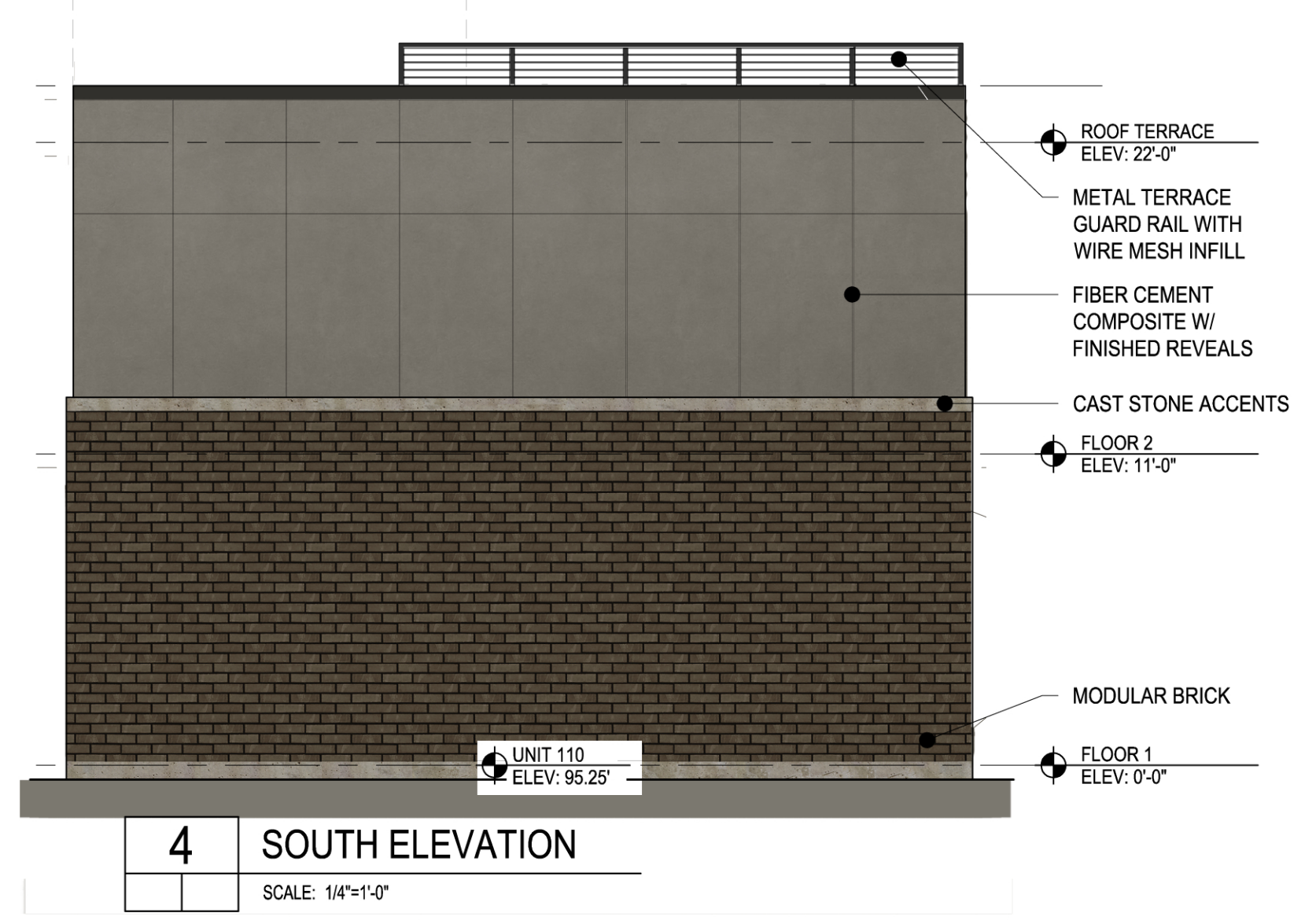
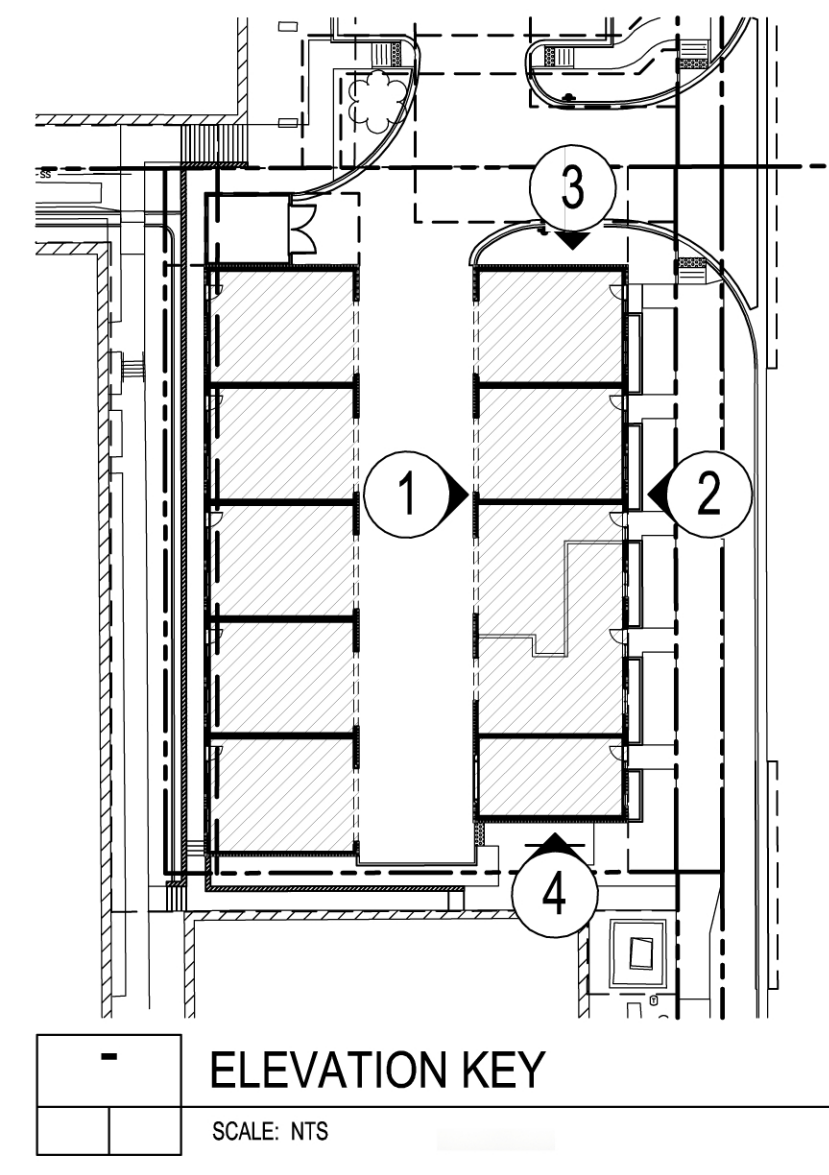
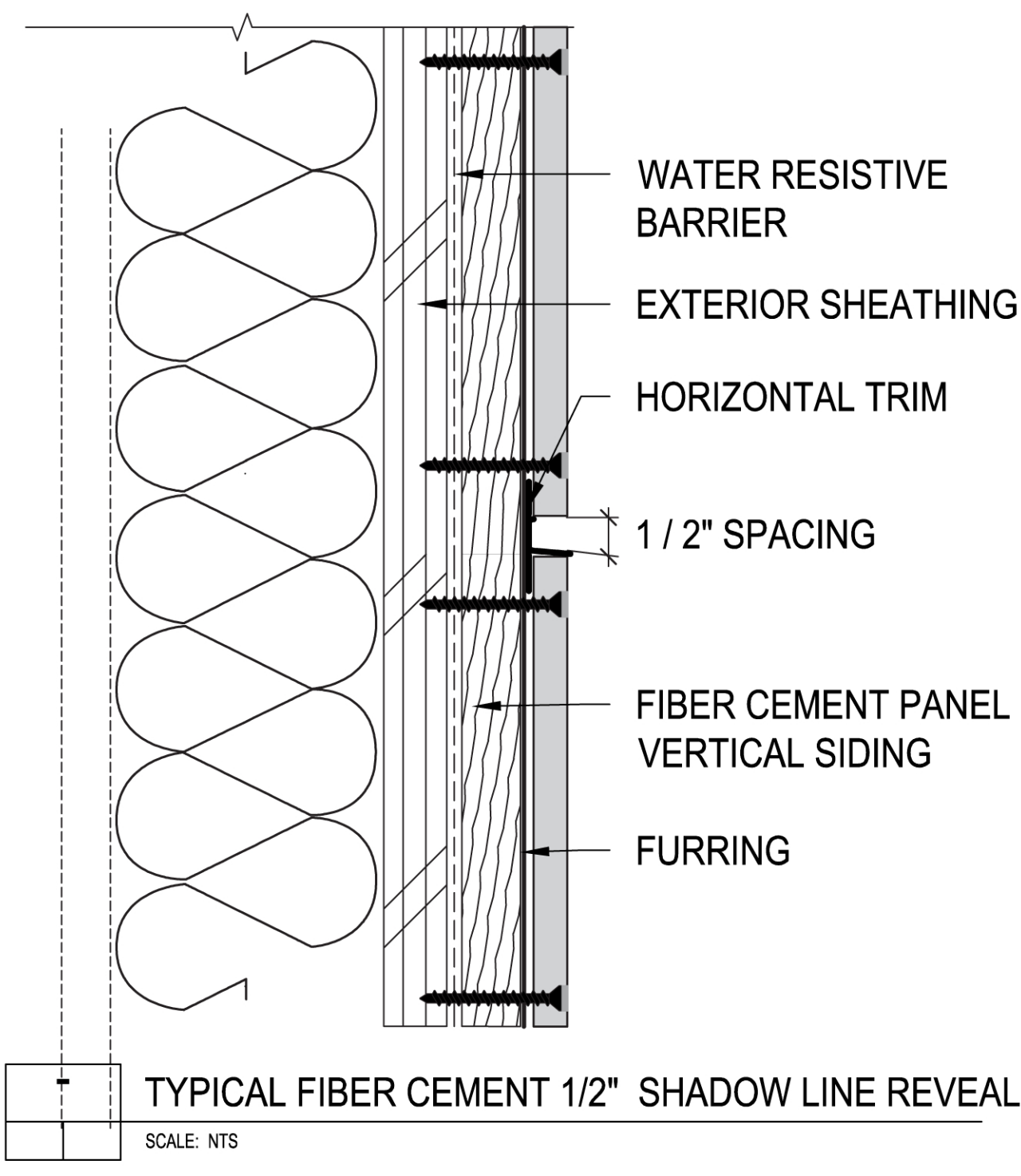
**CHOICE CENTER THIRD FILING**  
FORT COLLINS, CO 80525

Drawing Title

UNIT SUMMARY & ENLARGED PARKING PLANS

ENTITLEMENT DRAWINGS NOT FOR CONSTRUCTION	Date:
	Drawn By:
	Checked By:
	Project No:

Drawing No.



General Notes:

**PDP SUBMITTAL**

No.	Date	Description
04	06/02/17	PDP SUBMITTAL FINAL UPDATE
03	05/03/17	PDP SUBMITTAL #3
02	02/08/17	PDP SUBMITTAL #2
01	12/07/16	PDP SUBMITTAL

Submissions & Revisions

Owner  
**CORE SPACES**  
1643 N. MILWAUKEE AVE, 5TH FLR.  
CHICAGO, IL 60647  
P. 512-584-8601

General Contractor

Architect  
**ANTUNOVICH ASSOCIATES**  
ARCHITECTURE - PLANNING - INTERIOR DESIGN  
224 W Huron Street Main: 312.266.1126  
Chicago, Illinois 60654 Fax: 312.266.7123

Landscape Architect  
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1001 WARRENVILLE ROAD,  
SUITE 350  
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P. 630-487-5555

Structural Engineer

M.E.P. & F.P. Engineers  
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DENVER, CO 80202  
P. 720-862-3697

Civil Engineer  
**KIMLEY-HORN**  
1001 WARRENVILLE ROAD,  
SUITE 350  
LISLE, IL 60523  
P. 630-487-5555

Project Location  
**CHOICE CENTER THIRD FILING**  
FORT COLLINS, CO 80525

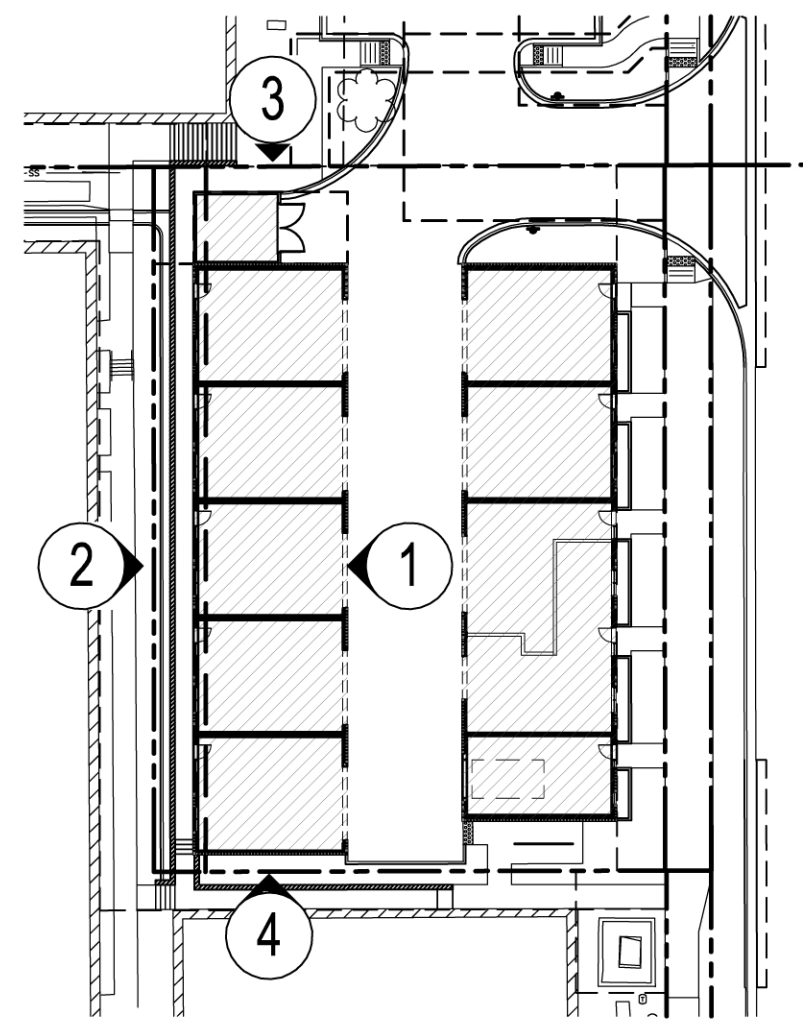
Drawing Title  
**EASTERN ELEVATIONS**

Seal  
ENTIREMENT DRAWINGS NOT FOR CONSTRUCTION

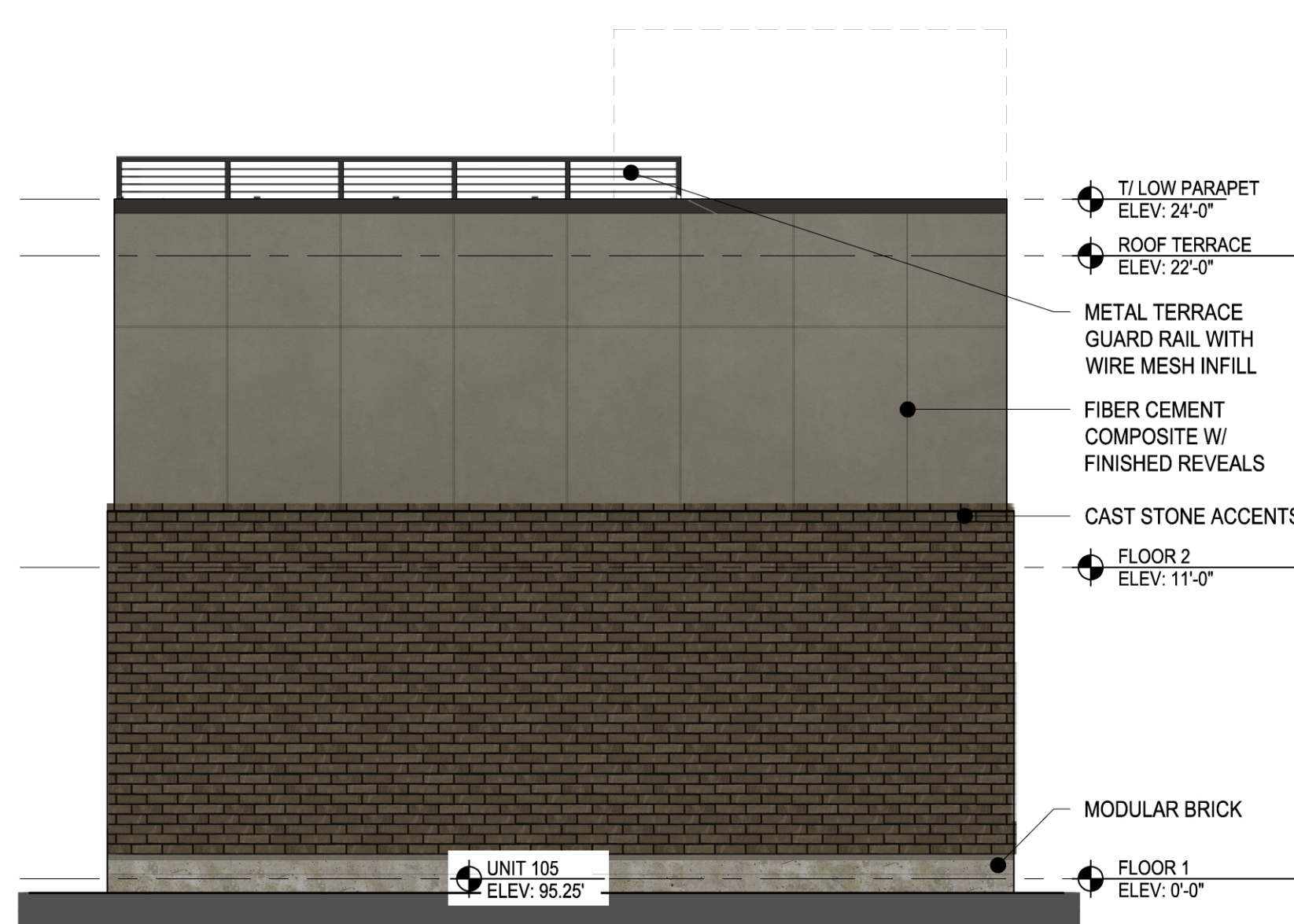
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Project No:

Drawing No. **A1**

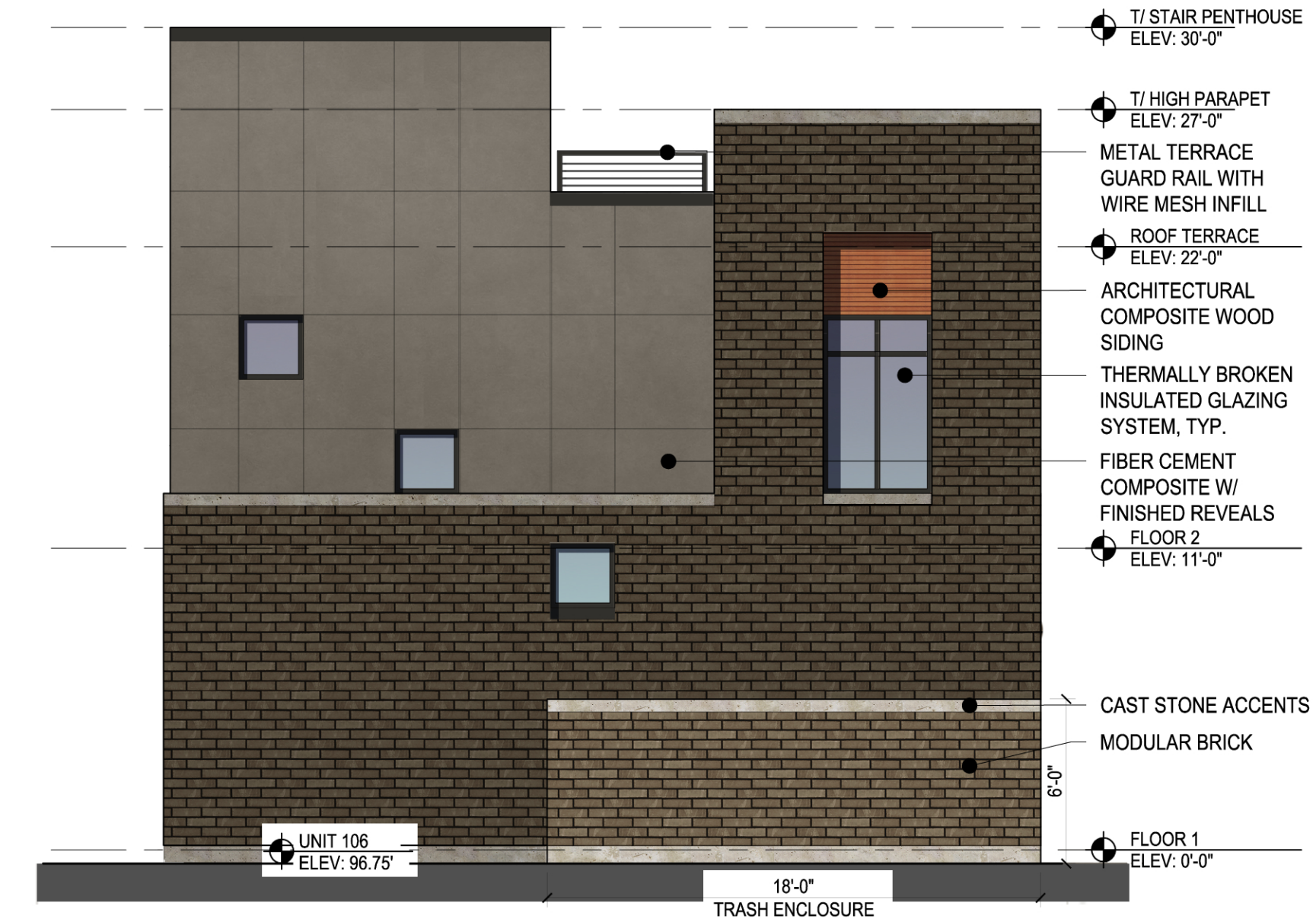
Copyright 2015



**ELEVATION KEY**  
SCALE: NTS



**4 SOUTH ELEVATION**  
SCALE: 1/4"=1'-0"



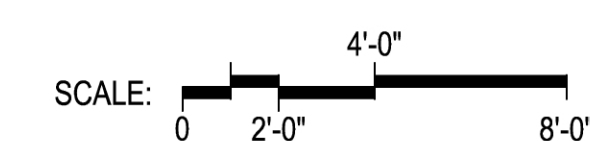
**3 NORTH ELEVATION**  
SCALE: 1/4"=1'-0"



**2 WEST ELEVATION - FACING ADJACENT APT. BLDG.**  
SCALE: 1/4"=1'-0"



**1 EAST ELEVATION - FACING INNER DRIVEWAY**  
SCALE: 1/4"=1'-0"



General Notes:

**PDP SUBMITTAL**

04	06/02/17	PDP SUBMITTAL FINAL UPDATE
03	05/03/17	PDP SUBMITTAL #3
02	02/08/17	PDP SUBMITTAL #2
01	12/07/16	PDP SUBMITTAL

Submissions & Revisions

Owner: **CORE SPACES**  
1643 N. MILWAUKEE AVE, 5TH FLR.  
CHICAGO, IL 60647  
P. 512-584-8601

General Contractor:

Architect: **ANTUNOVICH ASSOCIATES**  
ARCHITECTURE - PLANNING - INTERIOR DESIGN  
224 W Huron Street Main: 312.266.1126  
Chicago, Illinois 60654 Fax: 312.266.7123

Landscape Architect: **KIMLEY-HORN**  
1001 WARRENVILLE ROAD,  
SUITE 350  
LISLE, IL 60523  
P. 630-487-5555

Structural Engineer:

M.E.P. & F.P. Engineers: **AE DESIGN**  
1900 WAZEE STREET #350  
DENVER, CO 80202  
P. 720-862-3697

Civil Engineer: **KIMLEY-HORN**  
1001 WARRENVILLE ROAD,  
SUITE 350  
LISLE, IL 60523  
P. 630-487-5555

Project Location: **CHOICE CENTER THIRD FILING**  
FORT COLLINS, CO 80525

Drawing Title: **WESTERN ELEVATIONS**

Seal: **ENTITLEMENT DRAWINGS NOT FOR CONSTRUCTION**

Date: \_\_\_\_\_  
Drawn By: \_\_\_\_\_  
Checked By: \_\_\_\_\_  
Project No: \_\_\_\_\_

Drawing No. **A2**

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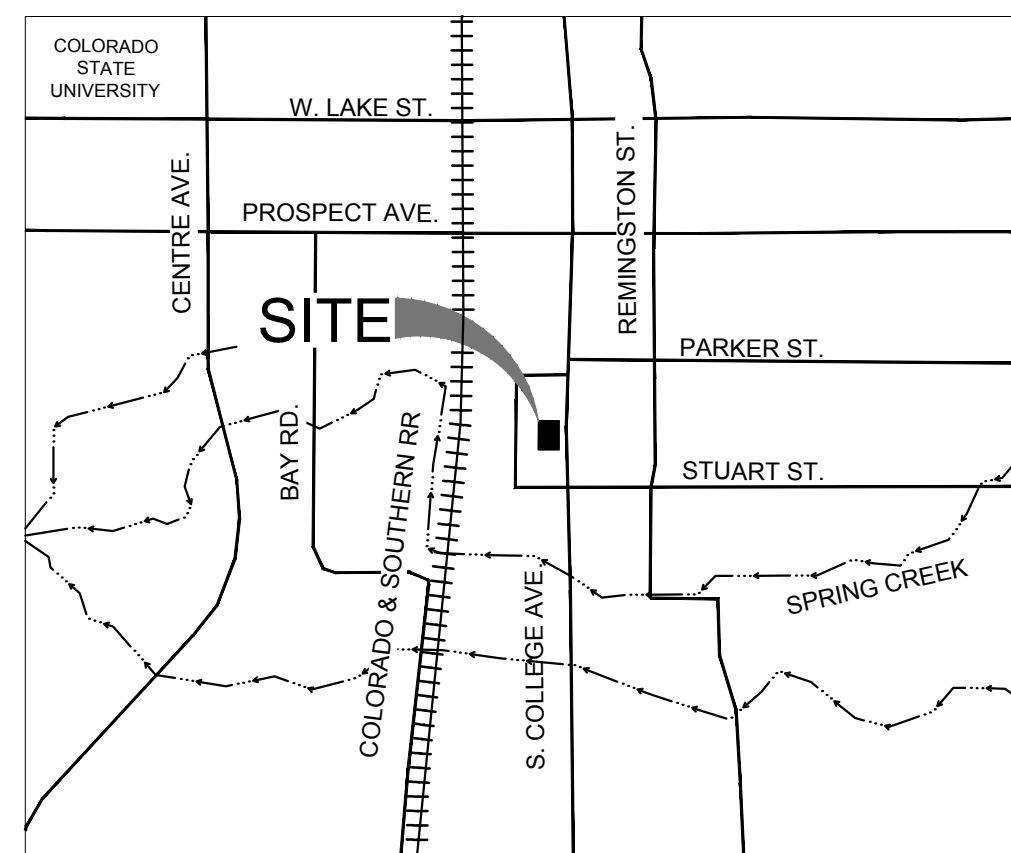


Know what's below.  
Call before you dig.

# UTILITY PLANS FOR CHOICE CENTER THIRD FILING

BEING A REPLAT OF LOT 2, CHOICE CENTER SUBDIVISION AND A PORTION OF LOT 3, FOX SHOPPING CENTER SUBDIVISION  
LOCATED WITHIN THE NORTHEAST QUARTER (NE 1/4) OF SECTION 23, TOWNSHIP 7 NORTH, RANGE 69 WEST OF THE SIXTH PRINCIPAL MERIDIAN  
CITY OF FORT COLLINS AND COUNTY OF LARIMER, STATE OF COLORADO.

JUNE 2017



VICINITY MAP  
1" = 1000'

## CONTACT INFORMATION

### PROJECT TEAM:

**OWNER**  
MARK GOEHAUSEN  
CORE FORT COLLINS LLC  
2234 W. NORTH AVENUE  
CHICAGO, IL 60647  
TEL: (773) 969-5748

### APPLICANT

MARK GOEHAUSEN  
CORE FORT COLLINS LLC  
2234 W. NORTH AVENUE  
CHICAGO, IL 60647  
TEL: (773) 969-5748

### ARCHITECT

JOE ANTUNOVICH  
ANTUNOVICH ASSOCIATES  
224 W. HURON STREET  
CHICAGO, IL 60654  
TEL: (312) 266-1126

### LANDSCAPE ARCHITECT

KEITH DEMCHINSKI  
KIMLEY-HORN AND ASSOCIATES, INC.  
4582 SOUTH ULSTER STREET  
SUITE 1500  
DENVER, CO 80237  
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### SITE ENGINEER

LESLEY NETER  
KIMLEY-HORN AND ASSOCIATES, INC.  
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DENVER, CO 80237  
TEL: (303) 487-5555

### SURVEYOR

RICHARD B. GABRIEL  
POWER SURVEYING COMPANY, INC.  
150 W. 84TH AVE.  
THORNTON, CO. 80260  
DATE: 11/11/2016  
REVISED: 11/15/2016  
REVISED: 12/14/2016  
REVISED: 01/30/2017  
REVISED: 02/07/2017  
REVISED: 03/06/2017

### TRAFFIC ENGINEER

CURTIS ROWE  
KIMLEY-HORN AND ASSOCIATES, INC.  
4582 SOUTH ULSTER STREET  
SUITE 1500  
DENVER, CO 80237  
TEL: (303) 228-2304

### GEOTECHNICAL ENGINEER

DAVID RICHER  
EARTH ENGINEERING CONSULTANTS, LLC  
4396 GREENFIELD DRIVE  
WINDSOR, CO 80550  
TEL: (970) 545-3908

### UTILITY CONTACTS:

#### GAS

TERRY STENCIL  
XCEL ENERGY  
TEL: (970) 225-7857

#### ELECTRIC

TODD VEDDER  
CITY OF FORT COLLINS LIGHT & POWER  
TEL: (970) 224-6152

#### CABLE

DON KAPPERMAN  
COMCAST  
TEL: (970) 567-0425

#### TELECOMMUNICATIONS

WILLIAM JOHNSON  
CENTURYLINK  
TEL: (970) 377-6401

#### WATER

WES LAMAR-UE  
CITY OF FORT COLLINS  
TEL: (970) 416-2418

#### WASTEWATER

WES LAMAR-UE  
CITY OF FORT COLLINS  
TEL: (970) 416-2418

#### STORMWATER

WES LAMAR-UE  
CITY OF FORT COLLINS  
TEL: (970) 416-2418

## SHEET INDEX

- 1 OF 12 COVER SHEET
- 2 OF 12 GENERAL NOTES
- 3 OF 12 DEMOLITION PLAN
- 4 OF 12 EROSION CONTROL PLAN
- 5 OF 12 EROSION CONTROL NOTES & DETAILS
- 6 OF 12 EXISTING DRAINAGE PLAN
- 7 OF 12 PROPOSED DRAINAGE PLAN
- 8 OF 12 GRADING PLAN
- 9 OF 12 UTILITY PLAN
- 10 OF 12 CONSTRUCTION DETAILS
- 11 OF 12 CONSTRUCTION DETAILS
- 12 OF 12 CONSTRUCTION DETAILS

### PROJECT BENCHMARKS:

PROJECT DATUM: NAVD88

BENCHMARK #S402 AT THE JUNCTION OF WEST PROSPECT STREET AND THE COLORADO AND SOUTHERN RAILROAD, 279.9 FEET NORTH OF THE CENTERLINE OF THE WEST BOUND LANES OF THE STREET, 107.0 FEET NORTH OF THE CENTER OF A PRIVATE DRIVE, 32.5 FEET WEST OF THE NEAR RAIL, AND 2.3 FEET SOUTH OF A UTILITY POLE.  
ELEVATION: 4,997.54

BENCHMARK #A105 AT COLORADO A. AND M. COLLEGE, 43 FEET NORTH OF THE SOUTHEAST CORNER OF THE FIELD HOUSE, 19 FEET SOUTH OF THE CENTER OF THE EAST ENTRANCE, 3 FEET ABOVE THE SIDEWALK, SET VERTICALLY IN THE EAST WALL OF THE FIELD HOUSE.  
ELEVATION = 4989.42

PLEASE NOTE: THIS PLAN SET IS USING NAVD88 FOR A VERTICAL DATUM. SURROUNDING DEVELOPMENTS HAVE USED NGVD29 UNADJUSTED FOR THEIR VERTICAL DATUMS.

IF NGVD29 UNADJUSTED DATUM IS REQUIRED FOR ANY PURPOSE THE FOLLOWING EQUATION SHOULD BE USED: NGVD29 UNADJUSTED = NAVD88 - 3.17'

### ORIGINAL FIELD SURVEY:

RICHARD B. GABRIEL  
POWER SURVEYING COMPANY, INC.  
150 W. 84TH AVE.  
THORNTON, CO. 80260  
DATE: 11/11/2016  
REVISED: 11/15/2016  
REVISED: 12/14/2016  
REVISED: 01/30/2017  
REVISED: 02/07/2017  
REVISED: 03/06/2017

### SUBSURFACE EXPLORATION BY:

DAVID RICHER  
EARTH ENGINEERING CONSULTANTS, LLC  
4396 GREENFIELD DRIVE  
WINDSOR, CO 80550

### DISCLAIMER STATEMENT:

THESE PLANS HAVE BEEN REVIEWED BY THE CITY OF FORT COLLINS FOR CONCEPT ONLY. THE REVIEW DOES NOT IMPLY RESPONSIBILITY BY THE REVIEWING DEPARTMENT, THE CITY OF FORT COLLINS ENGINEER, OR THE CITY OF FORT COLLINS FOR ACCURACY AND CORRECTNESS OF THE CALCULATIONS. FURTHERMORE, THE REVIEW DOES NOT IMPLY THAT QUANTITIES OF ITEMS ON THE PLANS ARE THE FINAL QUANTITIES REQUIRED. THE REVIEW SHALL NOT BE CONSTRUED FOR ANY REASON AS ACCEPTANCE OF FINANCIAL RESPONSIBILITY BY THE CITY OF FORT COLLINS FOR ADDITIONAL QUANTITIES OF ITEMS SHOWN THAT MAY BE REQUIRED DURING THE CONSTRUCTION PHASE.

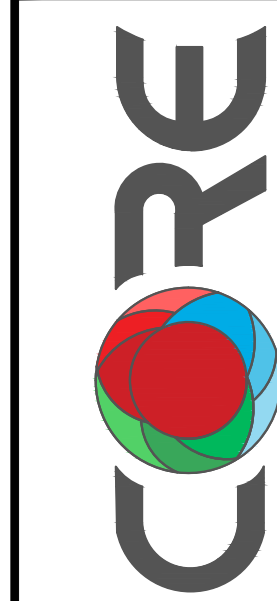
### CERTIFICATION STATEMENT:

I HEREBY AFFIRM THAT THESE FINAL CONSTRUCTION PLANS WERE PREPARED UNDER MY DIRECT SUPERVISION, IN ACCORDANCE WITH ALL APPLICABLE CITY OF FORT COLLINS AND STATE OF COLORADO STANDARDS AND STATUTES, RESPECTIVELY, AND THAT I AM FULLY RESPONSIBLE FOR THE ACCURACY OF ALL DESIGN, REVISIONS, AND RECORD CONDITIONS THAT I HAVE NOTED ON THESE PLANS.

NO.	PDP COMMENTS/SITE PLAN REVISION	DATE	BY
1		02/07/17	LEN
2		05/02/17	LEN
3		06/09/17	LEN
REVISIONS			



SCALE: AS NOTED  
DESIGNED BY: JPM  
DRAWN BY: JPM  
CHECKED BY: LEN



COVER SHEET

CHOICE CENTER  
THIRD FILING  
FORT COLLINS, CO 80525

ORIGINAL ISSUE:  
12/07/2016  
KHA PROJECT NO.  
168524002

SHEET NUMBER  
1 OF 12

CITY OF FORT COLLINS UTILITY PLAN APPROVAL		
APPROVED:	_____ CITY ENGINEER _____	DATE _____
CHECKED BY:	_____ WATER AND WASTEWATER UTILITY _____	DATE _____
CHECKED BY:	_____ STORMWATER UTILITY _____	DATE _____
CHECKED BY:	_____ PARKS AND RECREATION _____	DATE _____
CHECKED BY:	_____ TRAFFIC ENGINEER _____	DATE _____
CHECKED BY:	_____	DATE _____

Drawing name: K:\GIS\_LIVE\16840002\_CORE-FortCollins\_CO\_Collage\2\_Design\CAD\PlanSheets\UTILITY PLAN\2 OF 12 - GENERAL NOTES.dwg 2 OF 12 GENERAL NOTES Jun 09, 2017 11:30am By: steven.paine  
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GENERAL NOTES

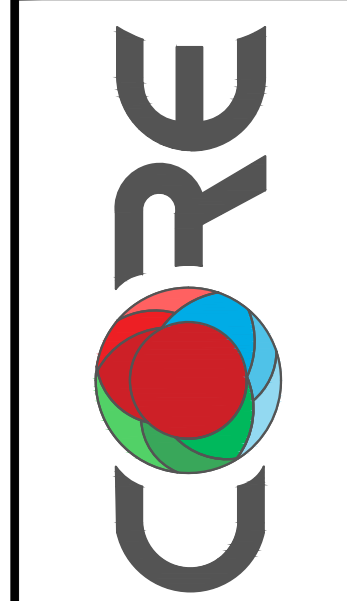
- 1. ALL MATERIALS, WORKMANSHIP, AND CONSTRUCTION OF PUBLIC IMPROVEMENTS SHALL MEET OR EXCEED THE STANDARDS AND SPECIFICATIONS SET FORTH IN THE LARIMER COUNTY URBAN AREA STREET STANDARDS AND APPLICABLE STATE AND FEDERAL REGULATIONS. WHERE THERE IS CONFLICT BETWEEN THESE PLANS AND THE SPECIFICATIONS, OR ANY APPLICABLE STANDARDS, THE MOST RESTRICTIVE STANDARD SHALL APPLY. ALL WORK SHALL BE INSPECTED AND APPROVED BY THE LOCAL ENTITY.
2. ALL REFERENCES TO ANY PUBLISHED STANDARDS SHALL REFER TO THE LATEST REVISION OF SAID STANDARD, UNLESS SPECIFICALLY STATED OTHERWISE.
3. THESE PUBLIC IMPROVEMENT CONSTRUCTION PLANS SHALL BE VALID FOR A PERIOD OF THREE YEARS FROM THE DATE OF APPROVAL BY THE LOCAL ENTITY ENGINEER. USE OF THESE PLANS AFTER THE EXPIRATION DATE WILL REQUIRE A NEW REVIEW AND APPROVAL PROCESS BY THE LOCAL ENTITY PRIOR TO COMMENCEMENT OF ANY WORK SHOWN IN THESE PLANS.
... [items 4-39] ...

- 40. THE DESIGNER SHALL PROVIDE, IN THIS LOCATION ON THE PLAN, THE LOCATION AND DESCRIPTION OF THE NEAREST SURVEY BENCHMARKS (2) FOR THE PROJECT AS WELL AS THE BASIS OF BEARINGS. THE INFORMATION SHALL BE AS FOLLOWS:  
PROJECT DATUM: NAVD88  
BENCHMARK #5402 AT THE JUNCTION OF WEST PROSPECT STREET AND THE COLORADO AND SOUTHERN RAILROAD, 279.9 FEET NORTH OF THE CENTERLINE OF THE WEST BOUND LANES OF THE STREET, 107.0 FEET NORTH OF THE CENTER OF A PRIVATE DRIVE, 32.5 FEET WEST OF THE NEAR RAIL, AND 2.3 FEET SOUTH OF A UTILITY POLE.  
ELEVATION: 4.997.54  
BENCHMARK #A105 AT COLORADO A. AND M. COLLEGE, 43 FEET NORTH OF THE SOUTHEAST CORNER OF THE FIELD HOUSE, 19 FEET SOUTH OF THE CENTER OF THE EAST ENTRANCE, 3 FEET ABOVE THE SIDEWALK, SET VERTICALLY IN THE EAST WALL OF THE FIELD HOUSE.  
ELEVATION = 4899.42  
PLEASE NOTE: THIS PLAN SET IS USING NAVD88 FOR A VERTICAL DATUM. SURROUNDING DEVELOPMENTS HAVE USED NGVD29 UNADJUSTED FOR THEIR VERTICAL DATUMS.  
IF NGVD29 UNADJUSTED DATUM IS REQUIRED FOR ANY PURPOSE THE FOLLOWING EQUATION SHOULD BE USED: NGVD29 UNADJUSTED = NAVD88 - 3.17'.
41. ALL STATIONING IS BASED ON FLOWLINE OF ROADWAYS UNLESS OTHERWISE NOTED.
42. DAMAGED CURB, GUTTER AND SIDEWALK EXISTING PRIOR TO CONSTRUCTION, AS WELL AS EXISTING FENCES, TREES, STREETS, SIDEWALKS, CURBS AND GUTTERS, LANDSCAPING, STRUCTURES, AND IMPROVEMENTS DESTROYED, DAMAGED OR REMOVED DUE TO CONSTRUCTION OF THIS PROJECT, SHALL BE REPLACED OR RESTORED IN LIKE KIND AT THE DEVELOPER'S EXPENSE, UNLESS OTHERWISE INDICATED ON THESE PLANS, PRIOR TO THE ACCEPTANCE OF COMPLETED IMPROVEMENTS AND/OR PRIOR TO THE ISSUANCE OF THE FIRST CERTIFICATE OF OCCUPANCY.
... [items 43-58] ...

- 5. FT. COLLINS ONLY. VALVE BOXES AND MANHOLES ARE TO BE BROUGHT UP TO GRADE AT THE TIME OF PAVEMENT PLACEMENT OR OVERLAY. VALVE BOX ADJUSTING RINGS ARE NOT ALLOWED.
6. WHEN AN EXISTING ASPHALT STREET MUST BE CUT, THE STREET MUST BE RESTORED TO A CONDITION EQUAL TO OR BETTER THAN ITS ORIGINAL CONDITION. THE EXISTING STREET CONDITION SHALL BE DOCUMENTED BY THE INSPECTOR BEFORE ANY CUTS ARE MADE. CUTTING AND PATCHING SHALL BE DONE IN CONFORMANCE WITH CHAPTER 25, RECONSTRUCTION AND REPAIR. THE FINISHED PATCH SHALL BLEND SMOOTHLY INTO THE EXISTING SURFACE. THE DETERMINATION OF NEED FOR A COMPLETE OVERLAY SHALL BE MADE BY THE LOCAL ENTITY ENGINEER. ALL OVERLAY WORK SHALL BE COORDINATED WITH ADJACENT LANDOWNERS SUCH THAT FUTURE PROJECTS DO NOT CUT THE NEW ASPHALT OVERLAY WORK.
7. ALL TRAFFIC CONTROL DEVICES SHALL BE IN CONFORMANCE WITH THESE PLANS OR AS OTHERWISE SPECIFIED IN M.U.T.C.D. (INCLUDING COLORADO SUPPLEMENT) AND AS PER THE RIGHT-OF-WAY WORK PERMIT TRAFFIC CONTROL PLAN.
... [items 8-23] ...

NO.	SCALE:	AS NOTED	DESIGNED BY: JPM	DRAWN BY: JPM	CHECKED BY: LEN
1					
2					
3					

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Lisle, IL 60532-4550  
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GENERAL NOTES

CITY OF FORT COLLINS  
UTILITY PLAN APPROVAL

APPROVED: \_\_\_\_\_ CITY ENGINEER \_\_\_\_\_ DATE \_\_\_\_\_

CHECKED BY: \_\_\_\_\_ WATER AND WASTEWATER UTILITY \_\_\_\_\_ DATE \_\_\_\_\_

CHECKED BY: \_\_\_\_\_ STORMWATER UTILITY \_\_\_\_\_ DATE \_\_\_\_\_

CHECKED BY: \_\_\_\_\_ PARKS AND RECREATION \_\_\_\_\_ DATE \_\_\_\_\_

CHECKED BY: \_\_\_\_\_ TRAFFIC ENGINEER \_\_\_\_\_ DATE \_\_\_\_\_

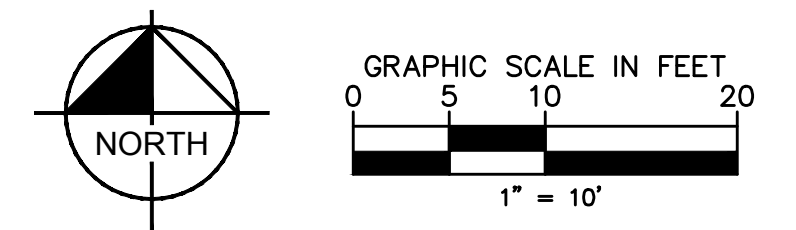
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CHOICE CENTER  
THIRD FILING  
FORT COLLINS, CO 80525

ORIGINAL ISSUE: 12/07/2016  
KHA PROJECT NO. 168524002  
SHEET NUMBER  
2 OF 12

# CHOICE CENTER THIRD FILING

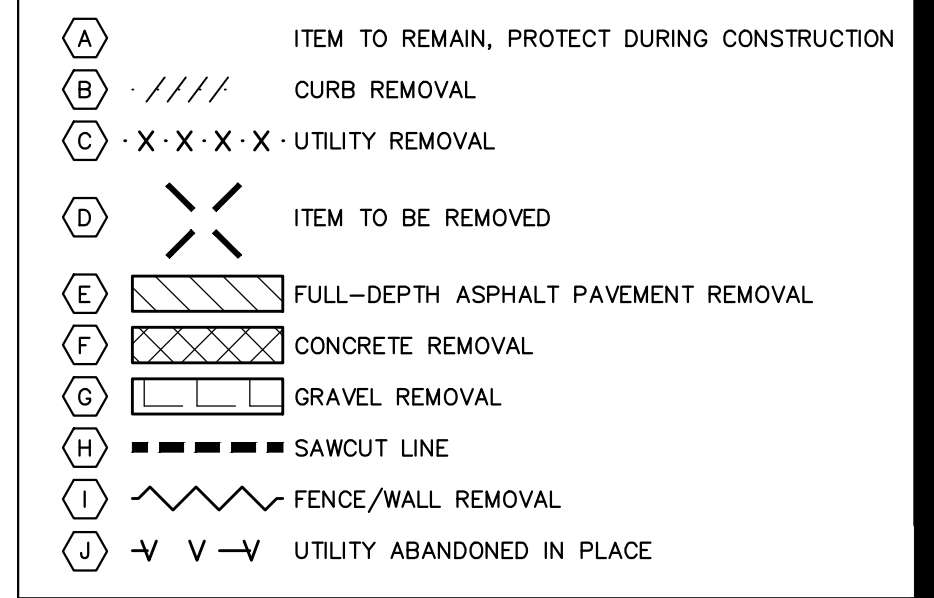
811 Know what's below. Call before you dig.



## DEMOLITION NOTES

- CONTRACTOR SHALL BE RESPONSIBLE FOR REMOVAL OF THE EXISTING STRUCTURES, RELATED UTILITIES, PAVING, AND ANY OTHER EXISTING IMPROVEMENTS AS NOTED.
- IF DEMOLITION OR CONSTRUCTION ON SITE WILL INTERFERE WITH THE ADJACENT PROPERTY OWNER'S TRAFFIC FLOW, THE CONTRACTOR SHALL COORDINATE WITH ADJACENT PROPERTY OWNER, TO MINIMIZE THE IMPACT ON TRAFFIC FLOW. TEMPORARY RE-ROUTING OF TRAFFIC IS TO BE ACCOMPLISHED BY USING CDOT APPROVED TRAFFIC BARRICADES, BARRELS, AND/OR CONES. TEMPORARY SIGNAGE AND FLAGMEN MAY BE ALSO NECESSARY.
- EROSION CONTROL MUST BE ESTABLISHED PRIOR TO ANY WORK ON SITE INCLUDING DEMOLITION.
- THE EXTENT OF SITE DEMOLITION WORK IS AS SHOWN ON THE CONTRACT DOCUMENTS AND AS SPECIFIED HEREIN.
- UNDERGROUND UTILITIES SHOWN ARE BASED ON ATLASSES AND AVAILABLE INFORMATION PRESENTED AT THE TIME OF SURVEY. CONTRACTOR SHOULD CALL "COLORADO 811" (1-800-922-1987) TO COORDINATE FIELD LOCATIONS OF EXISTING UNDERGROUND UTILITIES BEFORE ORDERING MATERIALS OR COMMENCING CONSTRUCTION. NOTIFY ENGINEER OF ANY DISCREPANCIES IMMEDIATELY. CONTRACTOR SHALL LOCATE AND PROTECT EXISTING UNDERGROUND AND OVERHEAD UTILITIES DURING CONSTRUCTION. UTILITY PROTECTION SHALL BE COORDINATED WITH THE RESPECTIVE UTILITY OWNER AND AS DIRECTED BY THE GOVERNING MUNICIPALITY. DAMAGED CABLES/CONDUITS SHALL BE REPLACED IMMEDIATELY. ALL EXISTING STRUCTURES TO REMAIN SHALL BE PROTECTED THROUGHOUT THE CONSTRUCTION PROCESS. ALL DAMAGED STRUCTURES SHALL BE REPLACED IN-KIND AND THEIR REPLACEMENT COST SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT. PROPER NOTIFICATION TO THE OWNERS OF THE EXISTING UTILITIES SHALL BE MADE AT LEAST 48 HOURS BEFORE CONSTRUCTION COMMENCES.
- LIMITS OF STREET CUT ARE APPROXIMATE. FINAL LIMITS ARE TO BE DETERMINED IN THE FIELD BY THE CITY ENGINEERING INSPECTOR.
- ALL REPAIRS TO BE IN ACCORDANCE WITH CITY STREET REPAIR STANDARDS.

## DEMOLITION LEGEND



COLLEGE AVENUE

## CITY OF FORT COLLINS UTILITY PLAN APPROVAL

APPROVED: \_\_\_\_\_ CITY ENGINEER \_\_\_\_\_ DATE \_\_\_\_\_

CHECKED BY: \_\_\_\_\_ WATER AND WASTEWATER UTILITY \_\_\_\_\_ DATE \_\_\_\_\_

CHECKED BY: \_\_\_\_\_ STORMWATER UTILITY \_\_\_\_\_ DATE \_\_\_\_\_

CHECKED BY: \_\_\_\_\_ PARKS AND RECREATION \_\_\_\_\_ DATE \_\_\_\_\_

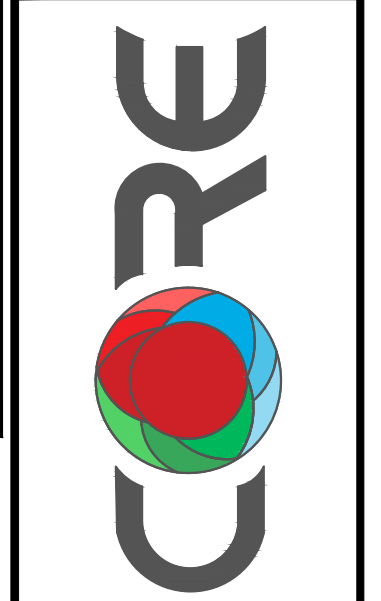
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CHECKED BY: \_\_\_\_\_ DATE \_\_\_\_\_

PDP COMMENTS	DATE	BY
1	02/07/17	LEN
2	05/02/17	LEN
3	06/09/17	LEN

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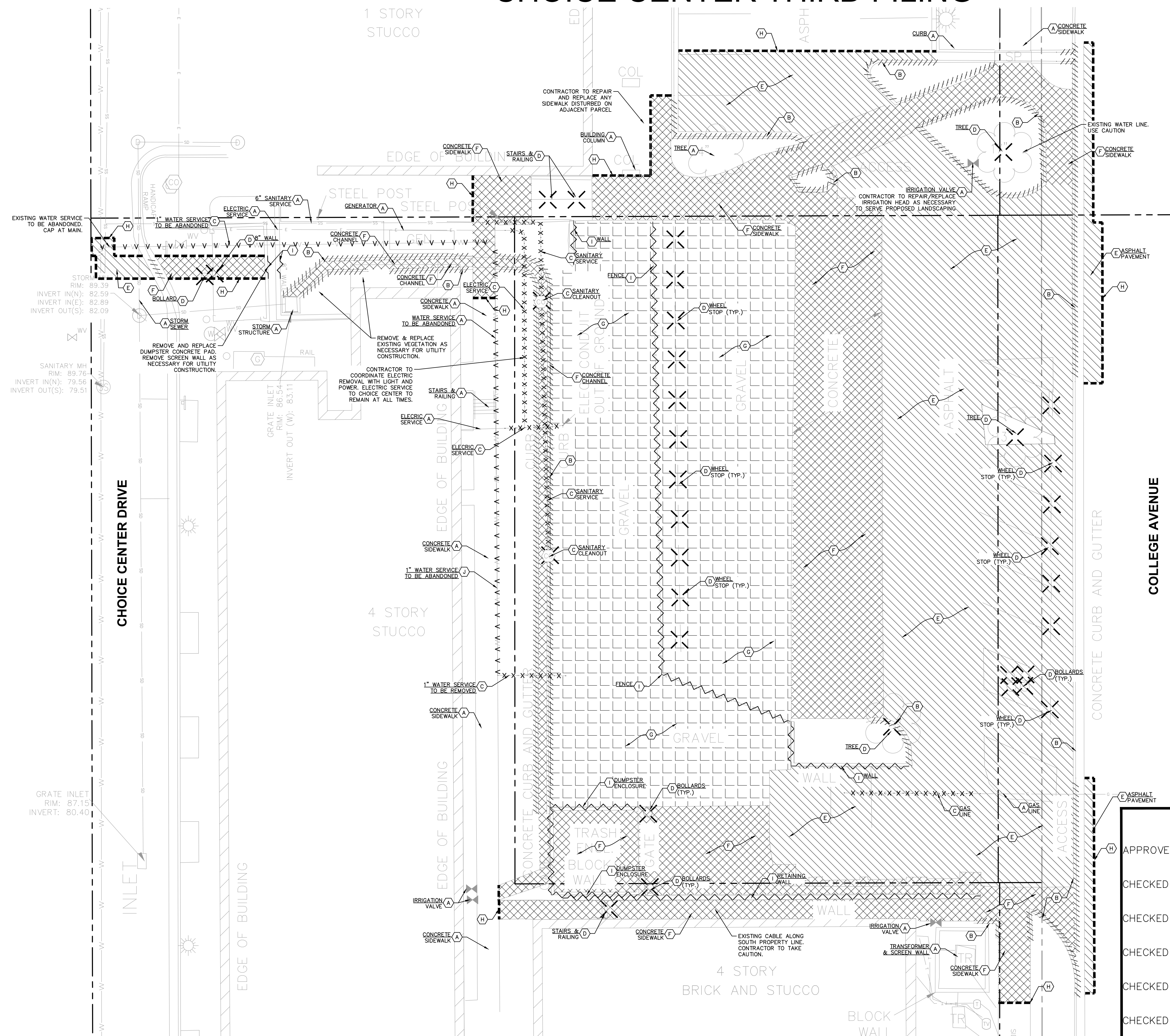
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DRAWN BY: JPM  
CHECKED BY: LEN



## DEMOLITION PLAN

CHOICE CENTER  
THIRD FILING  
FORT COLLINS, CO 80525

ORIGINAL ISSUE:  
12/07/2016  
KHA PROJECT NO.  
168524002  
SHEET NUMBER  
**3 OF 12**

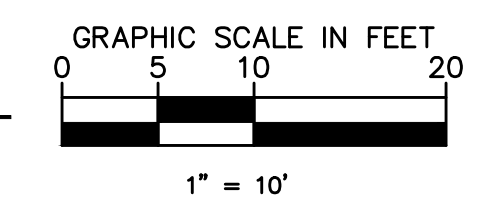
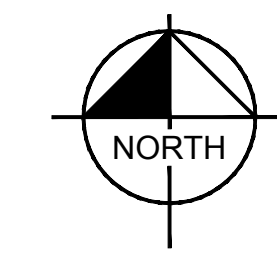


Drawing name: K:\GHS\_LIVE\16840002\_CORE\_FortCollins\_CO\_College\2 Design\CAD\PlanSheets\UTILITY PLAN\3 OF 12 - DEMOLITION PLAN.dwg Jun 08, 2017 11:30am by: steven.pelrine  
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# CHOICE CENTER THIRD FILING



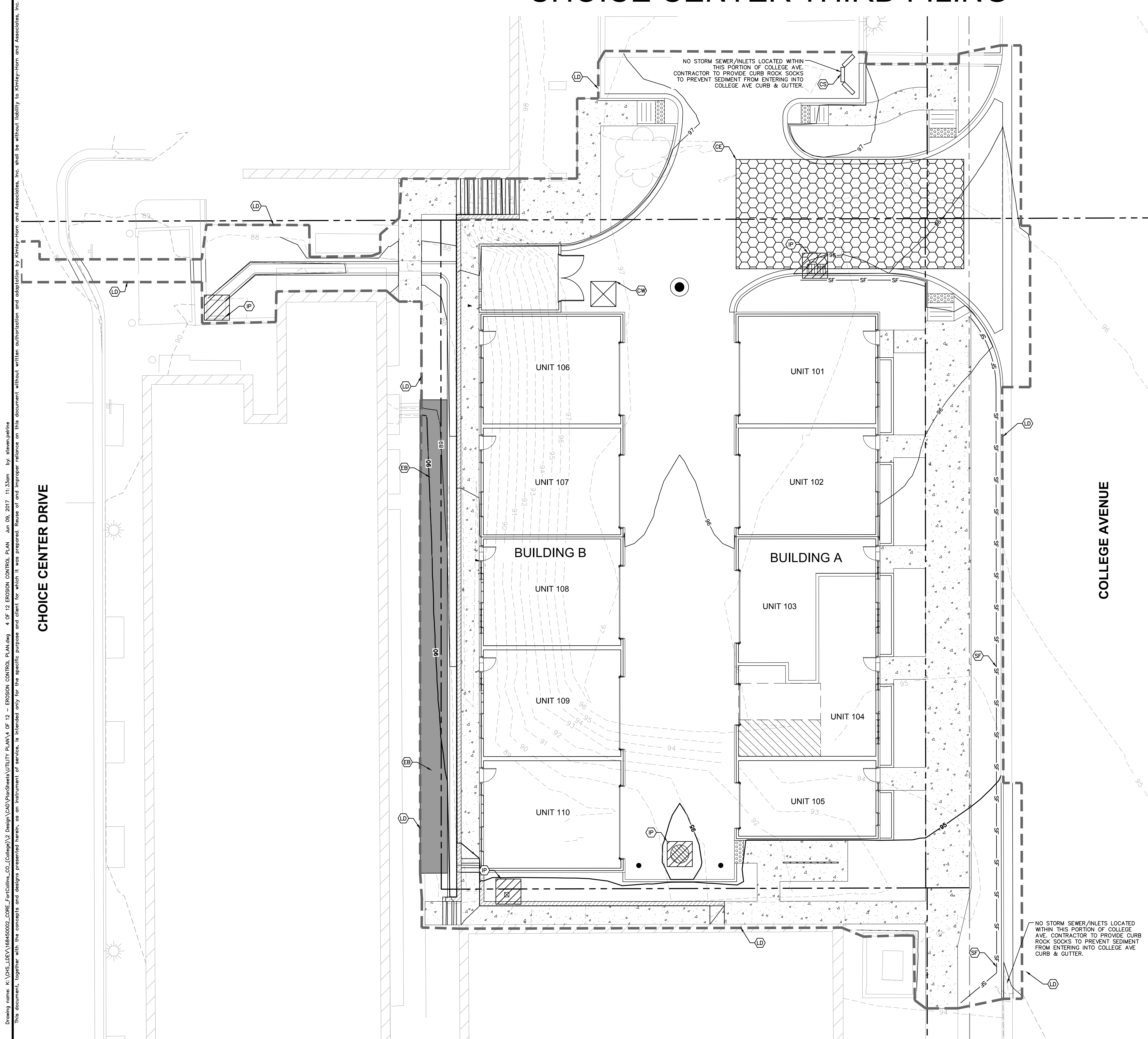
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### EROSION CONTROL LEGEND

	EB EROSION CONTROL BLANKET (SEE EROSION CONTROL DETAILS)
	CE CONSTRUCTION ENTRANCE (SEE EROSION CONTROL DETAILS)
	SF SILT FENCE (SEE EROSION CONTROL DETAILS)
	IP INLET PROTECTION (SEE EROSION CONTROL DETAILS)
	CS CURB ROCK SOCKS (SEE EROSION CONTROL DETAILS)
	CW CONCRETE WASHOUT (SEE EROSION CONTROL DETAILS) (TO BE DETERMINED BY CONTRACTOR)
	LD LIMITS OF DISTURBANCE
	95 EXISTING CONTOURS
	95 PROPOSED CONTOURS

- ### EROSION CONTROL SCHEDULE AND SEQUENCING:
- I. ROUGH GRADING CONSTRUCTION ENTRANCE/EXIT, SILT FENCE PROTECTION, CONCRETE WASHOUT AREA AND TREE PROTECTION SHALL BE INSTALLED PRIOR TO THE INITIATION OF ROUGH GRADING, AS NEEDED. TEMPORARY EROSION CONTROL MEASURES TO BE INSTALLED UPON COMPLETION OF ROUGH GRADING AND AS NECESSARY THROUGHOUT CONSTRUCTION.
  - II. UTILITY INSTALLATION ALL PRIOR EROSION CONTROL MEASURES INSTALLED ABOVE TO BE MAINTAINED AS NECESSARY DURING UTILITY INSTALLATION. STORM STRUCTURE INLET PROTECTION SHALL BE INSTALLED AS STORM DRAINAGE SYSTEM IS CONSTRUCTED.
  - III. PAVING ALL PRIOR EROSION CONTROL MEASURES INSTALLED ABOVE TO BE MAINTAINED AS NECESSARY DURING PAVING AND THROUGHOUT THE REMAINDER OF THE PROJECT.
  - IV. FINAL GRADING/STABILIZATION/LANDSCAPING ALL TEMPORARY EROSION CONTROL MEASURES TO BE REMOVED AT THE CONCLUSION OF THE PROJECT AS DIRECTED BY THE LOCAL MUNICIPALITY.



Drawing name: K:\GIS\_LIVE\16846002\_CORE\_FortCollins\_CO\_College\2 Design\CAD\PlanSheets\UTILITY PLAN\4 OF 12 - EROSION CONTROL PLAN.dwg 4 OF 12 EROSION CONTROL PLAN Jun 09, 2017 11:30am By: steven.pelrine  
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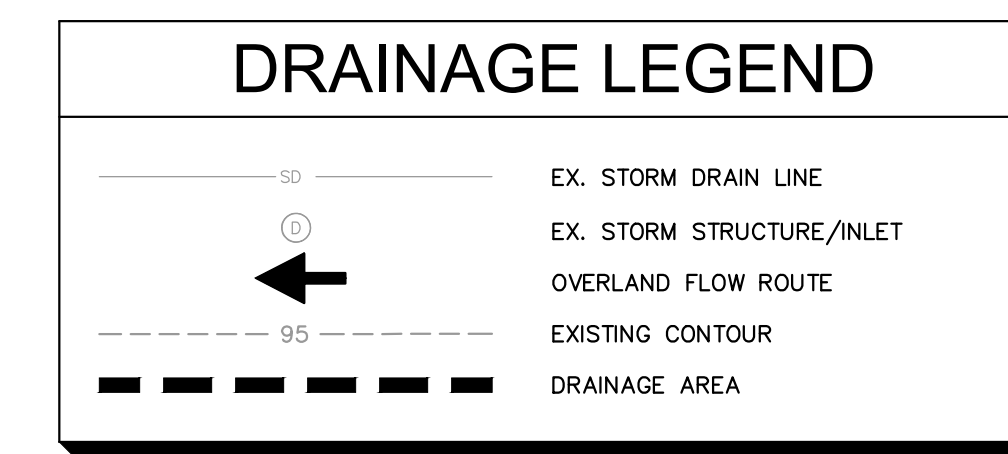
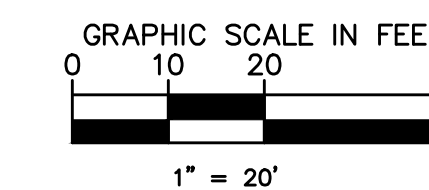
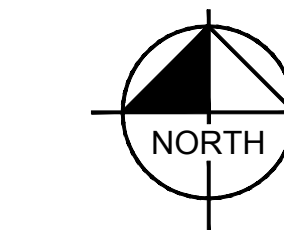
SCALE: AS NOTED	DESIGNED BY: JPM	CHECKED BY: LEN
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EROSION CONTROL PLAN		
CHOICE CENTER THIRD FILING FORT COLLINS, CO 80525		
ORIGINAL ISSUE: 12/07/2016	KHA PROJECT NO. 168524002	
SHEET NUMBER		
4 OF 12		
APPROVED: _____	CITY ENGINEER	DATE _____
CHECKED BY: _____	WATER AND WASTEWATER UTILITY	DATE _____
CHECKED BY: _____	STORMWATER UTILITY	DATE _____
CHECKED BY: _____	PARKS AND RECREATION	DATE _____
CHECKED BY: _____	TRAFFIC ENGINEER	DATE _____
CHECKED BY: _____		DATE _____
PDP COMMENTS/SITE PLAN REVISION	1	02/07/17
PDP COMMENTS	2	05/02/17
PDP COMMENTS	3	06/09/17
REVISIONS	NO.	DATE



# CHOICE CENTER THIRD FILING



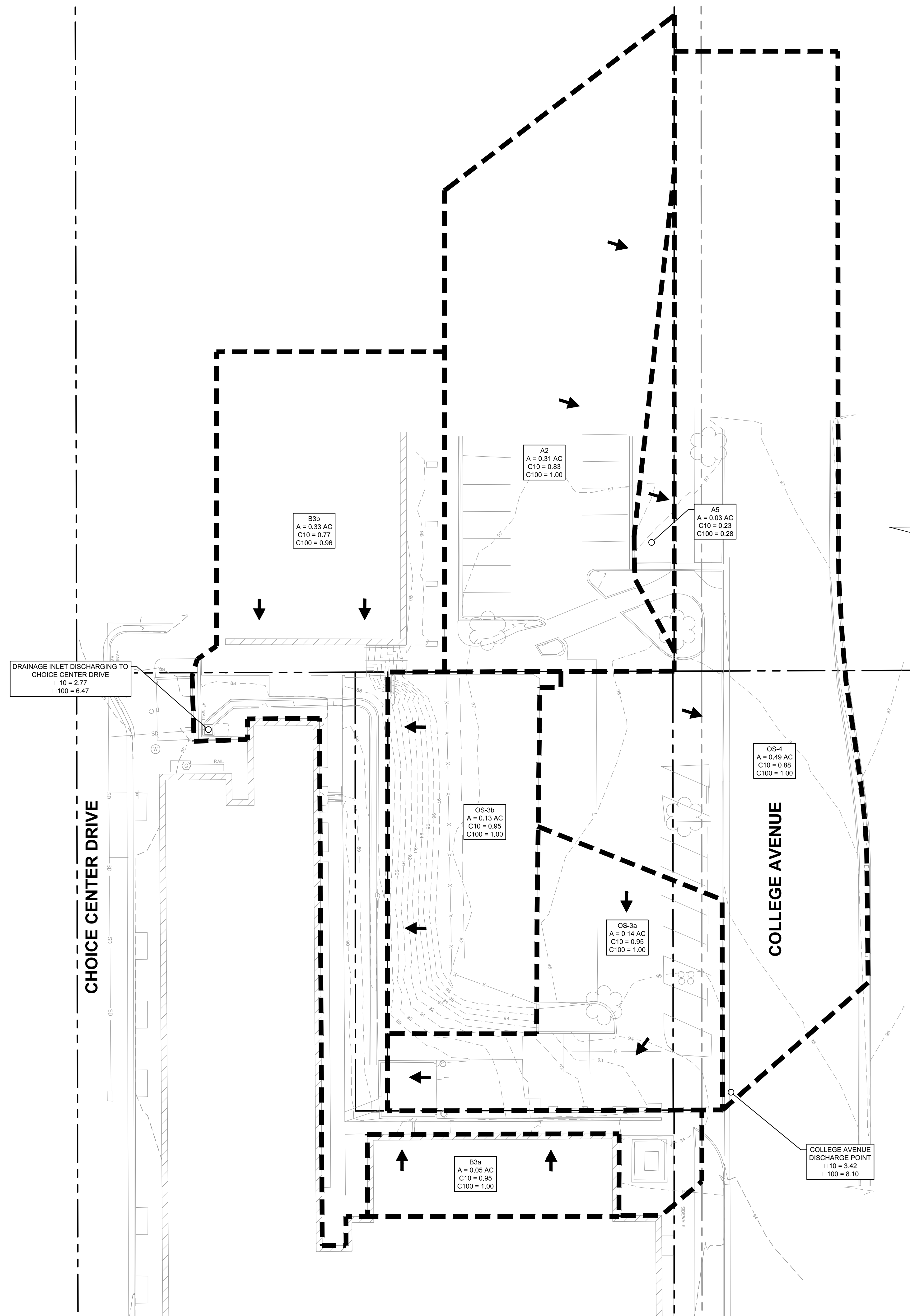
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## EXISTING DRAINAGE SUMMARY (FROM CHOICE CENTER PLANS DATED 10/4/2011)

DRAINAGE BASIN	AREA (AC)	IMPERVIOUS (AC)	C2	C10	C100	TC (MIN)	Q2 (CFS)	Q10 (CFS)	Q100 (CFS)
<b>WEST DRAINAGE - TRIBUTARY TO DRAINAGE INLET NEAR CHOICE CENTER DRIVE</b>									
B3a	0.05	0.05	-	0.95	1.00	-	-	0.24	0.52
B3b	0.33	0.25	-	0.77	0.96	-	-	1.24	3.17
OS-3a	0.14	0.14	-	0.95	1.00	-	-	0.67	1.44
OS-3b	0.13	0.13	-	0.95	1.00	-	-	0.62	1.34
<b>TOTAL FLOW</b>							-	2.77	6.47
<b>EAST DRAINAGE - TRIBUTARY TO COLLEGE AVENUE</b>									
A2	0.31	0.26	-	0.83	1.00	-	-	1.27	3.12
A5	0.03	0.002	-	0.23	0.28	-	-	0.03	0.07
OS-4	0.49	0.45	-	0.88	1.00	-	-	2.12	4.91
<b>TOTAL FLOW</b>							-	3.42	8.10

- EXISTING DRAINAGE AREAS, RUNOFF COEFFICIENTS, AND FLOWS FROM CHOICE CENTER DRAINAGE AND EROSION CONTROL PLAN DATED 10/4/2011.
- RUNOFF COEFFICIENTS AND FLOW RATES FOR THE 2-YEAR EVENT WERE NOT PROVIDED IN THE 10/4/2011 PLAN.
- ONLY DRAINAGE AREAS RELATIVE TO THE SITE DEVELOPMENT WORK ARE SHOWN, SEE THE CHOICE CENTER DRAINAGE AND EROSION CONTROL PLAN FOR COMPLETE DETAILS.
- TIME OF CONCENTRATION NOT SPECIFIED ON THE EXISTING DRAINAGE PLAN



DRAINAGE INLET DISCHARGING TO CHOICE CENTER DRIVE  
 □ 10 = 2.77  
 □ 100 = 6.47

COLLEGE AVENUE DISCHARGE POINT  
 □ 10 = 3.42  
 □ 100 = 8.10

CITY OF FORT COLLINS  
UTILITY PLAN APPROVAL

APPROVED: \_\_\_\_\_ CITY ENGINEER \_\_\_\_\_ DATE \_\_\_\_\_

CHECKED BY: \_\_\_\_\_ WATER AND WASTEWATER UTILITY \_\_\_\_\_ DATE \_\_\_\_\_

CHECKED BY: \_\_\_\_\_ STORMWATER UTILITY \_\_\_\_\_ DATE \_\_\_\_\_

CHECKED BY: \_\_\_\_\_ PARKS AND RECREATION \_\_\_\_\_ DATE \_\_\_\_\_

CHECKED BY: \_\_\_\_\_ TRAFFIC ENGINEER \_\_\_\_\_ DATE \_\_\_\_\_

CHECKED BY: \_\_\_\_\_ \_\_\_\_\_ DATE \_\_\_\_\_

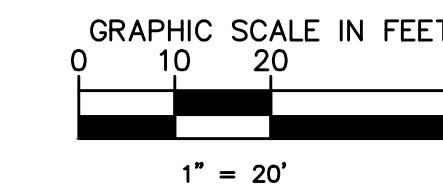
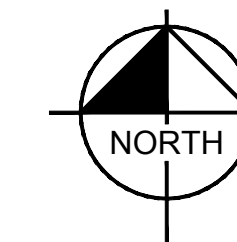
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1				2				3			
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SCALE:	AS NOTED	DESIGNED BY:	JPM	DRAWN BY:	JPM	CHECKED BY:	LEN				
EXISTING DRAINAGE PLAN											
CHOICE CENTER THIRD FILING FORT COLLINS, CO 80525											
ORIGINAL ISSUE: 12/07/2016											
KHA PROJECT NO. 168524002											
SHEET NUMBER											
6 OF 12											

Drawing name: K:\GIS\_LISEY\168450002\_CORE\_FortCollins\_CO\_College\2 Design\CAD\PlanSheets\UTILITY PLAN\6 OF 12 - EXISTING DRAINAGE PLAN.dwg 6 OF 12 EXISTING DRAINAGE PLAN Jun 09, 2017 11:34am by: steven.pelrine  
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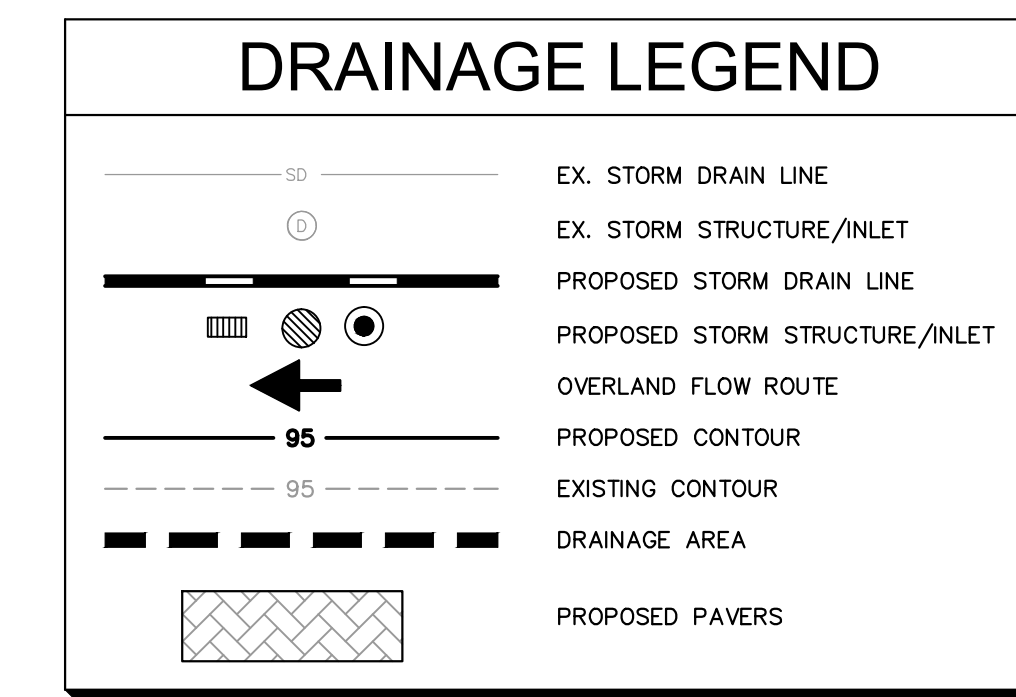
# CHOICE CENTER THIRD FILING



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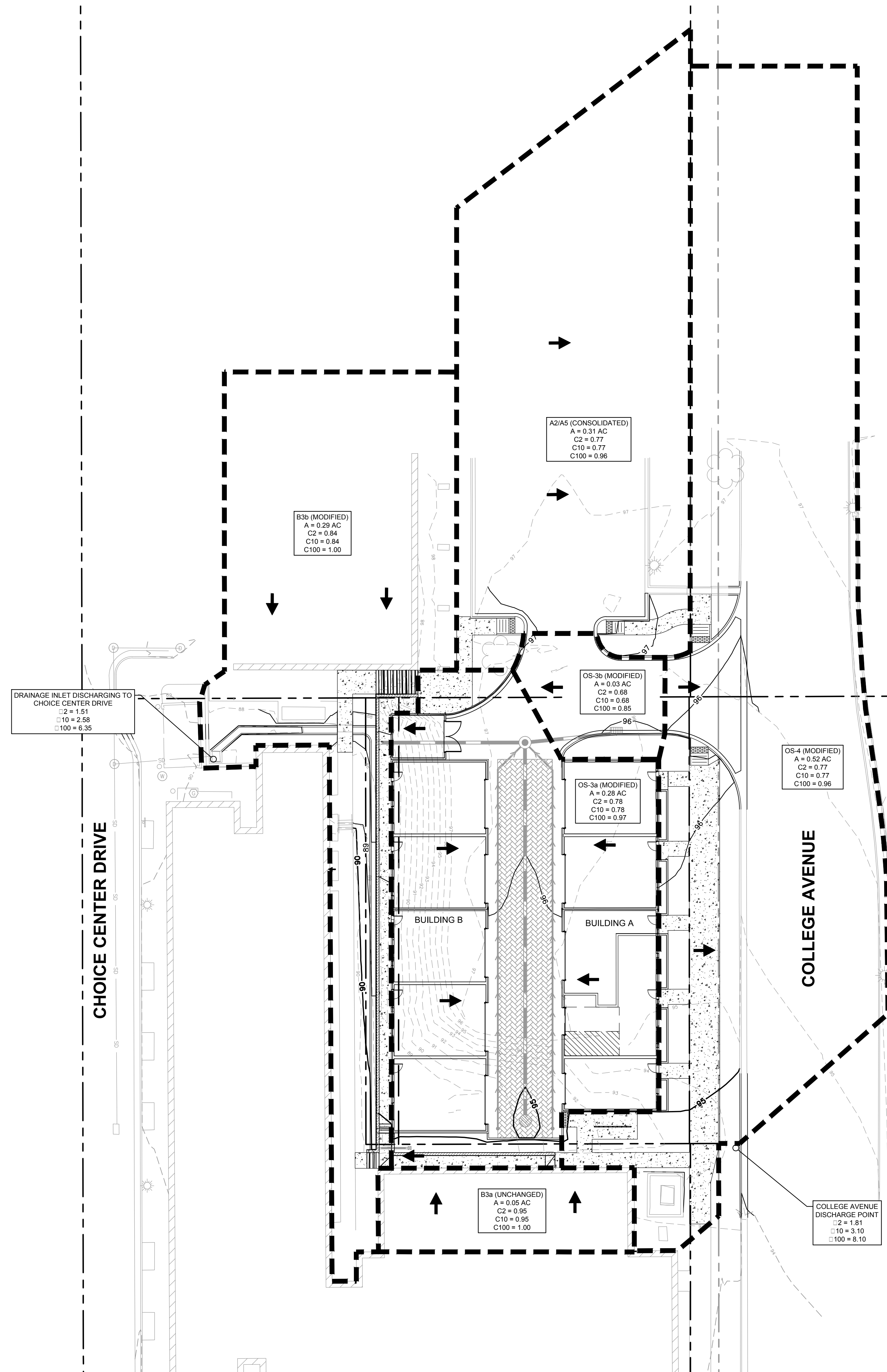


LID TABLE	
ON-SITE TREATMENT BY LID REQUIREMENT	
TOTAL NEW IMPERVIOUS AREA	11,559 SF
IMPERVIOUS AREA TREATED BY PAVERS	9,583 SF
ACTUAL % ON-SITE TREATED BY LID	82.91%
POROUS PAVEMENT REQUIREMENT	
NEW PAVEMENT AREA	3,913 SF
AREA OF PERVIOUS PAVERS	2,242 SF
TOTAL PAVEMENT AREA	6,155 SF
ACTUAL % OF PERVIOUS PAVEMENT PROVIDED	36.43%



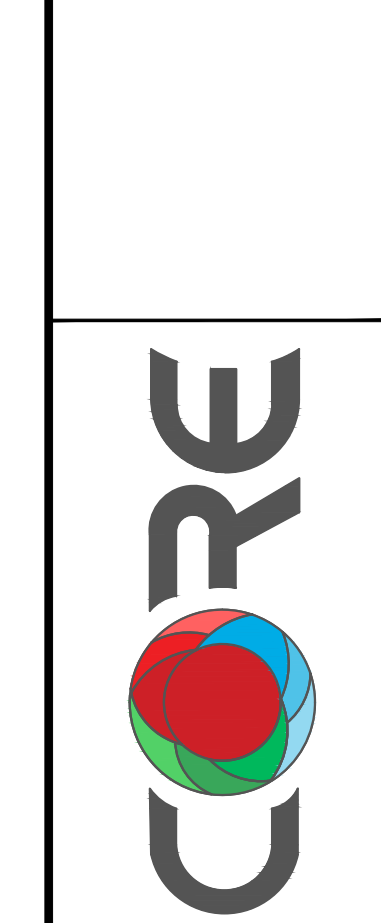
PROPOSED DRAINAGE SUMMARY											
DRAINAGE BASIN	AREA (AC)	IMPERVIOUS AREA (AC)	PERVIOUS AREA (AC)	C2	C10	C100	TC (MIN)	Q2 (CFS)	Q10 (CFS)	Q100 (CFS)	
<b>WEST DRAINAGE - TRIBUTARY TO DRAINAGE INLET NEAR CHOICE CENTER DRIVE</b>											
B3a (UNCHANGED)	0.05	0.05	0.00	0.95	0.95	1.00	5.00	0.14	0.23	0.50	
B3b (MODIFIED)	0.29	0.25	0.04	0.84	0.84	1.00	5.00	0.69	1.19	2.89	
OS-3a (MODIFIED)	0.28	0.22	0.06	0.78	0.78	0.97	5.00	0.62	1.06	2.71	
OS-3b (MODIFIED)	0.03	0.02	0.01	0.68	0.68	0.85	5.00	0.06	0.10	0.25	
								PROPOSED FLOW	1.51	2.58	6.35
								EXISTING FLOW	N/A	2.77	6.47
								FLOW REDUCTION	N/A	-0.19	-0.12
<b>EAST DRAINAGE - TRIBUTARY TO COLLEGE AVENUE</b>											
A2/A5 (CONSOLIDATED)	0.31	0.24	0.07	0.77	0.77	0.96	5.00	0.68	1.16	2.97	
OS-4 (MODIFIED)	0.52	0.40	0.12	0.77	0.77	0.96	5.00	1.13	1.94	4.95	
								PROPOSED FLOW	1.81	3.10	7.92
								EXISTING FLOW	N/A	3.42	8.10
								FLOW REDUCTION	N/A	-0.32	-0.18

- DRAINAGE NOTES**
- EXISTING DRAINAGE AREAS FROM CHOICE CENTER DRAINAGE AND EROSION CONTROL PLAN DATED 10/4/2011.
  - EXISTING RUNOFF COEFFICIENTS AND FLOWS CALCULATED USING THE LATEST METHODOLOGY FROM USDCM AND FORT COLLINS. SEE SHEET 6 OF 12 - EXISTING DRAINAGE PLAN.
  - ONLY DRAINAGE AREAS RELATIVE TO THE SITE DEVELOPMENT WORK ARE SHOWN. SEE THE CHOICE CENTER DRAINAGE AND EROSION CONTROL PLAN FOR COMPLETE DETAILS.
  - MINIMUM TIME OF CONCENTRATION OF 5 MINUTES ASSUMED FOR ALL AREAS
  - FLOW RATES CALCULATED USING THE RATIONAL METHOD (Q = CIA)
- RUNOFF COEFFICIENTS**
- C10 IMPERVIOUS AREAS = 0.95
  - C10 PERVIOUS AREAS = 0.15
  - C100 = MIN(1.00 OR 1.25\*C10)
- RAINFALL INTENSITIES**
- (2) = 5 MIN, 2 YEAR = 2.85 IN/HR
  - (10) = 5 MIN, 10 YEAR = 4.87 IN/HR
  - (100) = 5 MIN, 100 YEAR = 9.95 IN/HR



NO.	DATE	REVISIONS	BY
1	02/07/17	PDP COMMENTS	
2	05/02/17	PDP COMMENTS/SITE PLAN REVISION	
3	06/09/17	PDP COMMENTS	

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**PROPOSED DRAINAGE PLAN**

**CHOICE CENTER THIRD FILING**  
FORT COLLINS, CO 80525

ORIGINAL ISSUE: 12/07/2016  
KHA PROJECT NO. 168524002  
SHEET NUMBER  
**7 OF 12**

CITY OF FORT COLLINS  
UTILITY PLAN APPROVAL

APPROVED: \_\_\_\_\_ CITY ENGINEER \_\_\_\_\_ DATE \_\_\_\_\_

CHECKED BY: \_\_\_\_\_ WATER AND WASTEWATER UTILITY \_\_\_\_\_ DATE \_\_\_\_\_

CHECKED BY: \_\_\_\_\_ STORMWATER UTILITY \_\_\_\_\_ DATE \_\_\_\_\_

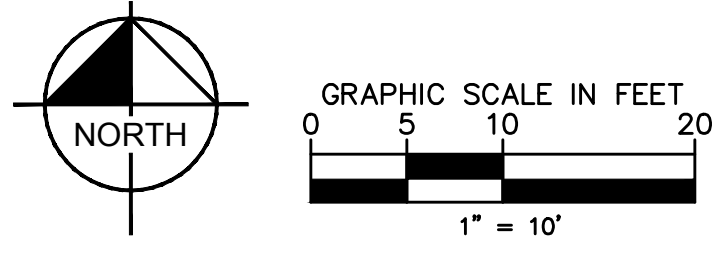
CHECKED BY: \_\_\_\_\_ PARKS AND RECREATION \_\_\_\_\_ DATE \_\_\_\_\_

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CHECKED BY: \_\_\_\_\_ DATE \_\_\_\_\_

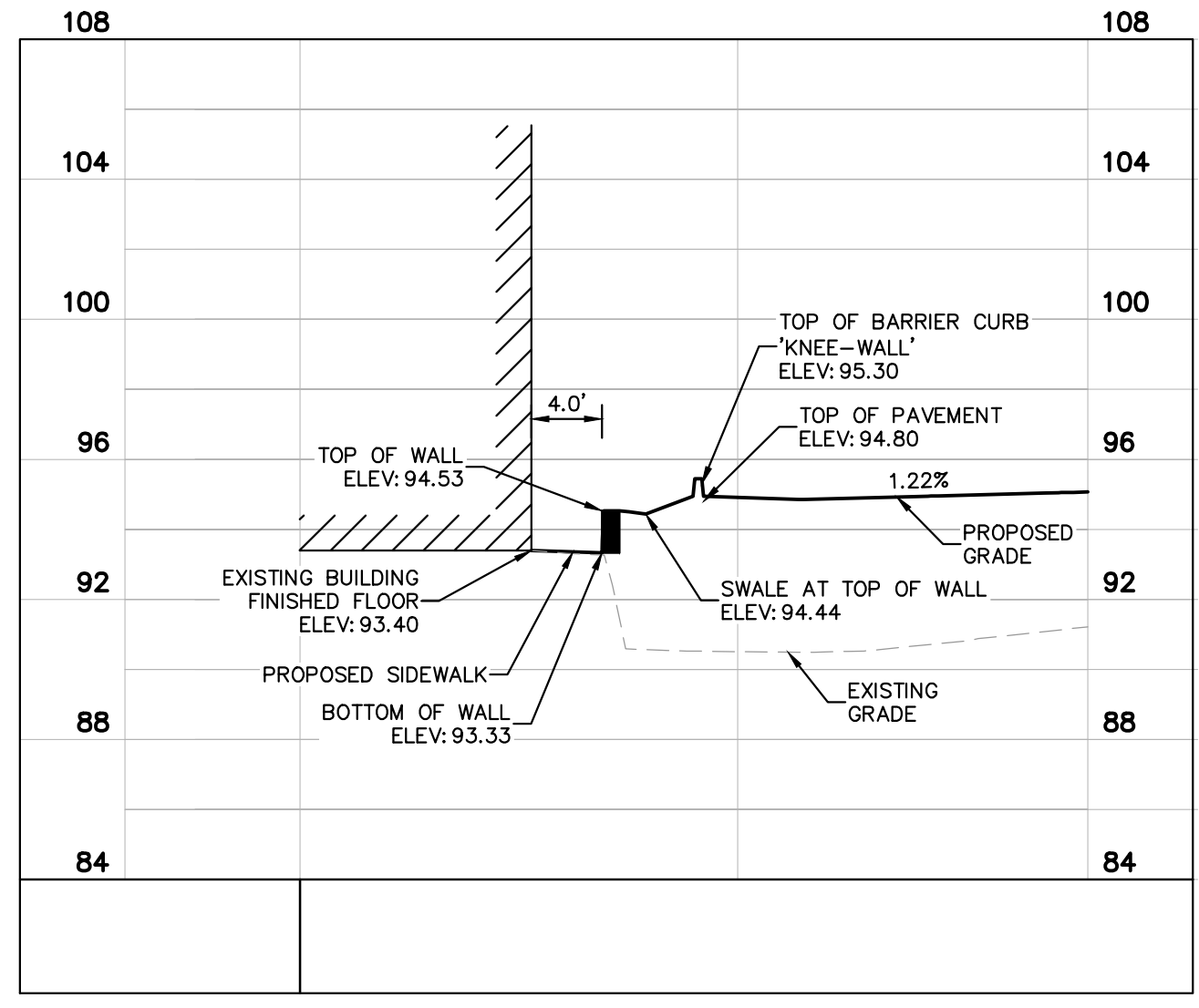
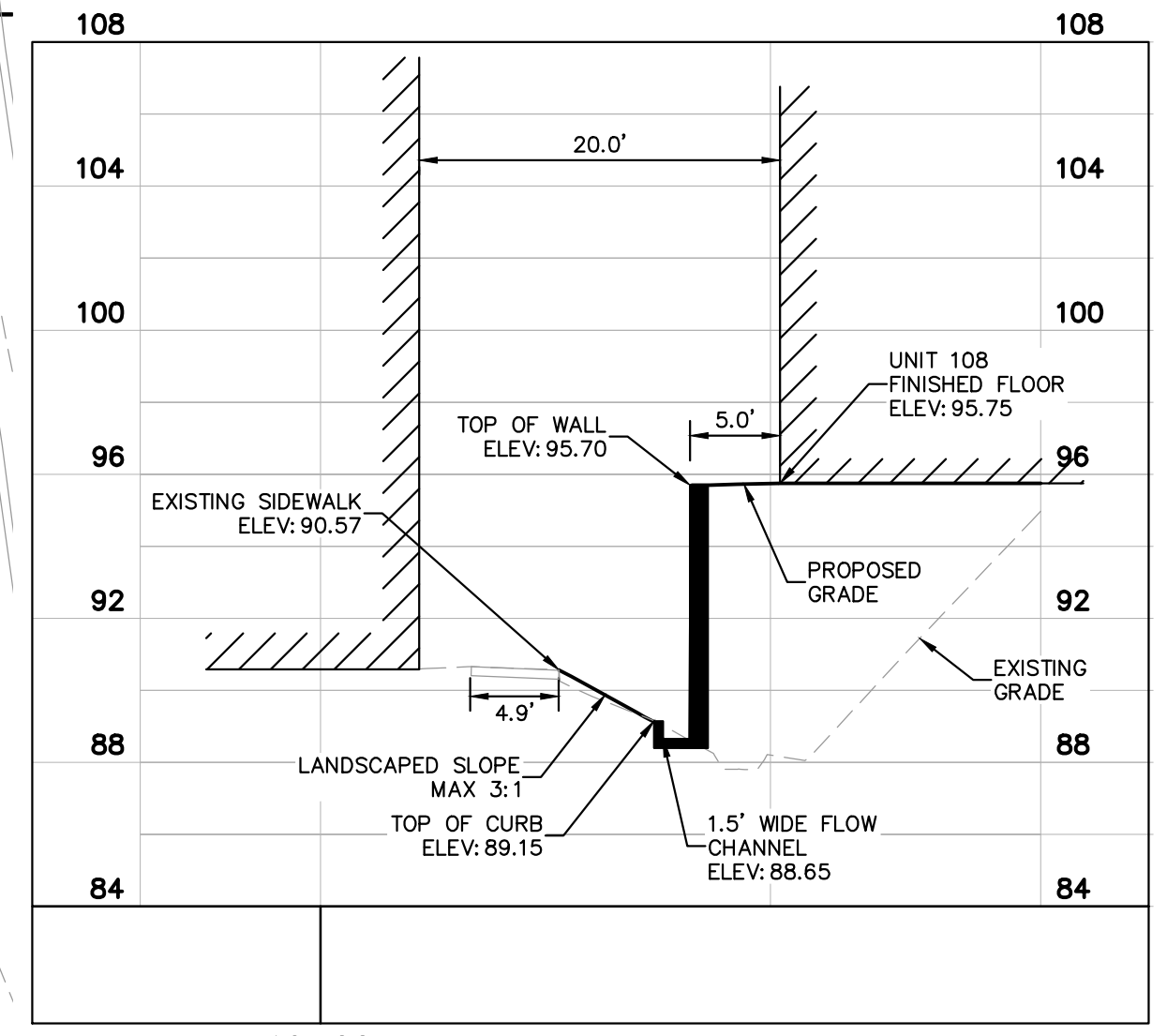
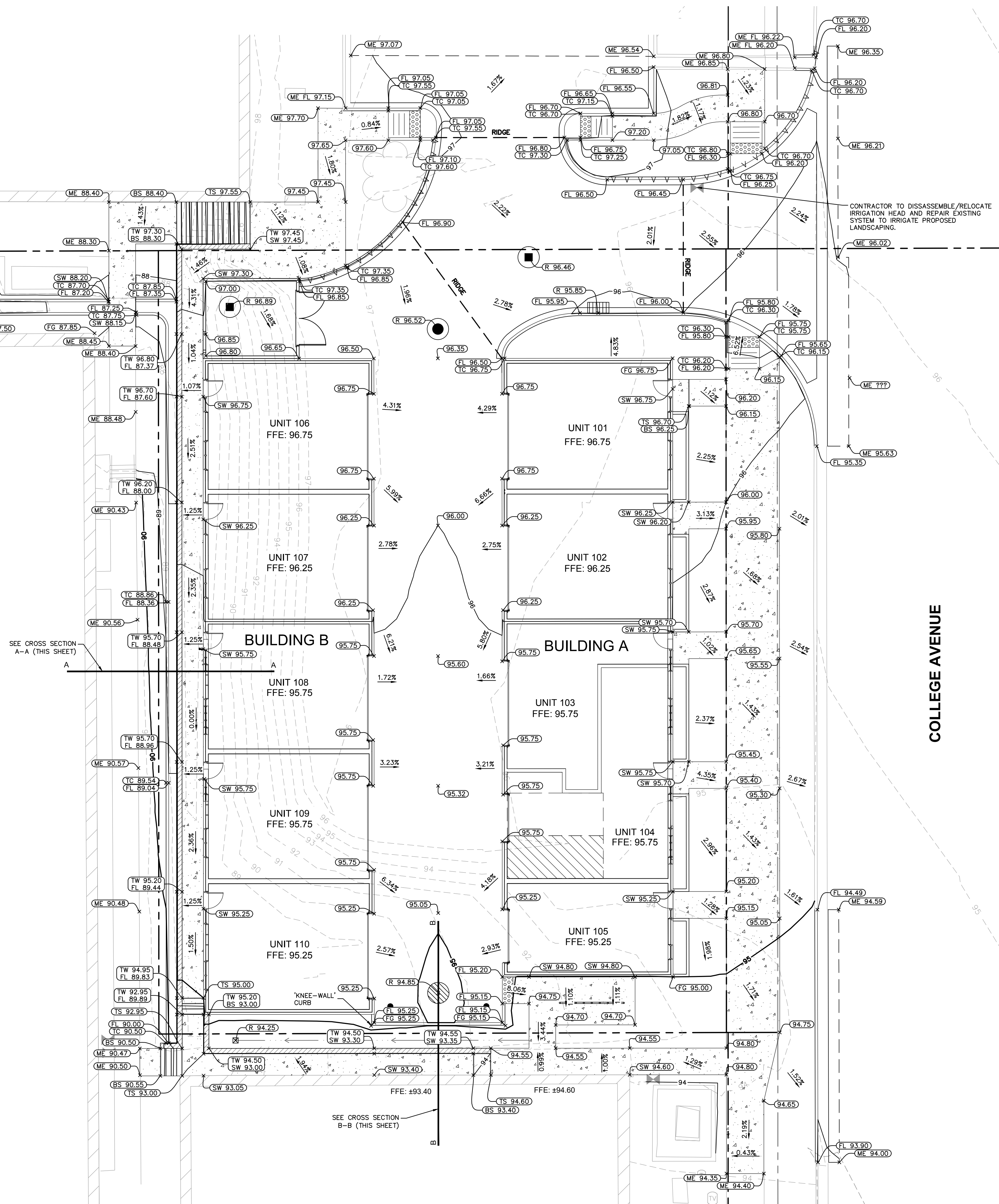
Drawing name: K:\GIS\_LISE\16846002\_CORE\_FortCollins\_CO\_College\2 Design\CAD\PlanSheets\UTILITY PLAN\7 OF 12 - PROPOSED DRAINAGE PLAN.dwg 7 OF 12 PROPOSED DRAINAGE PLAN Jun 09, 2017 11:34am by: steven.pine  
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# CHOICE CENTER THIRD FILING



- ### GRADING NOTES
- CONTRACTOR TO VERIFY ALL EXISTING TOPOGRAPHY AND STRUCTURES ON THE SITE AND IMMEDIATELY NOTIFY THE ENGINEER OF ANY DISCREPANCIES PRIOR TO STARTING WORK.
  - ALL PAVEMENT SPOT GRADE ELEVATIONS AND RIM ELEVATIONS WITHIN OR ALONG CURB AND GUTTER REFER TO EDGE OF PAVEMENT ELEVATIONS UNLESS OTHERWISE NOTED.
  - ALL ELEVATIONS SHOWN DEPICT FINISHED GRADE OR EDGE OF PAVEMENT UNLESS OTHERWISE NOTED. GENERAL CONTRACTOR TO COORDINATE WITH EXCAVATION, LANDSCAPE AND PAVING SUBCONTRACTORS REGARDING TOPSOIL THICKNESS FOR LANDSCAPE AREAS AND PAVEMENT SECTION THICKNESS FOR PAVED AREAS TO PROPERLY ENSURE ADEQUATE CUT TO ESTABLISH SUBGRADE ELEVATIONS.
  - NO EARTHEN SLOPE SHALL BE GREATER THAN 3:1, UNLESS OTHERWISE NOTED.
  - MAXIMUM SLOPE IN ACCESSIBLE PARKING SPACES AND LOADING ZONES SHALL NOT EXCEED 2.0% IN ALL DIRECTIONS.
  - MAXIMUM RUNNING SLOPE SHALL NOT EXCEED 5% AND CROSS SLOPE SHALL NOT EXCEED 2% ON ALL SIDEWALKS AND ACCESSIBLE ROUTES UNLESS OTHERWISE NOTED.
  - WHEN NATURAL FLOW OF DRAINAGE IS AWAY FROM CURB, CONTRACTOR TO INSTALL REVERSE GUTTER PITCH.
  - MATCH EXISTING ELEVATIONS AT THE PROPERTY LIMITS.
  - ADD 4900 FT TO ALL ELEVATIONS TO CORRESPOND TO NAVD88 VERTICAL DATUM.

NO.	REVISIONS	DATE	BY
1	PDP COMMENTS	02/07/17	LEN
2	PDP COMMENTS/SITE PLAN REVISION	05/02/17	LEN
3	PDP COMMENTS	06/09/17	LEN



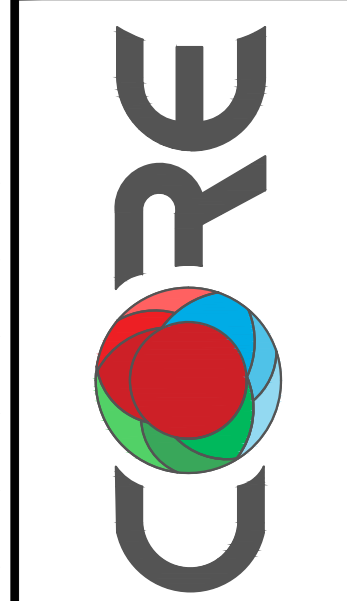
### GRADING LEGEND

SW	=	SIDEWALK ELEVATION
EP	=	EDGE OF PAVEMENT
FL	=	FLOW LINE
TC	=	TOP OF CURB
ME	=	MATCH ELEVATION
R	=	RIM ELEVATION
TW	=	TOP OF WALL
FG	=	FINISHED GRADE
TS	=	TOP OF STAIRS
BS	=	BOTTOM OF STAIRS
---	=	PROPOSED CONTOUR
- - - -	=	EXISTING CONTOUR
—RIDGE—	=	RIDGE LINE
X-XXX	=	SLOPE AND FLOW DIRECTION
	=	PROPOSED RETAINING WALL
∩∩∩∩	=	REVERSED PITCH CURB AND GUTTER

### CITY OF FORT COLLINS UTILITY PLAN APPROVAL

APPROVED:	_____	CITY ENGINEER	_____	DATE
CHECKED BY:	_____	WATER AND WASTEWATER UTILITY	_____	DATE
CHECKED BY:	_____	STORMWATER UTILITY	_____	DATE
CHECKED BY:	_____	PARKS AND RECREATION	_____	DATE
CHECKED BY:	_____	TRAFFIC ENGINEER	_____	DATE
CHECKED BY:	_____		_____	DATE

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**GRADING PLAN**

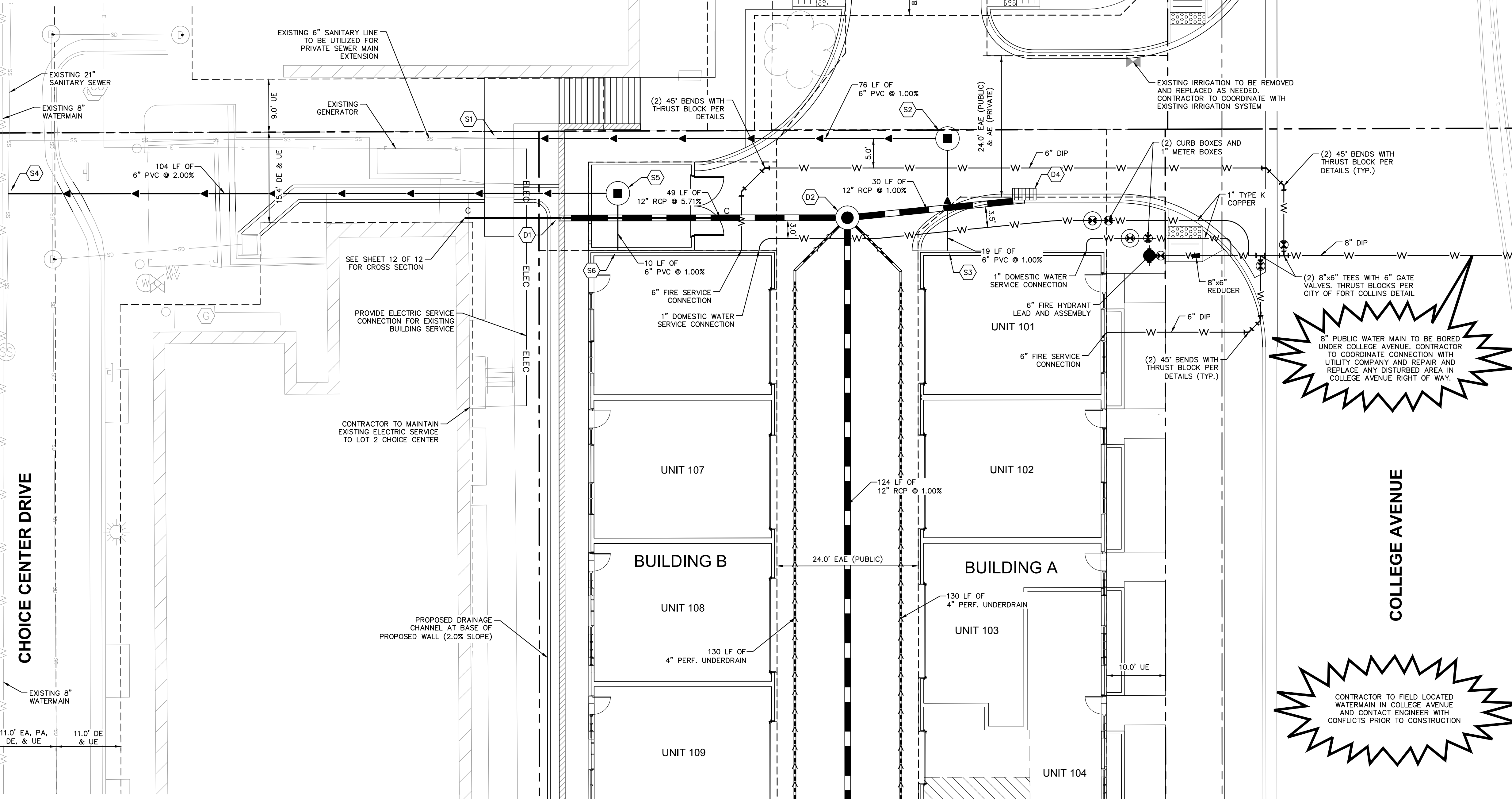
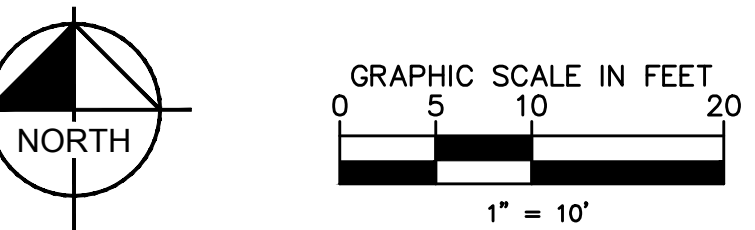
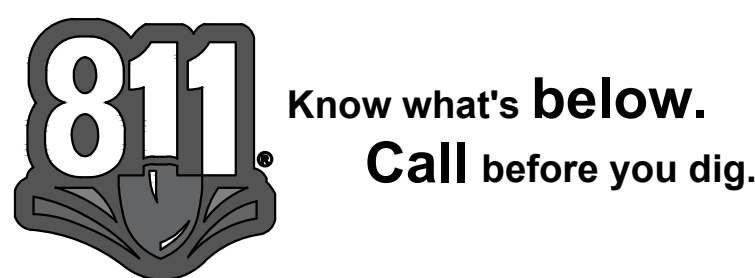
**CHOICE CENTER THIRD FILING**  
FORT COLLINS, CO 80525

ORIGINAL ISSUE: 12/07/2016  
KHA PROJECT NO. 168524002  
SHEET NUMBER: 8 OF 12

Drawing name: K:\GIS\_LIEV\168460002\_CORE\_FortCollins\_CO\_College\2\_Design\CAD\PlanSheets\UTILITY PLAN\8 OF 12 GRADING PLAN Jun 09, 2017 11:34am by: steven.pelrine  
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# CHOICE CENTER THIRD FILING



### UTILITY LEGEND

W-W	EX. WATER LINE
W-W	EX. HYDRANT
W-W	EX. WATER VALVE
SS	EX. SANITARY SEWER LINE
SS	EX. SANITARY SEWER MANHOLE
SD	EX. STORM DRAIN LINE
SD	EX. STORM STRUCTURE/INLET
G	EX. GAS LINE
ELEC	EX. UNDERGROUND ELECTRIC LINE
TV	EX. UNDERGROUND TELEPHONE LINE
ELEC	EX. LIGHT POLE
GAS	EX. UNDERGROUND GAS LINE (BY GAS COMPANY)
TEL	EX. UNDERGROUND TELEPHONE LINE
---	PROPOSED PHONE LINE
---	PROPOSED STORM SEWER LINE
---	PROPOSED STORM STRUCTURE
---	PROPOSED SANITARY SEWER LINE
---	PROPOSED SANITARY MANHOLE
---	PROPOSED STORM/SANITARY CLEANOUT
---	PROPOSED WATER LINE
---	PROPOSED VALVE VAULT
---	PROPOSED VALVE BOX
---	PROPOSED FIRE HYDRANT
---	PROPOSED UNDERDRAIN

- ### UTILITY NOTES
- ALL WATER LINES  $\geq$  3" SHALL BE DUCTILE IRON PIPE, CLASS 52. WATER LINES  $<$  3" SHALL BE TYPE K COPPER.
  - ALL SANITARY SEWER LINES SHALL BE PVC MEETING, ASTM D-3034 SDR 26 EXCEPT FOR SANITARY SEWER THAT CROSSES ABOVE WATER MAIN, THIS PIPE SHALL BE AWWA C900 (UNLESS WATER MAIN CASING IS UTILIZED). PROVIDE 42" MINIMUM COVER.
  - CONTRACTOR SHALL COORDINATE ANY DISRUPTIONS TO EXISTING UTILITY SERVICES WITH ADJACENT PROPERTY OWNERS.
  - ALL ELECTRIC AND TELEPHONE EXTENSIONS INCLUDING SERVICE LINES SHALL BE CONSTRUCTED TO THE APPROPRIATE UTILITY COMPANY SPECIFICATIONS. ALL UTILITY DISCONNECTIONS SHALL BE COORDINATED WITH THE DESIGNATED UTILITY COMPANIES.
  - CONSTRUCTION SHALL NOT START ON ANY PUBLIC UTILITY SYSTEM UNTIL WRITTEN APPROVAL HAS BEEN RECEIVED BY THE ENGINEER FROM THE APPROPRIATE GOVERNING AUTHORITY AND CONTRACTOR HAS BEEN NOTIFIED BY THE ENGINEER.
  - CONTRACTOR TO CALL "811" TO COORDINATE FIELD LOCATIONS OF EXISTING UNDERGROUND UTILITIES BEFORE ORDERING MATERIALS OR COMMENCING CONSTRUCTION. NOTIFY ENGINEER OF ANY DISCREPANCIES IMMEDIATELY.
  - PRIOR TO THE CONSTRUCTION OF OR CONNECTION TO ANY STORM DRAIN, SANITARY SEWER, WATER MAIN OR ANY OTHER UTILITIES, THE CONTRACTOR SHALL EXCAVATE, VERIFY AND CALCULATE ALL POINTS OF CONNECTION AND ALL UTILITY CROSSINGS AND INFORM THE ENGINEER AND THE OWNER/DEVELOPER OF ANY CONFLICT OR REQUIRED DEVIATIONS FROM THE PLAN. NOTIFICATION SHALL BE MADE A MINIMUM OF 72 HOURS PRIOR TO CONSTRUCTION. THE ENGINEER AND ITS CLIENTS SHALL BE HELD HARMLESS IN THE EVENT THAT THE CONTRACTOR FAILS TO MAKE SUCH NOTIFICATION. THE MUNICIPALITY SHALL BE NOTIFIED OF ANY AND ALL CHANGES TO THE DESIGN PLANS.
  - CONTRACTOR SHALL COMPLY COMPLETELY WITH THE LATEST STANDARDS OF OSHA DIRECTIVES OR ANY OTHER AGENCY HAVING JURISDICTION FOR EXCAVATION AND TRENCHING PROCEDURES. THE CONTRACTOR SHALL USE SUPPORT SYSTEMS, SLOPING, BENCHING AND OTHER MEANS OF PROTECTION. THIS IS TO INCLUDE, BUT NOT LIMITED TO ACCESS AND EGRESS FROM ALL EXCAVATION AND TRENCHING. CONTRACTOR IS RESPONSIBLE FOR COMPLYING WITH PERFORMANCE CRITERIA AS REQUIRED BY OSHA.
  - CONTRACTOR TO AVOID DISRUPTION OF ANY ADJACENT TENANT'S TRAFFIC OPERATIONS DURING INSTALLATION OF UTILITIES.
  - ALL DIMENSIONS ARE TO CENTERLINE OF PIPE OR CENTER OF MANHOLE UNLESS NOTED OTHERWISE.
  - SEE ARCHITECTURAL AND MEP PLANS FOR EXACT UTILITY CONNECTION LOCATIONS AT BUILDING.
  - SEE DETAILS FOR LOCATING STORM STRUCTURES WITHIN THE CURB LINE.
  - STORMWATER FACILITIES MUST BE FUNCTIONAL BEFORE BUILDING CONSTRUCTION BEGINS.
  - ADD 4900 FT TO ALL ELEVATIONS TO CORRESPOND TO THE NAVD88 VERTICAL DATUM.

### SANITARY STRUCTURE TABLE

STRUCTURE NAME:	DETAILS:
S1	CONNECT TO EXISTING SANITARY SEWER. CONTRACTOR TO FIELD VERIFY THE EXISTING LOCATION, SIZE, AND DEPTH PRIOR TO ORDERING MATERIALS. FG: 88.26 INV IN: 81.58 (E) INV OUT: 81.58 (W)
S2	4" DIA. SANITARY DROP MH RIM: 96.46 INV IN: 90.31 (S) INV OUT: 82.34 (W)
S3	SANITARY BUILDING CONNECTION RIM: 96.75 INV OUT: 90.50 (N)
S4	45" WYE TO EXISTING SANITARY SEWER. CONTRACTOR TO FIELD VERIFY THE EXISTING LOCATION, SIZE, AND DEPTH PRIOR TO ORDERING MATERIALS. FG: 81.22 INV IN: 80.70 (E)
S5	4" DIA. SANITARY DROP MH RIM: 96.89 INV IN: 90.40 (S) INV OUT: 82.77 (W)
S6	SANITARY BUILDING CONNECTION FG: 96.76 INV OUT: 90.50 (N)

### STORM STRUCTURE TABLE

STRUCTURE NAME:	DETAILS:
D1	STORM OUTFALL TO PROPOSED CHANNEL AT BASE OF WALL. SEE DETAIL ON SHEET 12 OF 12. FG: 89.67 INV IN: 87.40 (E)
D2	48" DIA. CLOSED LID MANHOLE RIM: 96.52 INV IN: 90.20 (S) INV IN: 92.20 (E) INV IN: 92.00 (SW) INV IN: 92.00 (SE) INV OUT: 90.20 (W)
D3	48" DIA. OPEN LID MANHOLE. SEE SHEET 11 OF 12 FOR SPECIFICATIONS. RIM: 94.85 INV OUT: 91.44 (N)
D4	48" DIA. OPEN LID MANHOLE. SEE SHEET 11 OF 12 FOR SPECIFICATIONS. RIM: 95.85 INV OUT: 92.50 (W)
D5	STORM OUTFALL TO PROPOSED CHANNEL AT BASE OF WALL. SEE DETAIL ON SHEET 12 OF 12. FG: 91.35 INV IN: 90.00 (E)
D6	4" STORM CLEANOUT RIM: 94.25 INV OUT: 90.22 (W)
D7	4" STORM CLEANOUT RIM: 95.10 INV OUT: 92.00 (N)
D8	4" STORM CLEANOUT RIM: 95.16 INV OUT: 92.00 (N)

### CITY OF FORT COLLINS UTILITY PLAN APPROVAL

APPROVED: \_\_\_\_\_ CITY ENGINEER \_\_\_\_\_ DATE \_\_\_\_\_

CHECKED BY: \_\_\_\_\_ WATER AND WASTEWATER UTILITY \_\_\_\_\_ DATE \_\_\_\_\_

CHECKED BY: \_\_\_\_\_ STORMWATER UTILITY \_\_\_\_\_ DATE \_\_\_\_\_

CHECKED BY: \_\_\_\_\_ PARKS AND RECREATION \_\_\_\_\_ DATE \_\_\_\_\_

CHECKED BY: \_\_\_\_\_ TRAFFIC ENGINEER \_\_\_\_\_ DATE \_\_\_\_\_

CHECKED BY: \_\_\_\_\_ \_\_\_\_\_ DATE \_\_\_\_\_

LEN	02/07/17	COMMENTS	
LEN	05/02/17	PLAN REVISION	
LEN	06/09/17	REVISION	

SCALE: AS NOTED  
DESIGNED BY: JPM  
DRAWN BY: JPM  
CHECKED BY: LEN

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**CORE**

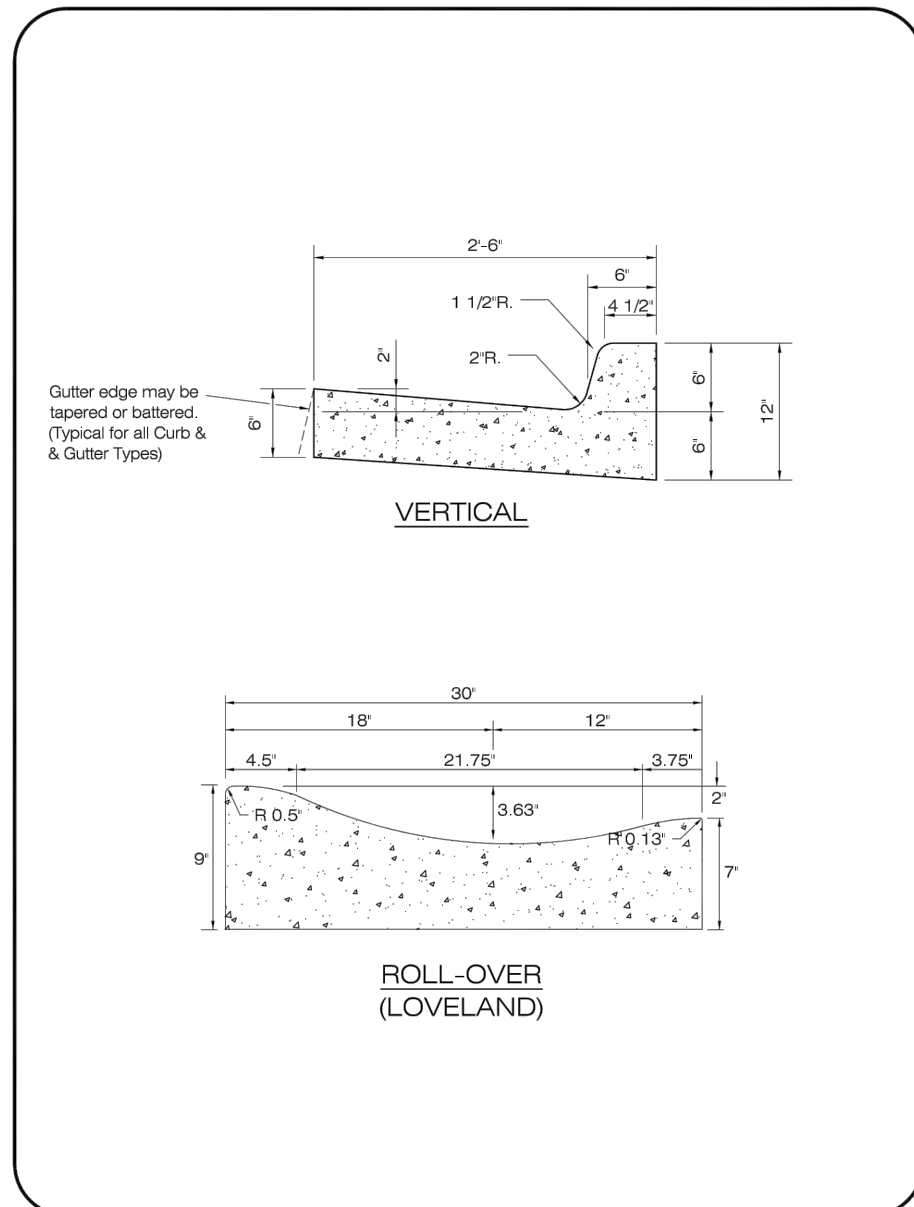
UTILITY PLAN

CHOICE CENTER THIRD FILING  
FORT COLLINS, CO 80525

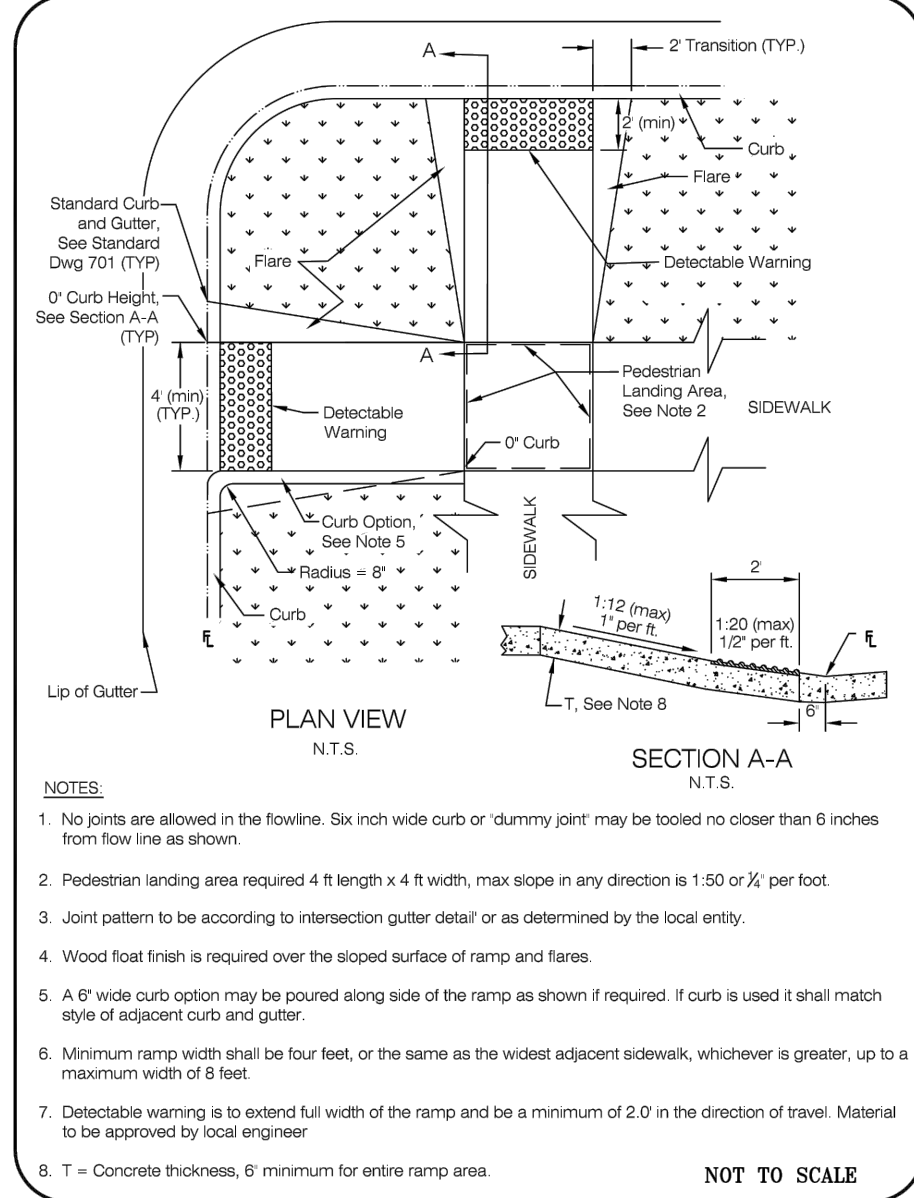
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KHA PROJECT NO. 168524002  
SHEET NUMBER 9 OF 12

Drawing name: K:\GIS\_LIVE\16846002\_CORE\_FortCollins\_CO\_College\_2 Design\CAD\PlanSheets\UTILITY PLAN\9 OF 12 - UTILITY PLAN.dwg 9 OF 12 UTILITY PLAN Jun 09, 2017 11:34am by: steven.mahline  
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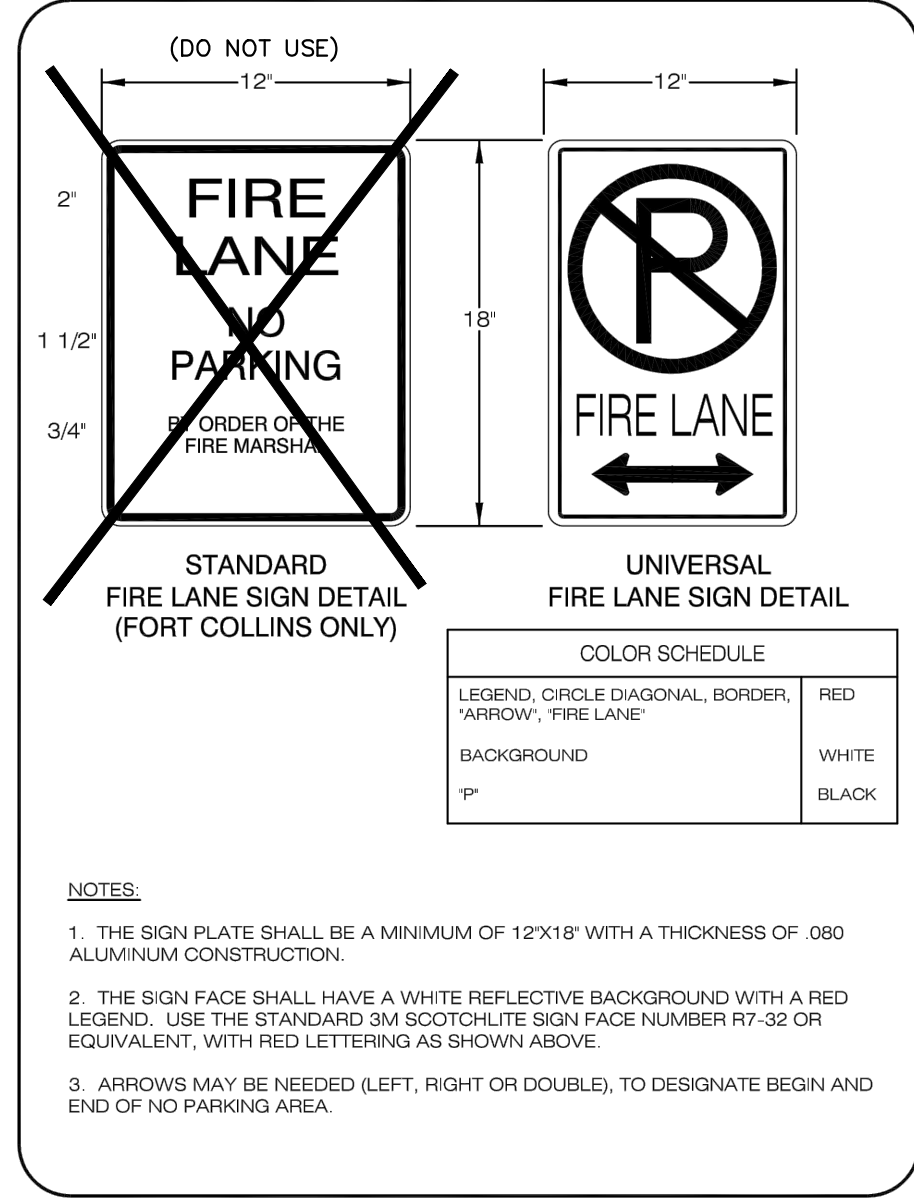
# CHOICE CENTER THIRD FILING



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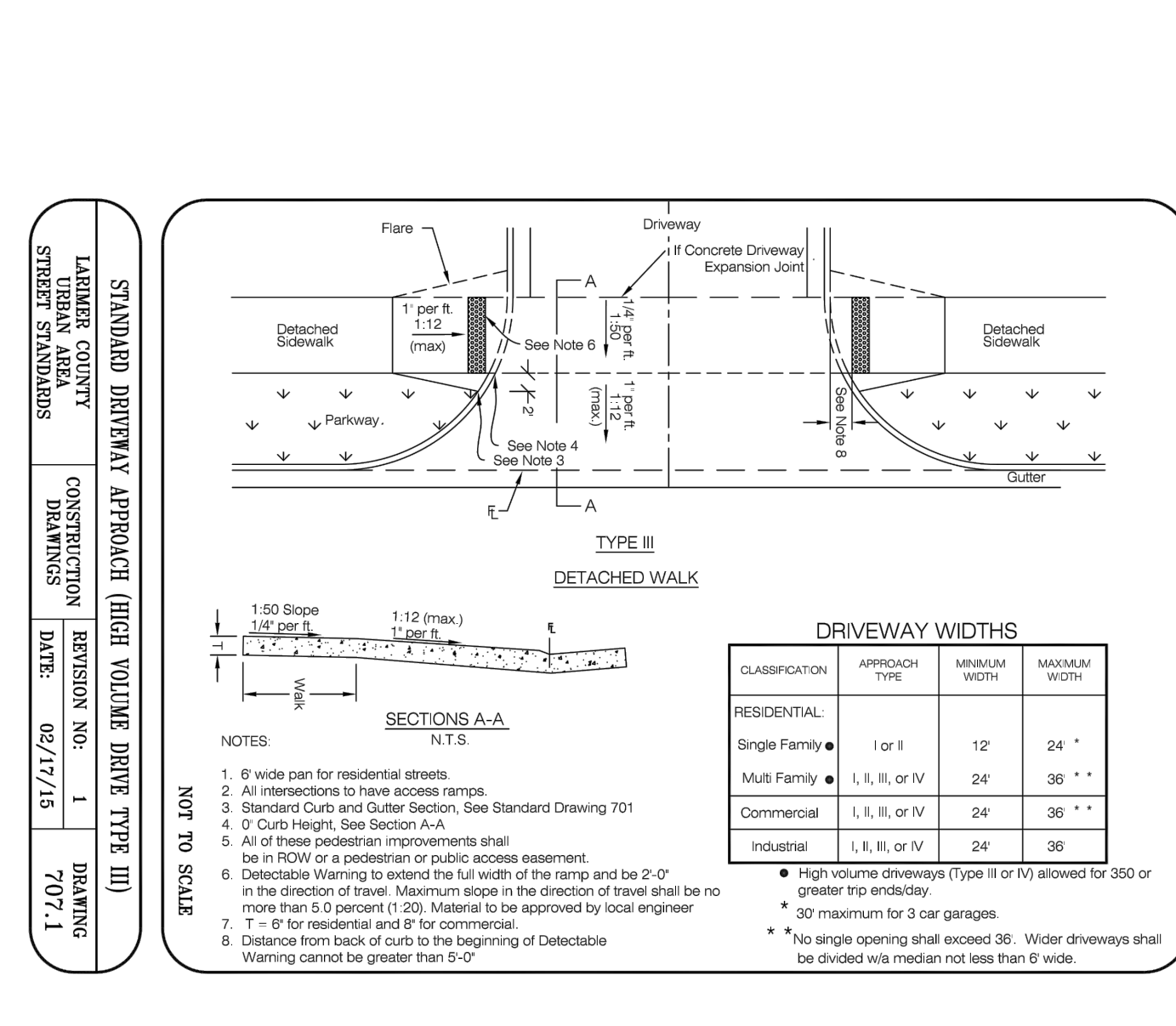


**DECORATIVE CONC. CROSSWALK (PATTERNED/COLORED CONC.)**  
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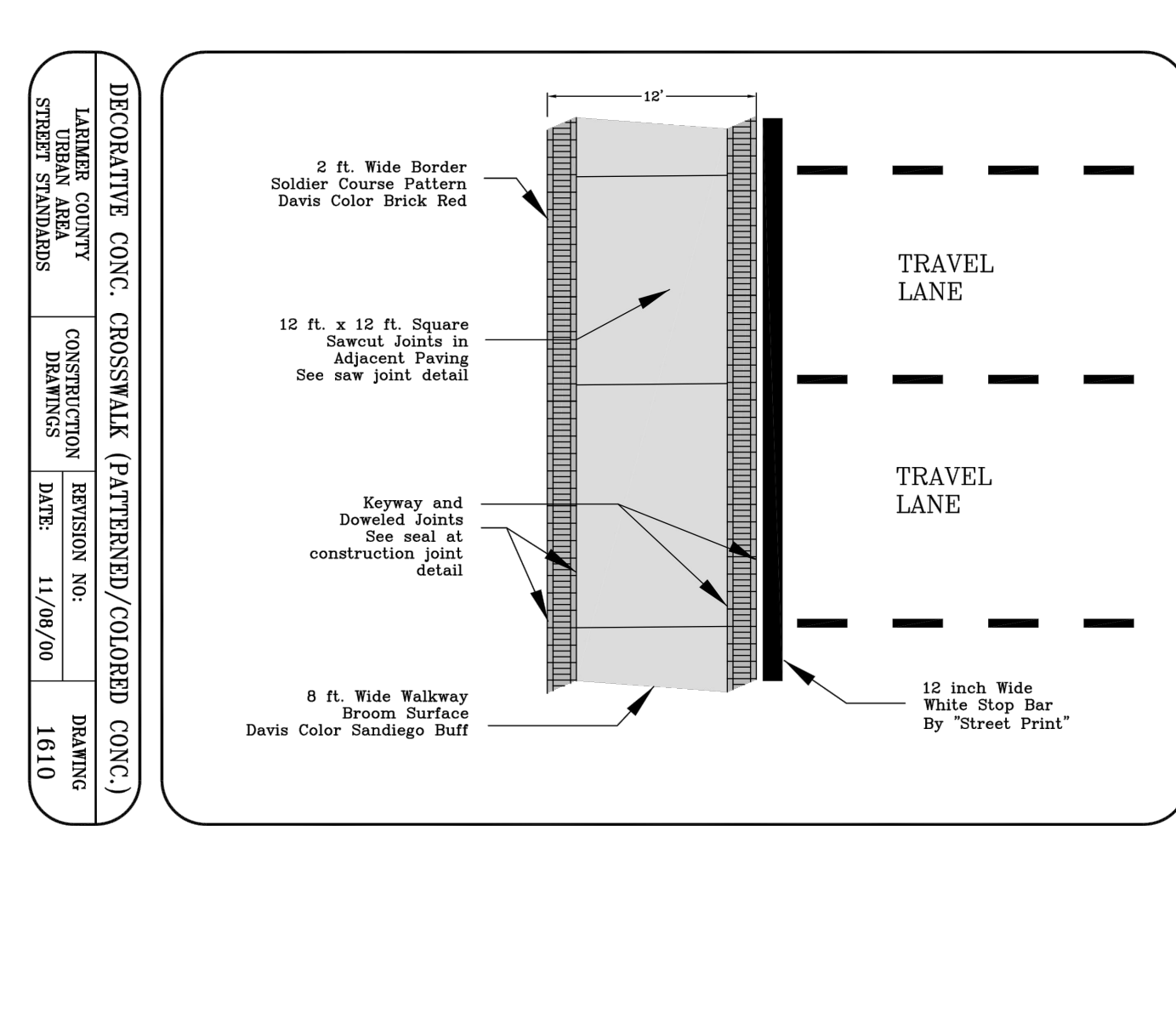


**DIRECTIONAL ACCESS RAMP DETAIL & DETACHED SIDEWALK**  
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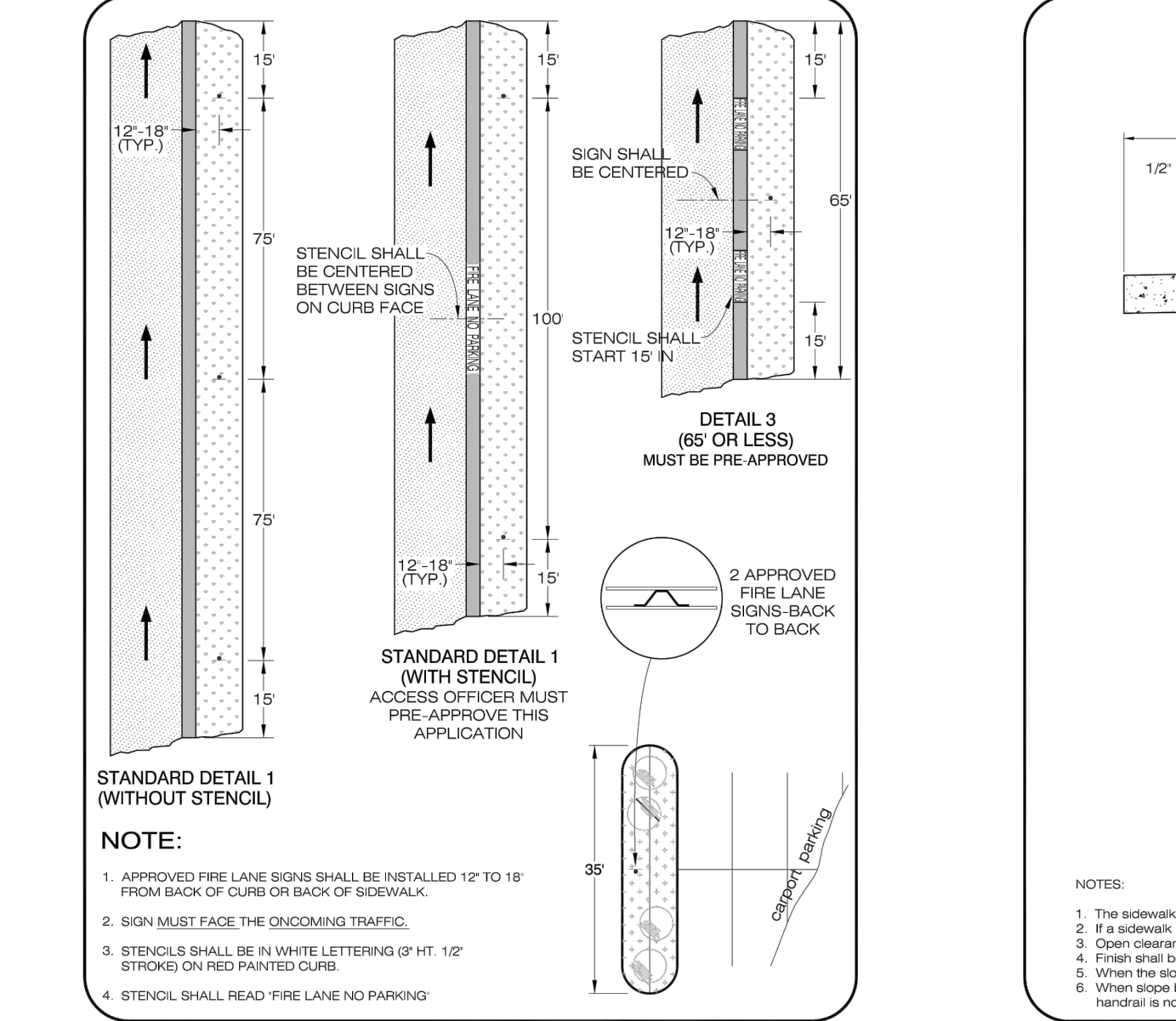
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LARIMER COUNTY URBAN AREA STREET STANDARDS CONSTRUCTION DRAWINGS REVISION NO: 2 DRAWING DATE: 04/01/07 1418



**STANDARD DRIVEWAY APPROACH (HIGH VOLUME DRIVE TYPE III)**  
LARIMER COUNTY URBAN AREA STREET STANDARDS CONSTRUCTION DRAWINGS REVISION NO: 1 DRAWING DATE: 02/17/15 707.1

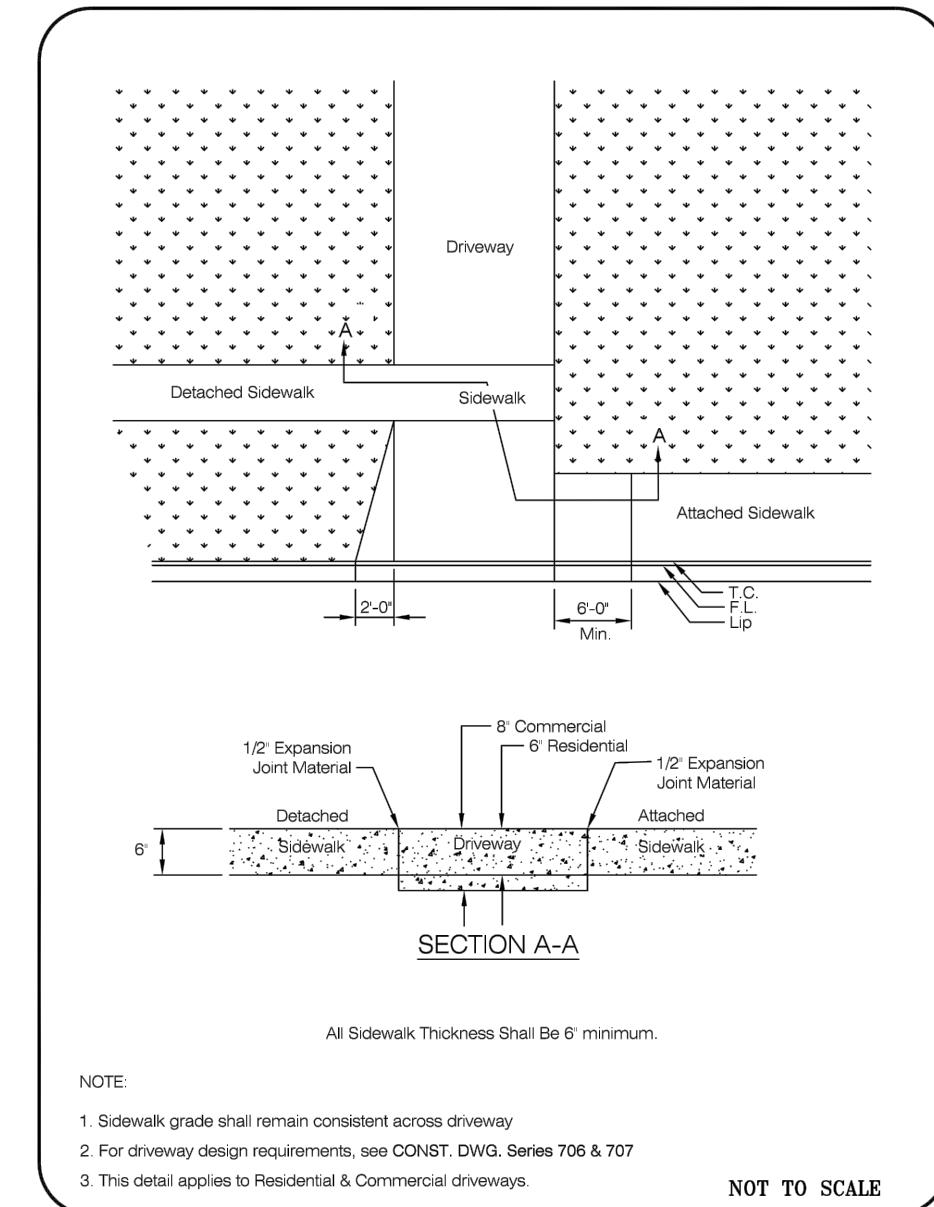


**TRUNCATED DOME WARNING FOR ACCESS RAMP**  
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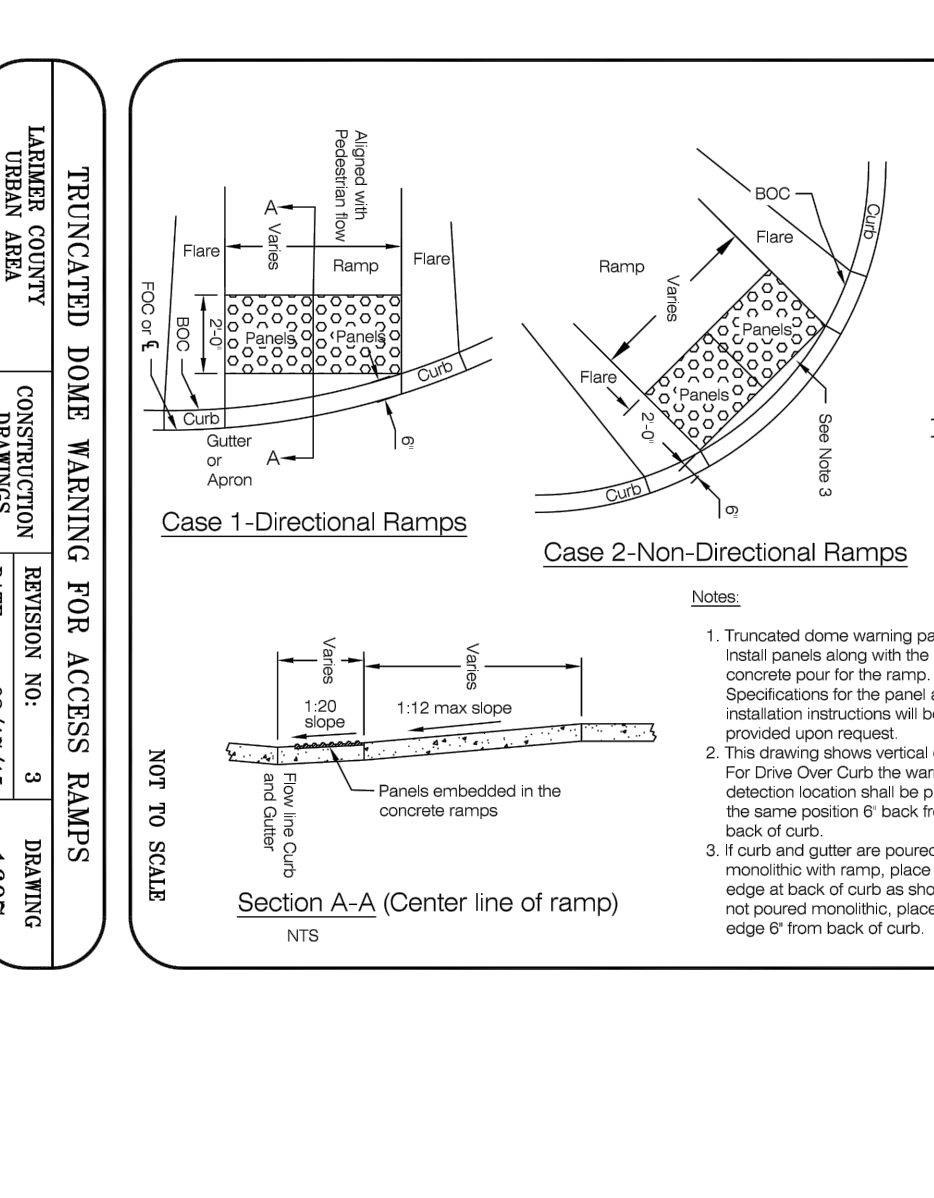


**HANDRAIL DETAILS**  
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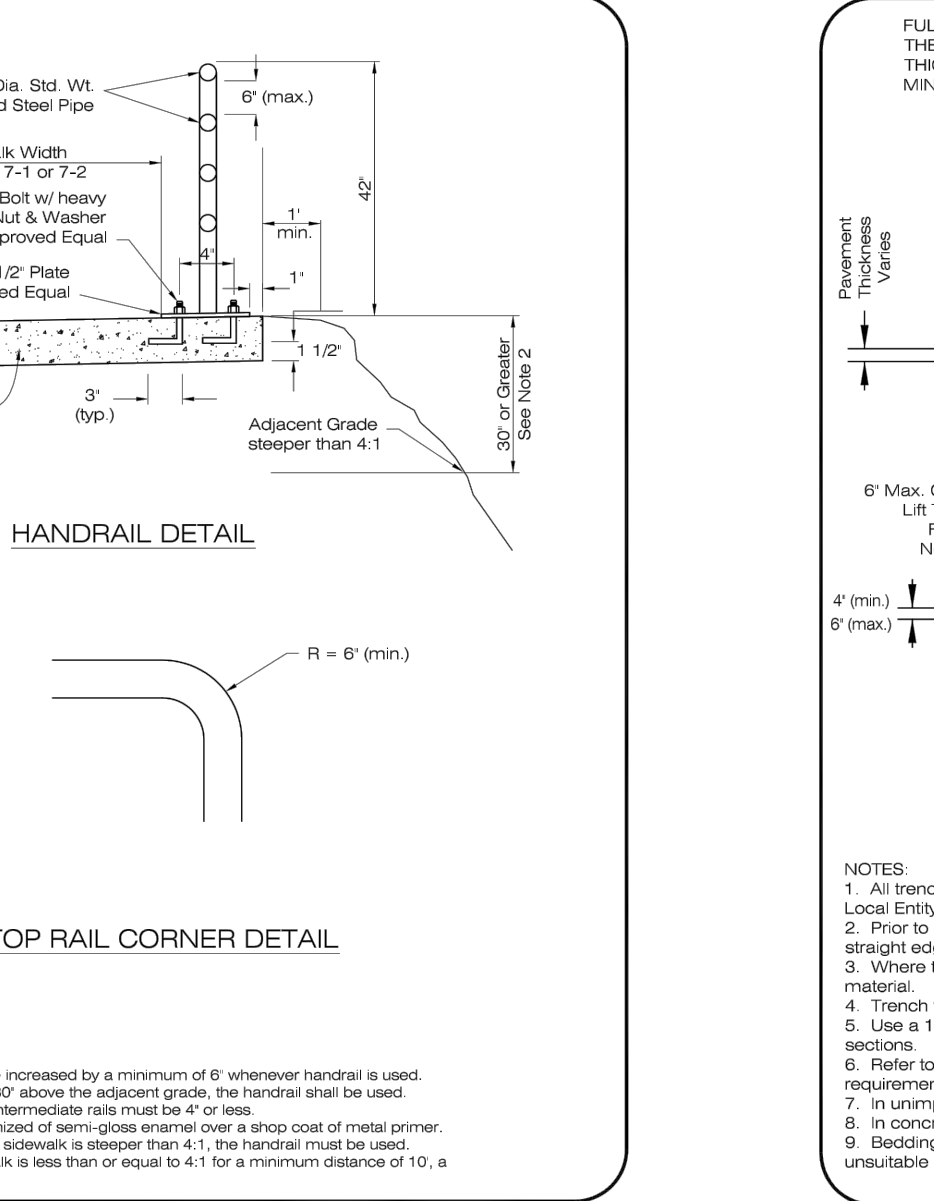
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LARIMER COUNTY URBAN AREA STREET STANDARDS CONSTRUCTION DRAWINGS REVISION NO: 1 DRAWING DATE: 04/01/07 2201



**ATTACHED SIDEWALK DETAIL**  
LARIMER COUNTY URBAN AREA STREET STANDARDS CONSTRUCTION DRAWINGS REVISION NO: 4 DRAWING DATE: 02/17/15 1601

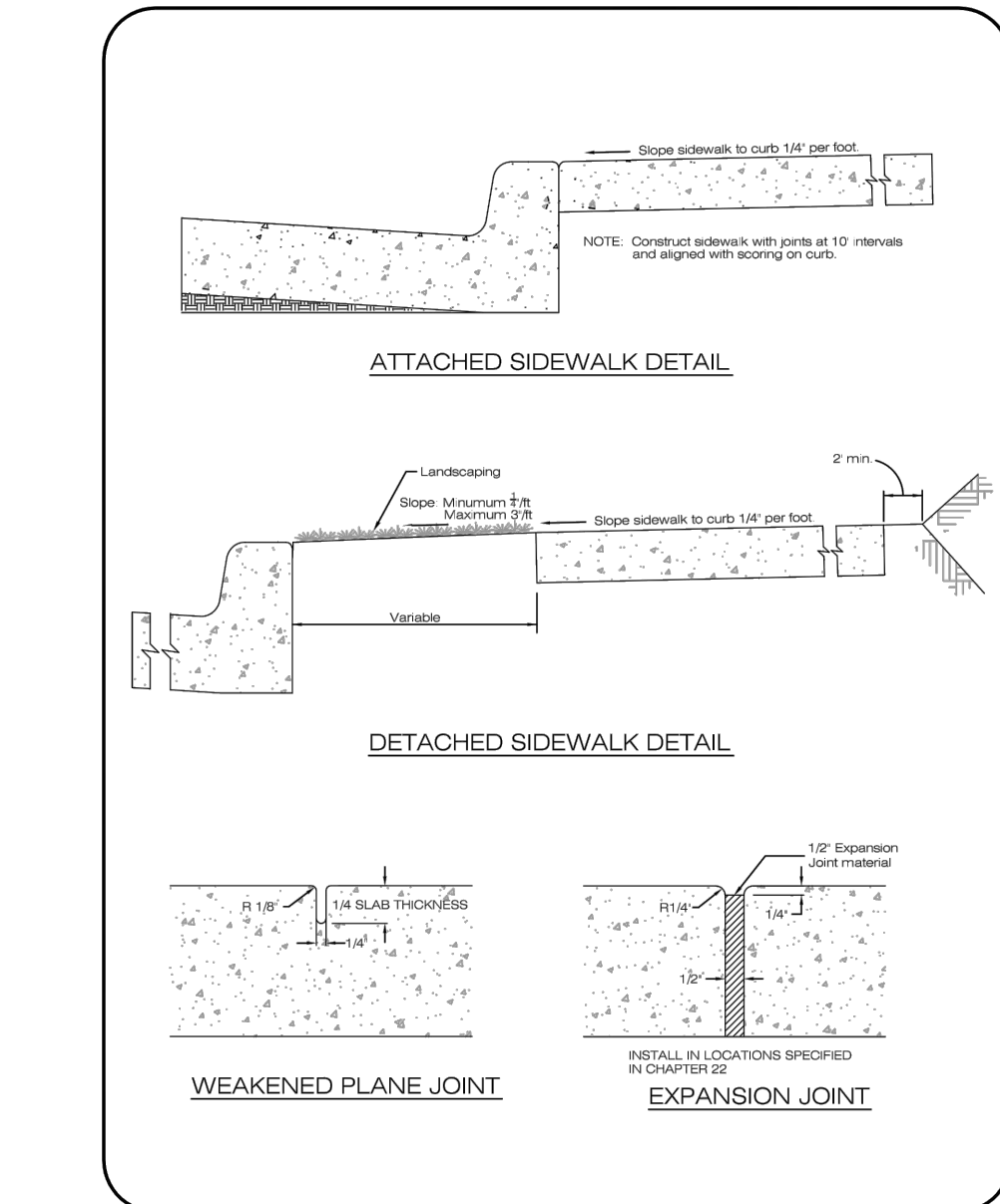


**JACK & BORE**  
LARIMER COUNTY URBAN AREA STREET STANDARDS CONSTRUCTION DRAWINGS REVISION NO: 2 DRAWING DATE: 04/01/07 1602

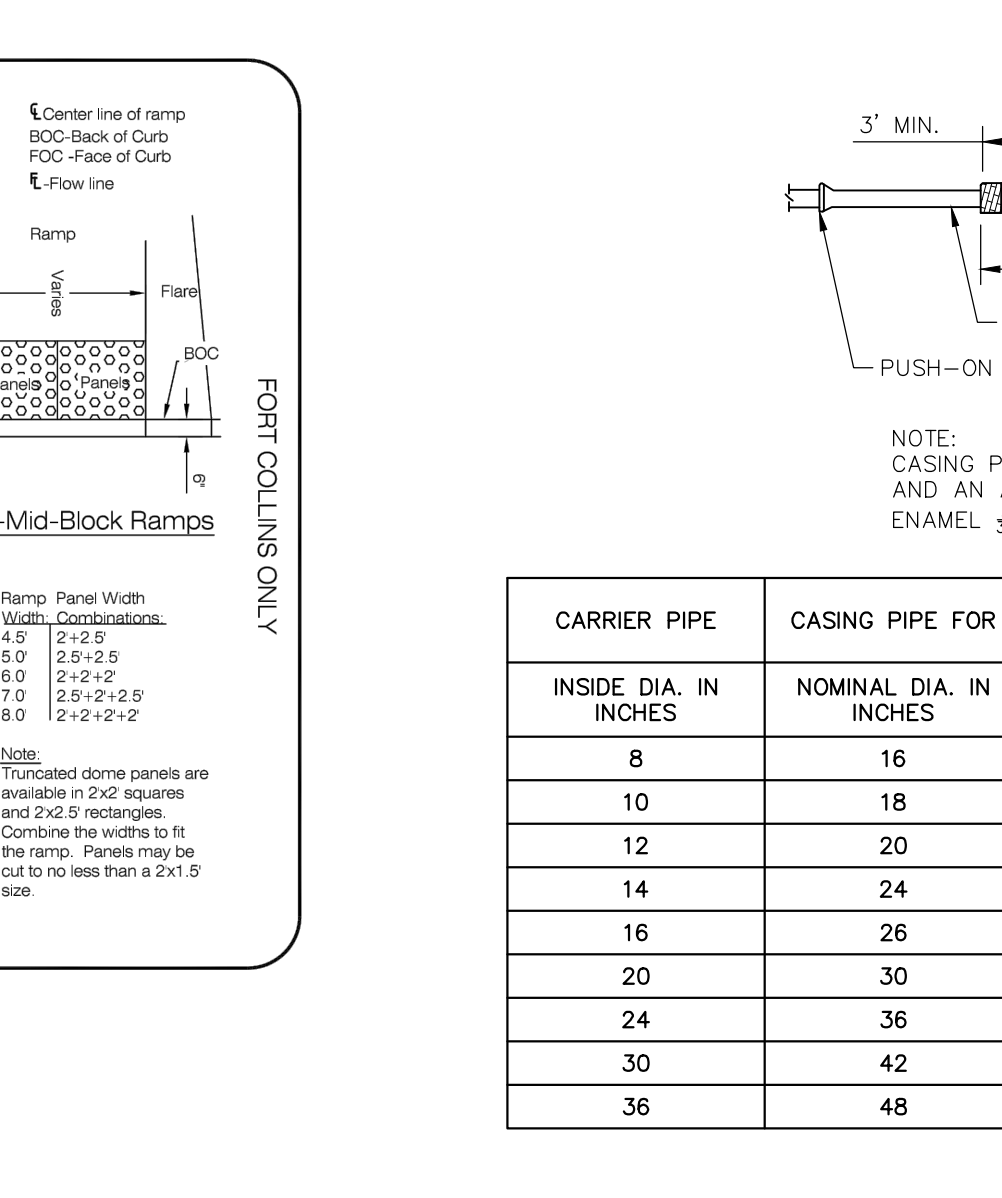


**STANDARD SIDEWALK**  
LARIMER COUNTY URBAN AREA STREET STANDARDS CONSTRUCTION DRAWINGS REVISION NO: 2 DRAWING DATE: 02/17/15 1603

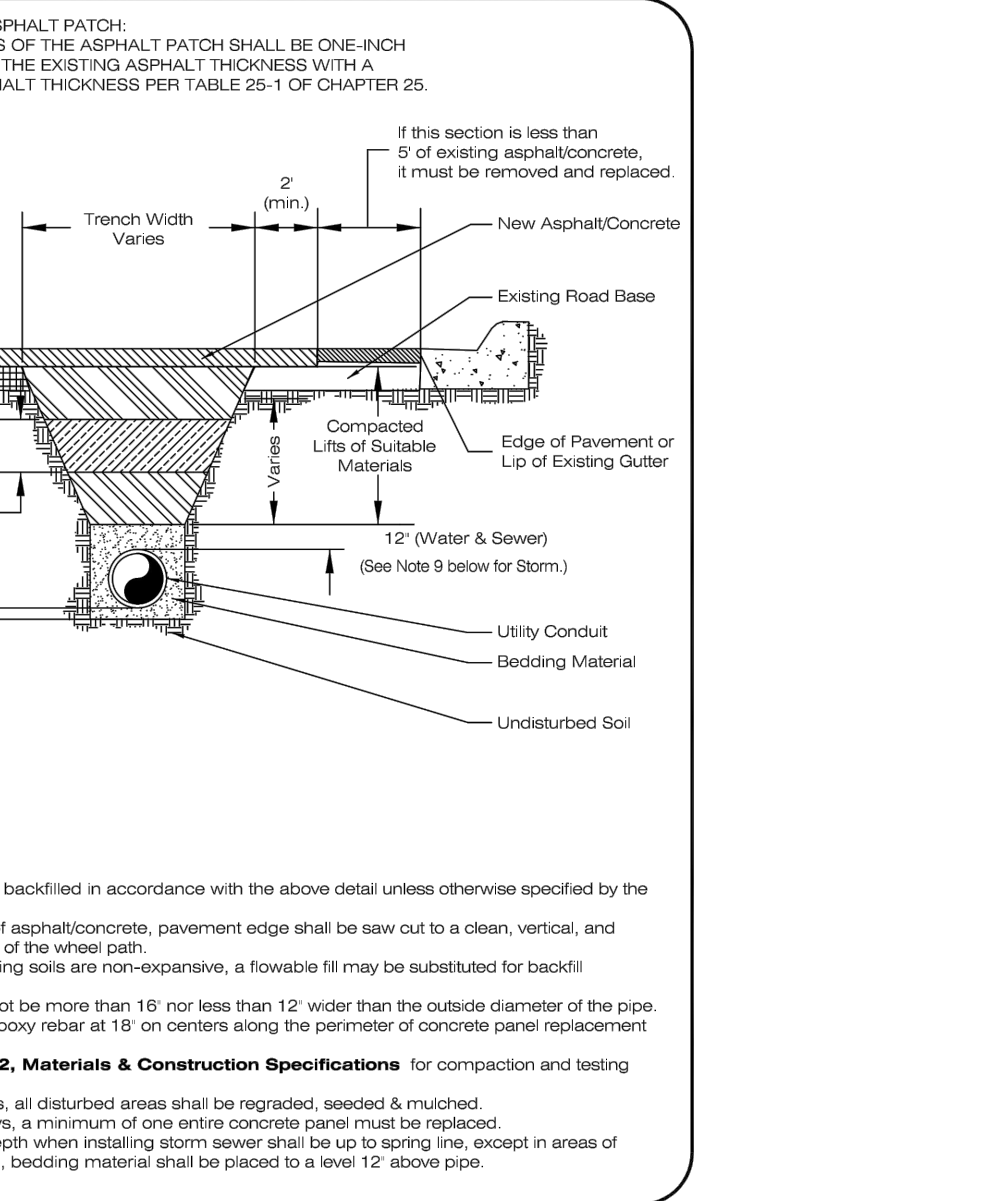
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**CORNER LOCATION**  
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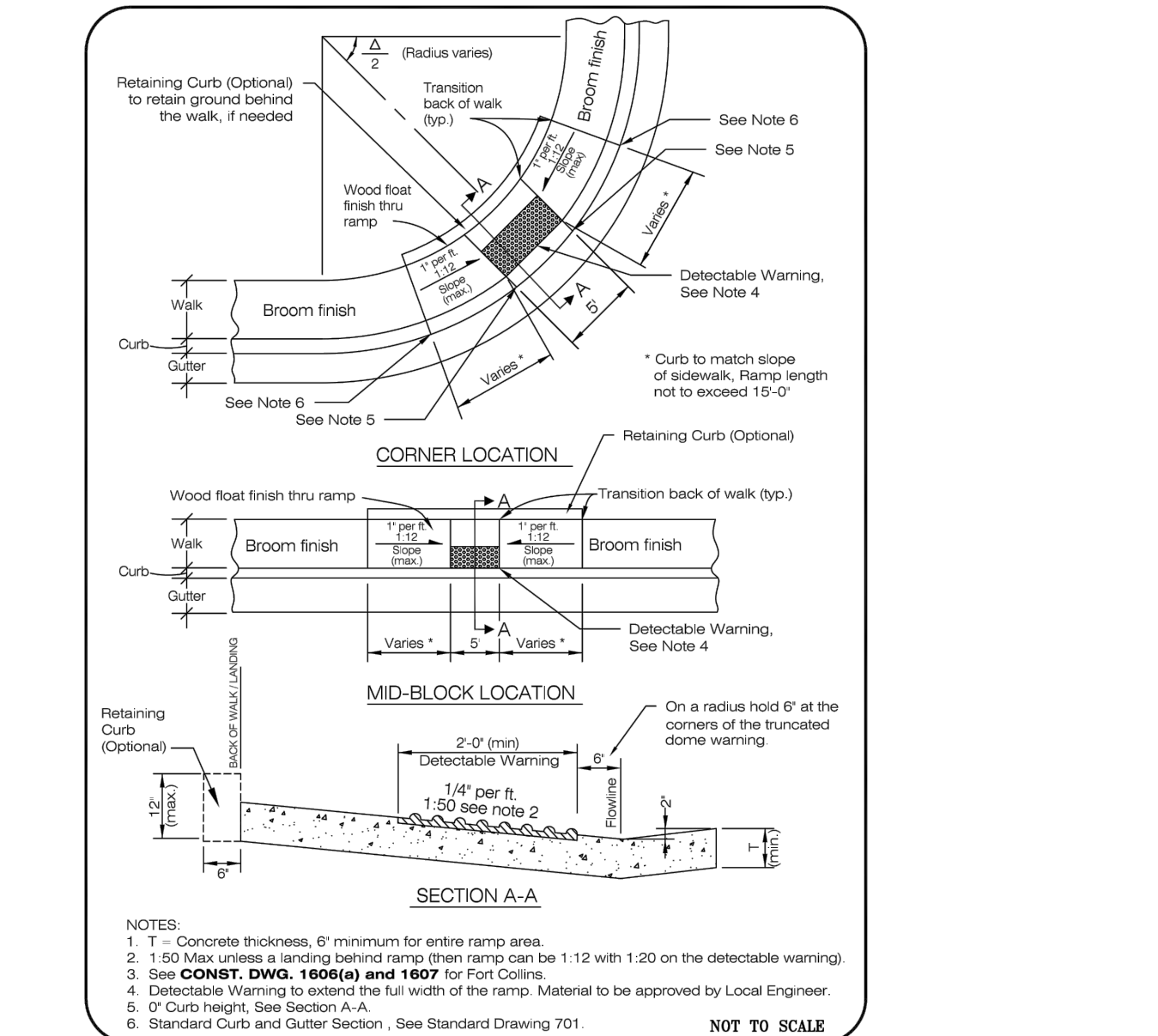


**TYPICAL HIGHWAY CROSSING**  
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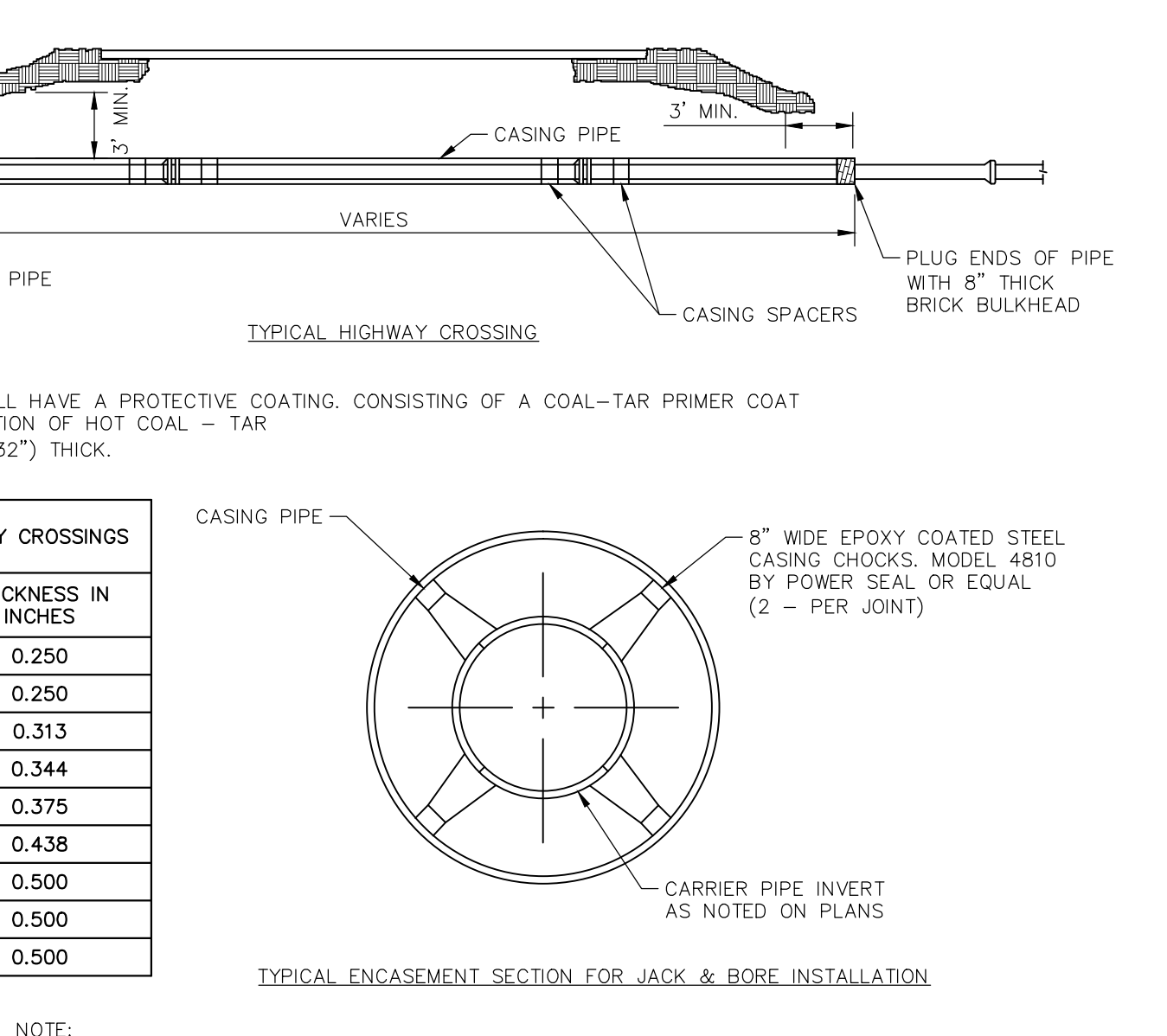


**HANDRAIL DETAILS**  
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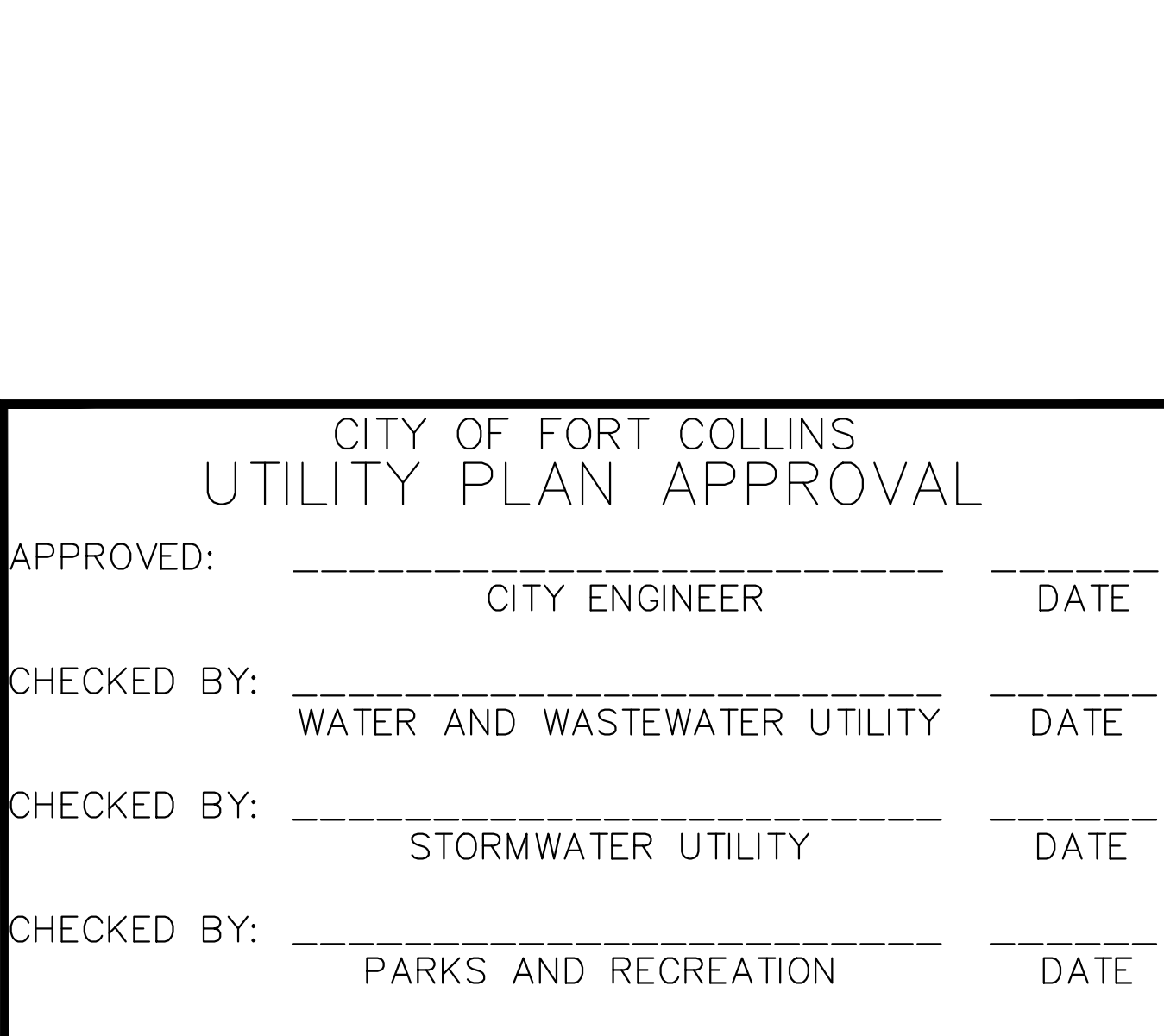
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**DECORATIVE CONC. CROSSWALK (PATTERNED/COLORED CONC.)**  
LARIMER COUNTY URBAN AREA STREET STANDARDS CONSTRUCTION DRAWINGS REVISION NO: 3 DRAWING DATE: 02/17/15 1607



**DIRECTIONAL ACCESS RAMP DETAIL & DETACHED SIDEWALK**  
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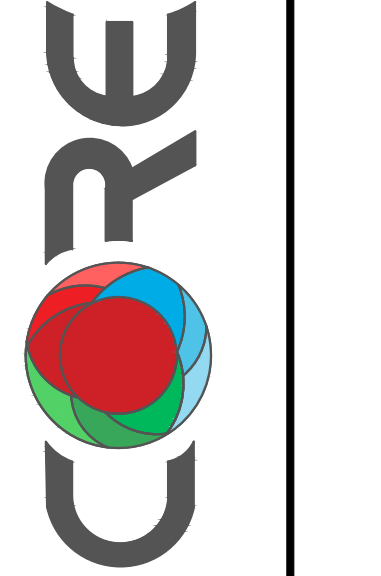


**FIRE ACCESS ROAD SIGNS**  
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**FIRE APPARATUS ACCESS ROAD STENCILING & FIRE LANE SIGN INSTALLATION**  
LARIMER COUNTY URBAN AREA STREET STANDARDS CONSTRUCTION DRAWINGS REVISION NO: 1 DRAWING DATE: 02/27/02 1419

SCALE:	AS NOTED	DESIGNED BY: JPM	DRAWN BY: JPM	CHECKED BY: LEN
02/07/17	LEN			
05/02/17	REVISION			
06/09/17	REVISION			

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1001 WARRENVILLE ROAD, SUITE 350,  
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## CONSTRUCTION DETAILS

CHOICE CENTER THIRD FILING  
FORT COLLINS, CO 80525

ORIGINAL ISSUE: 12/07/2016  
KHA PROJECT NO. 168524002  
SHEET NUMBER 10 OF 12

**CITY OF FORT COLLINS UTILITY PLAN APPROVAL**

APPROVED: \_\_\_\_\_ DATE \_\_\_\_\_  
CITY ENGINEER

CHECKED BY: \_\_\_\_\_ DATE \_\_\_\_\_  
WATER AND WASTEWATER UTILITY

CHECKED BY: \_\_\_\_\_ DATE \_\_\_\_\_  
STORMWATER UTILITY

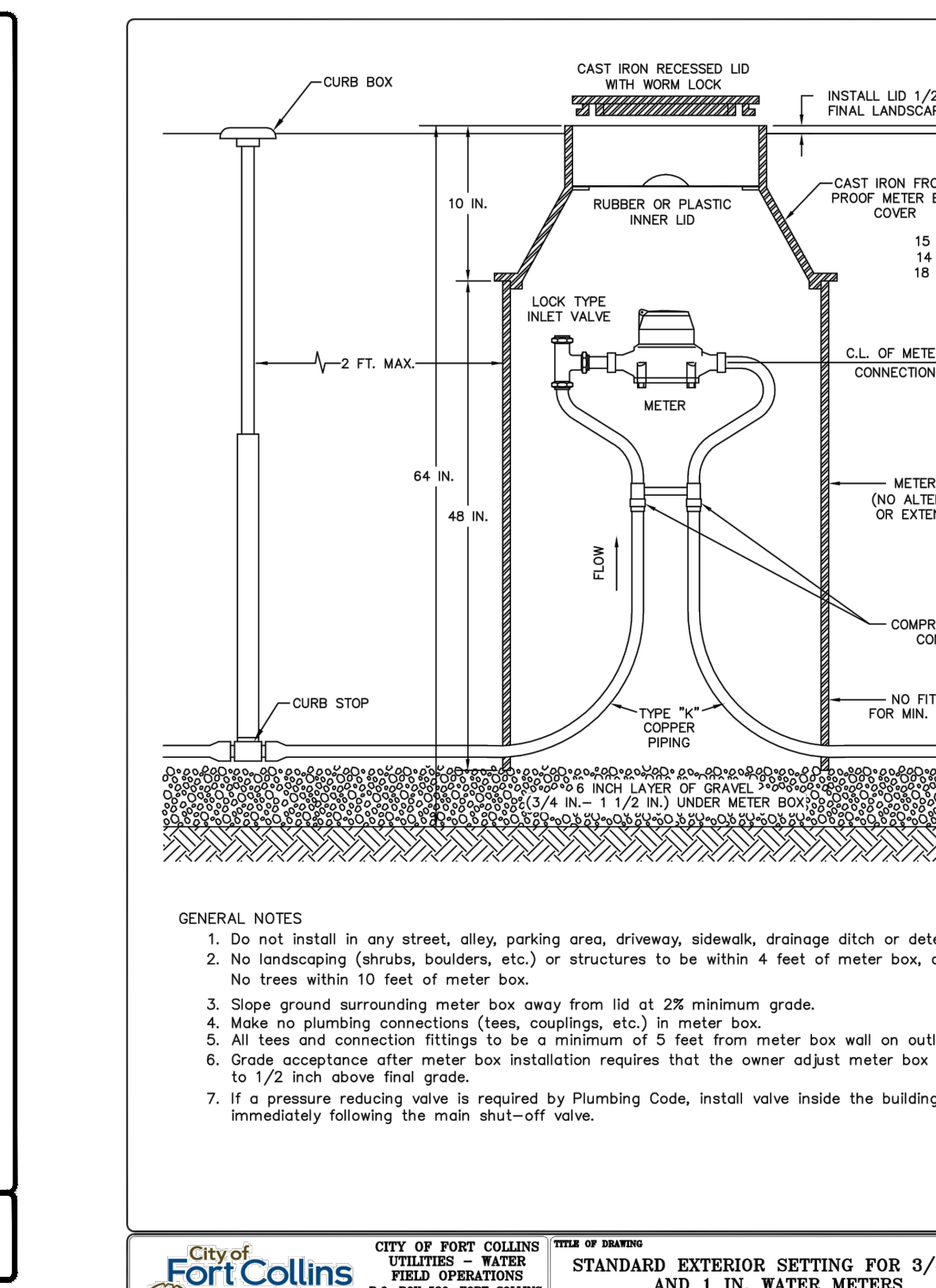
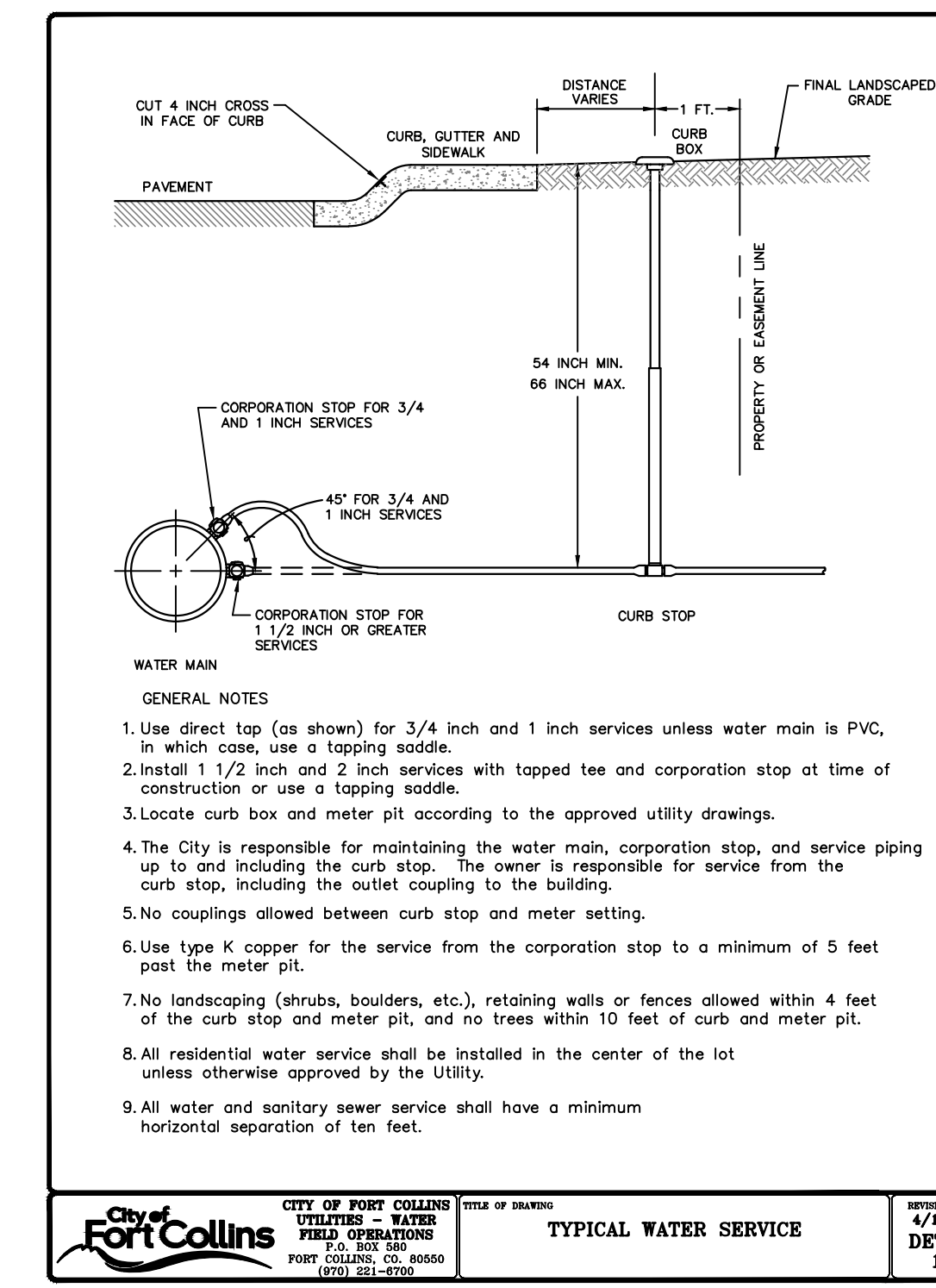
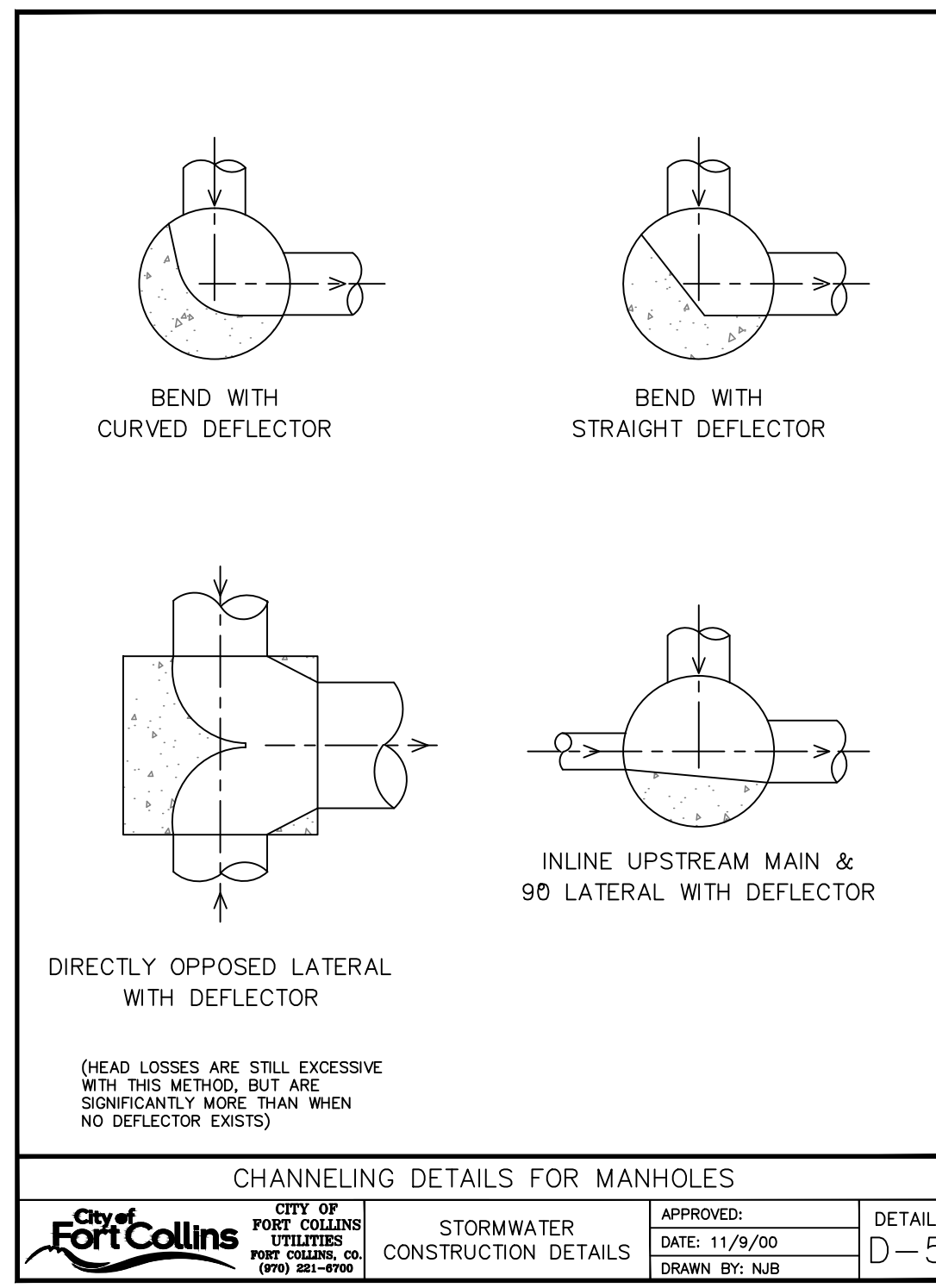
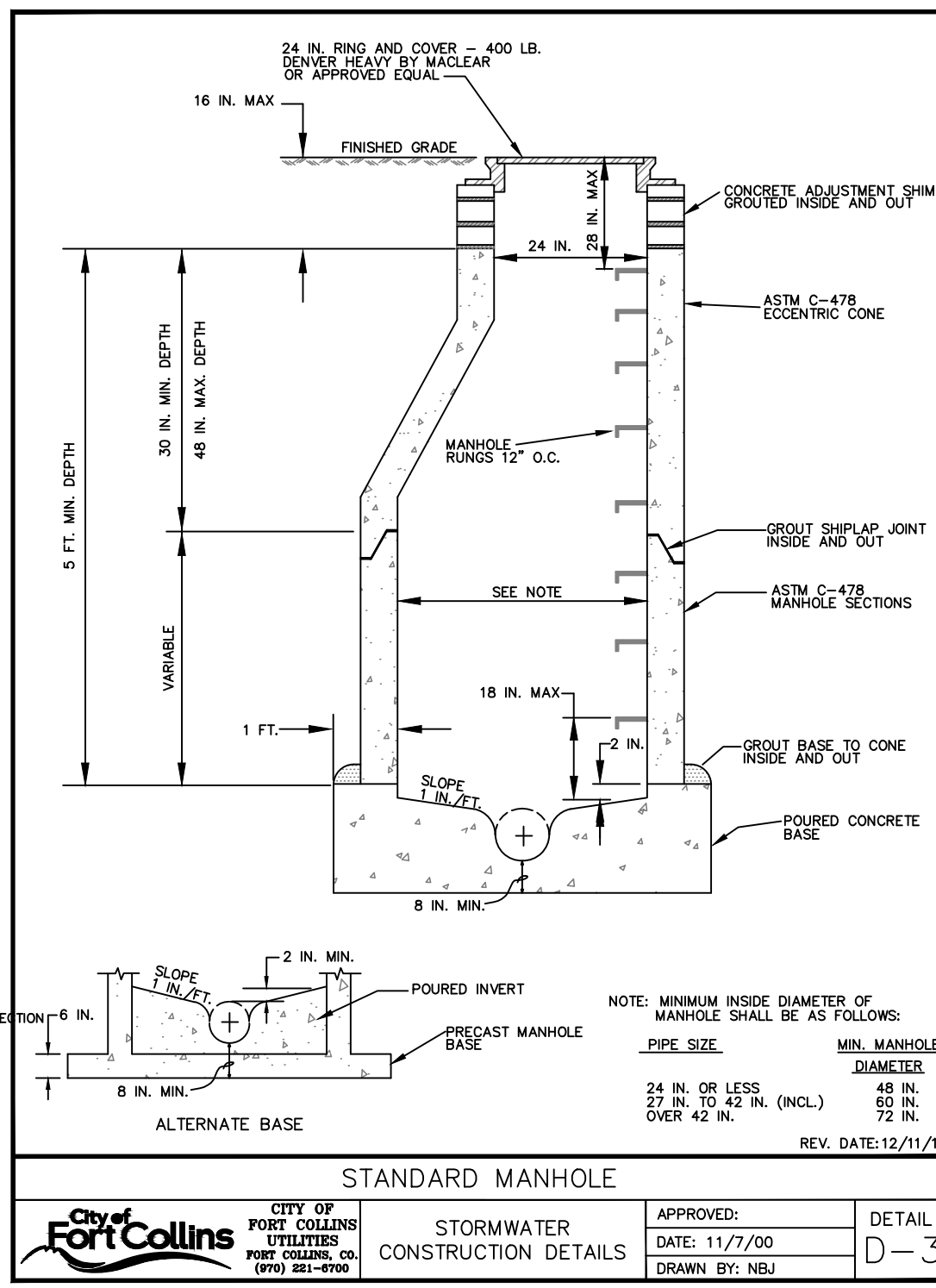
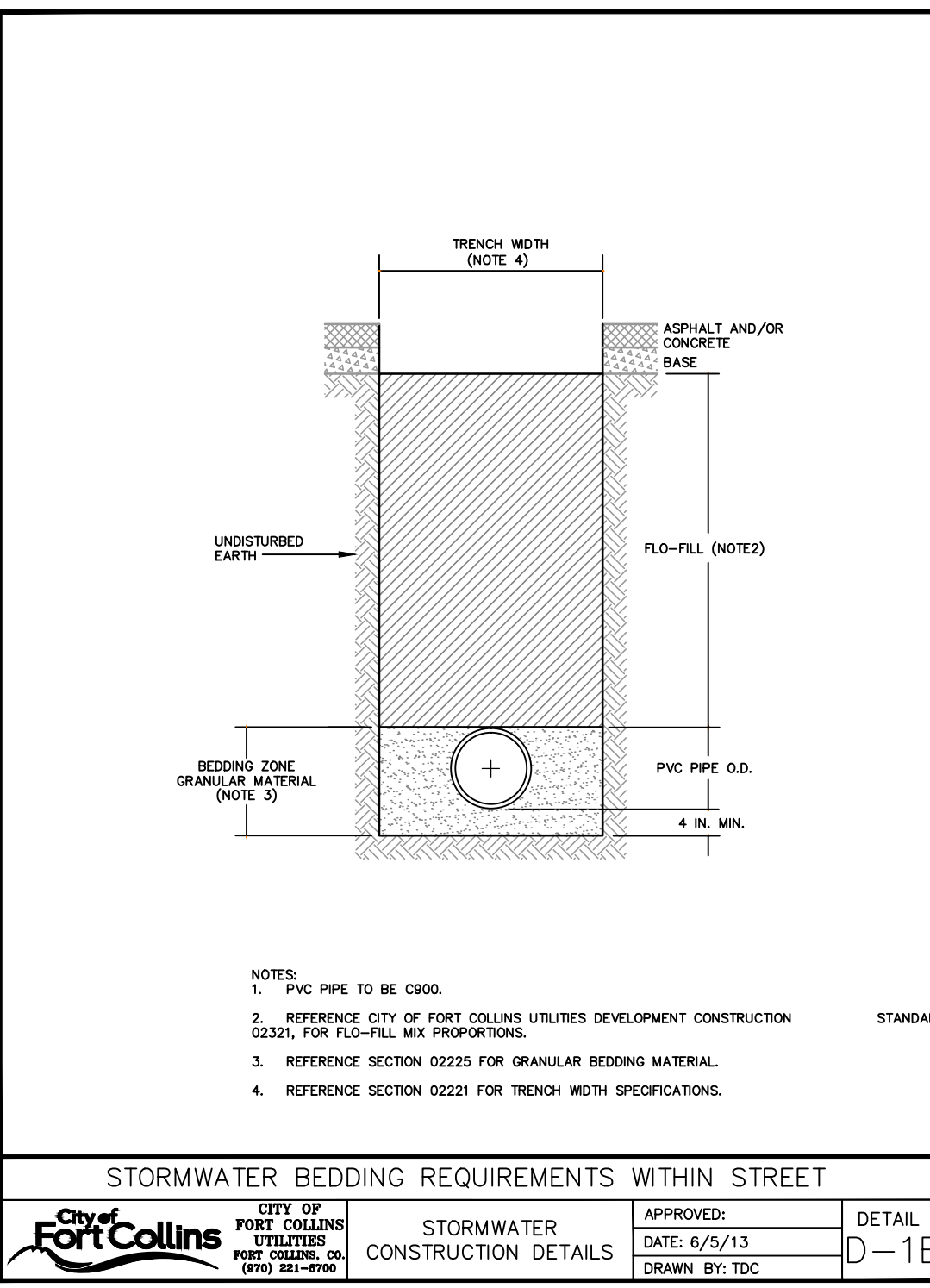
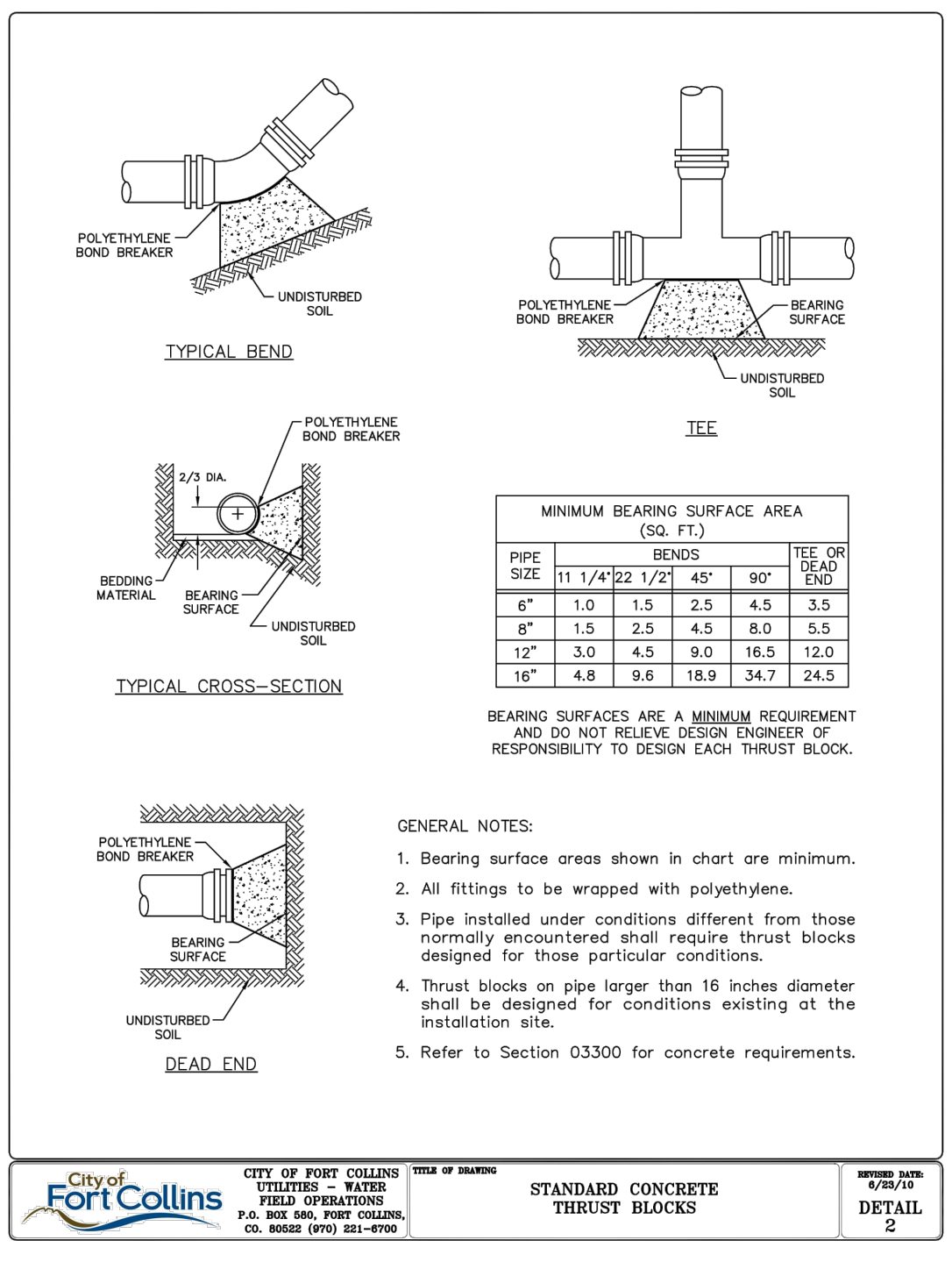
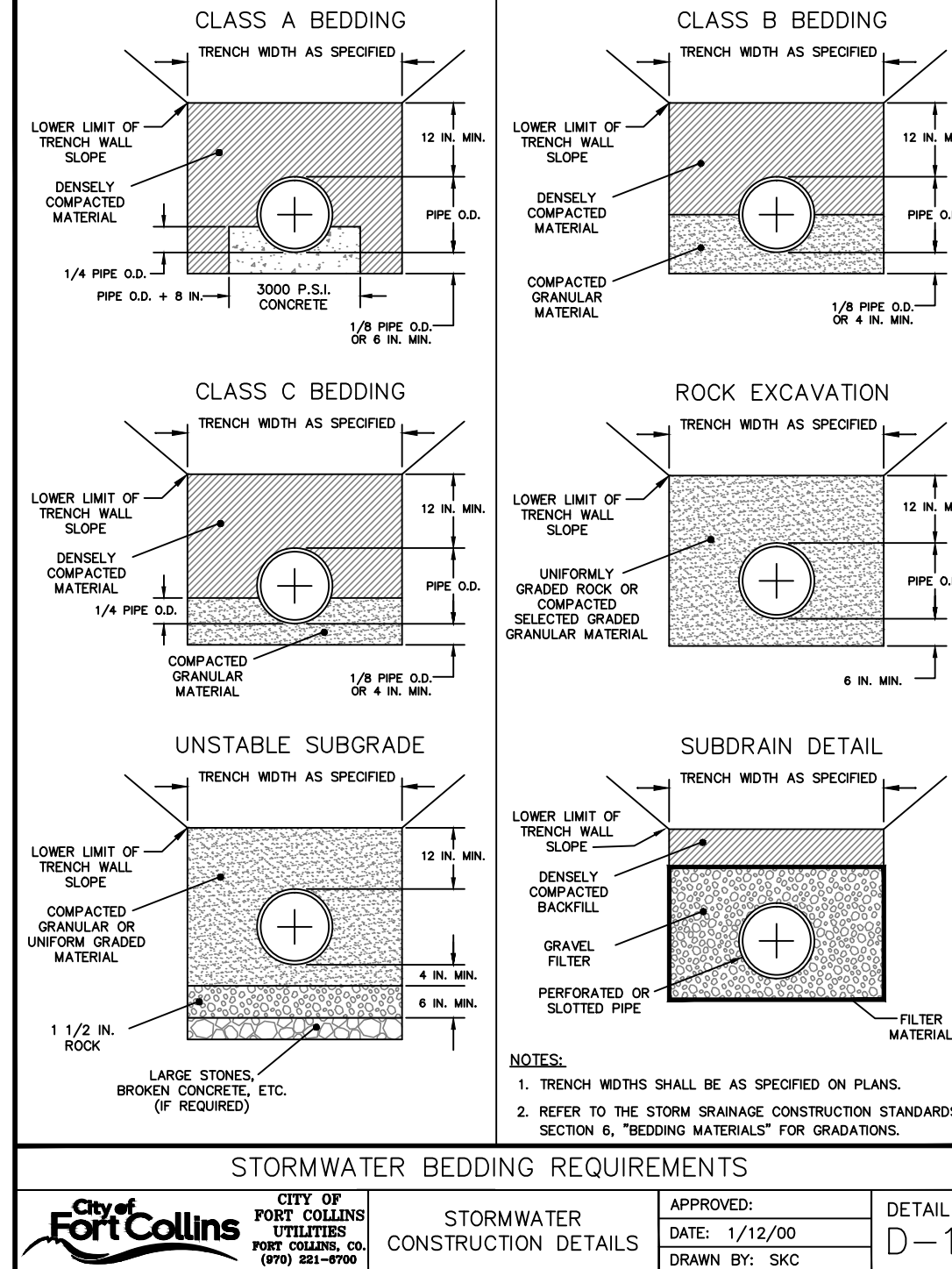
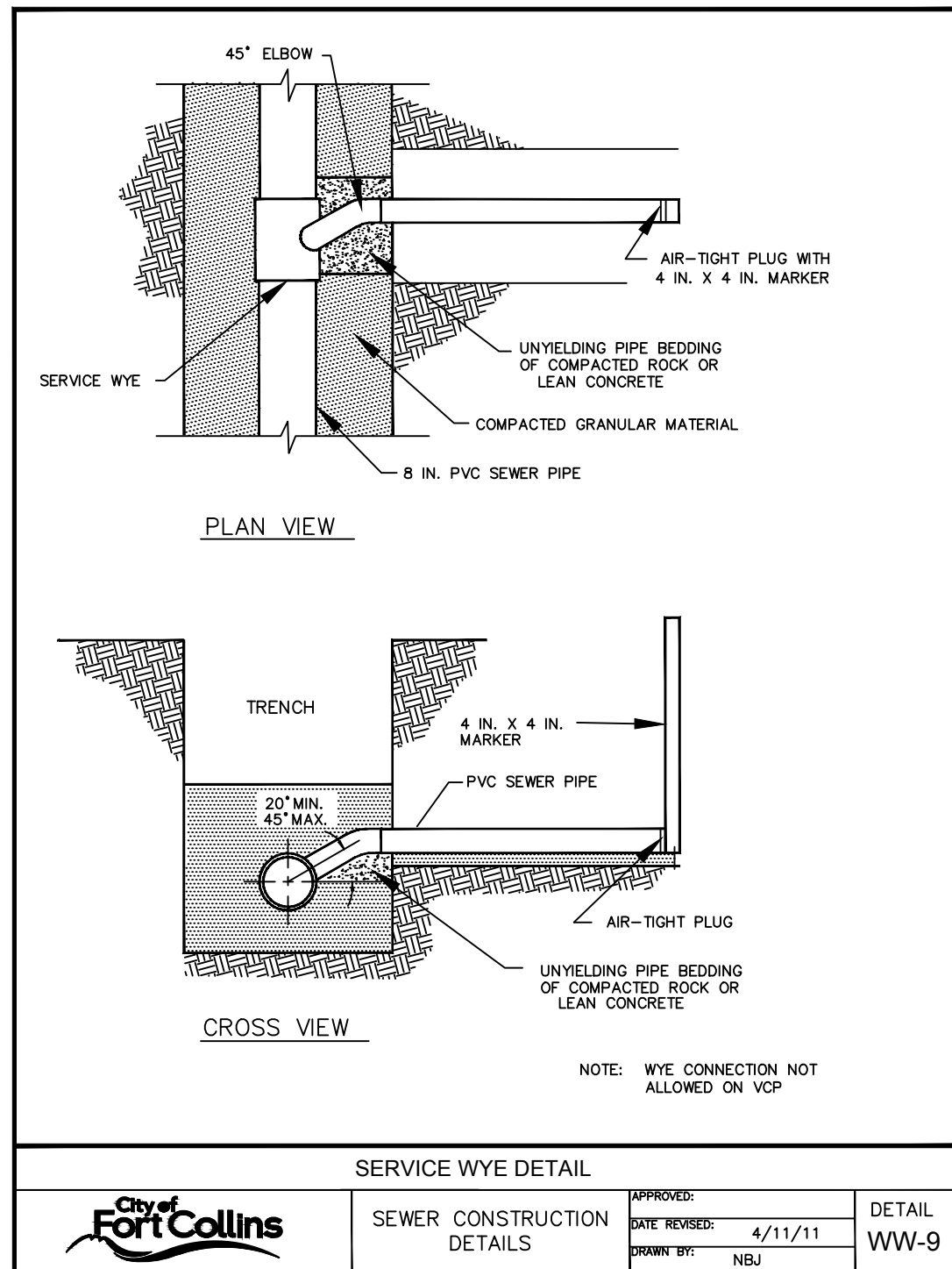
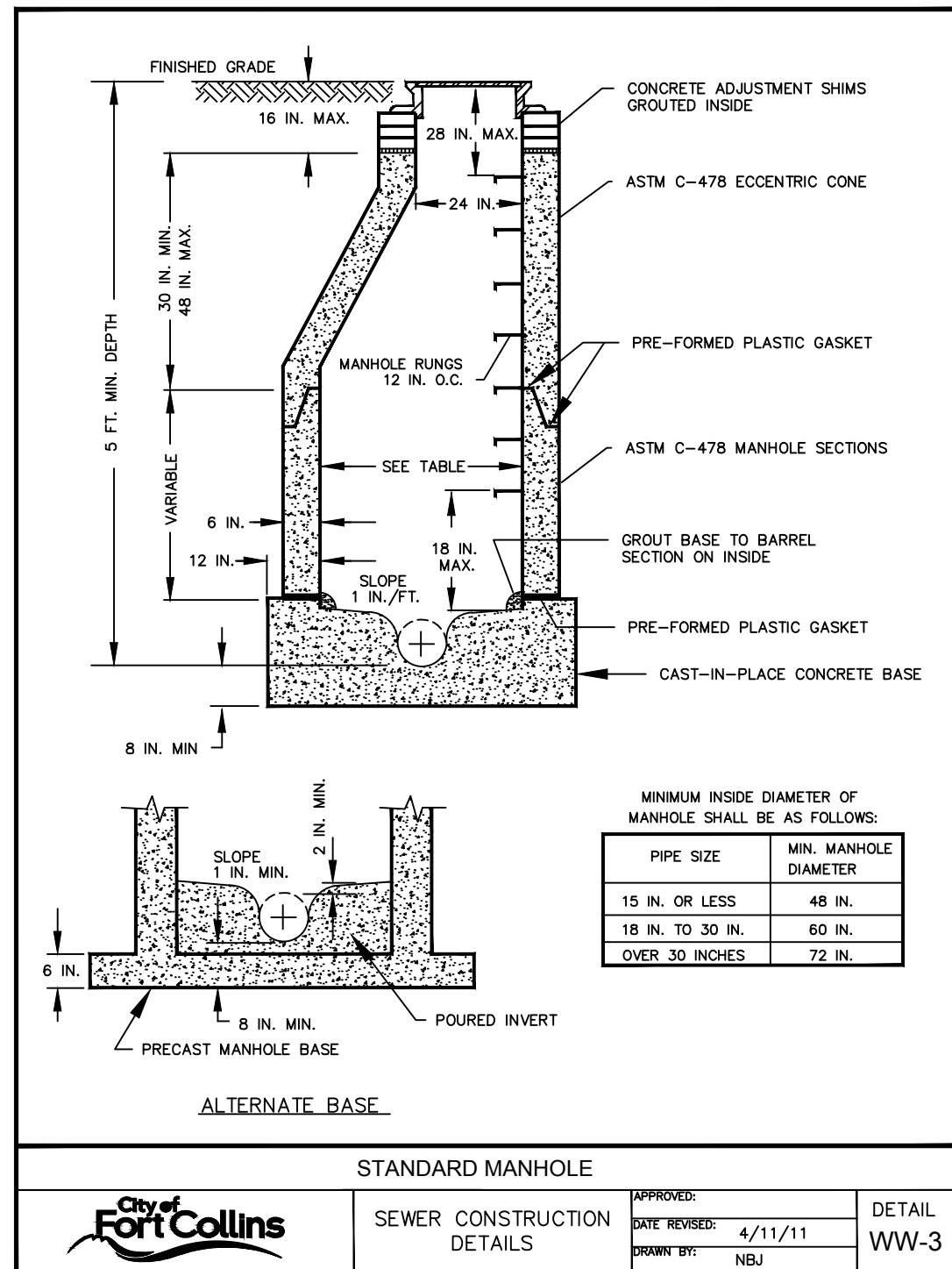
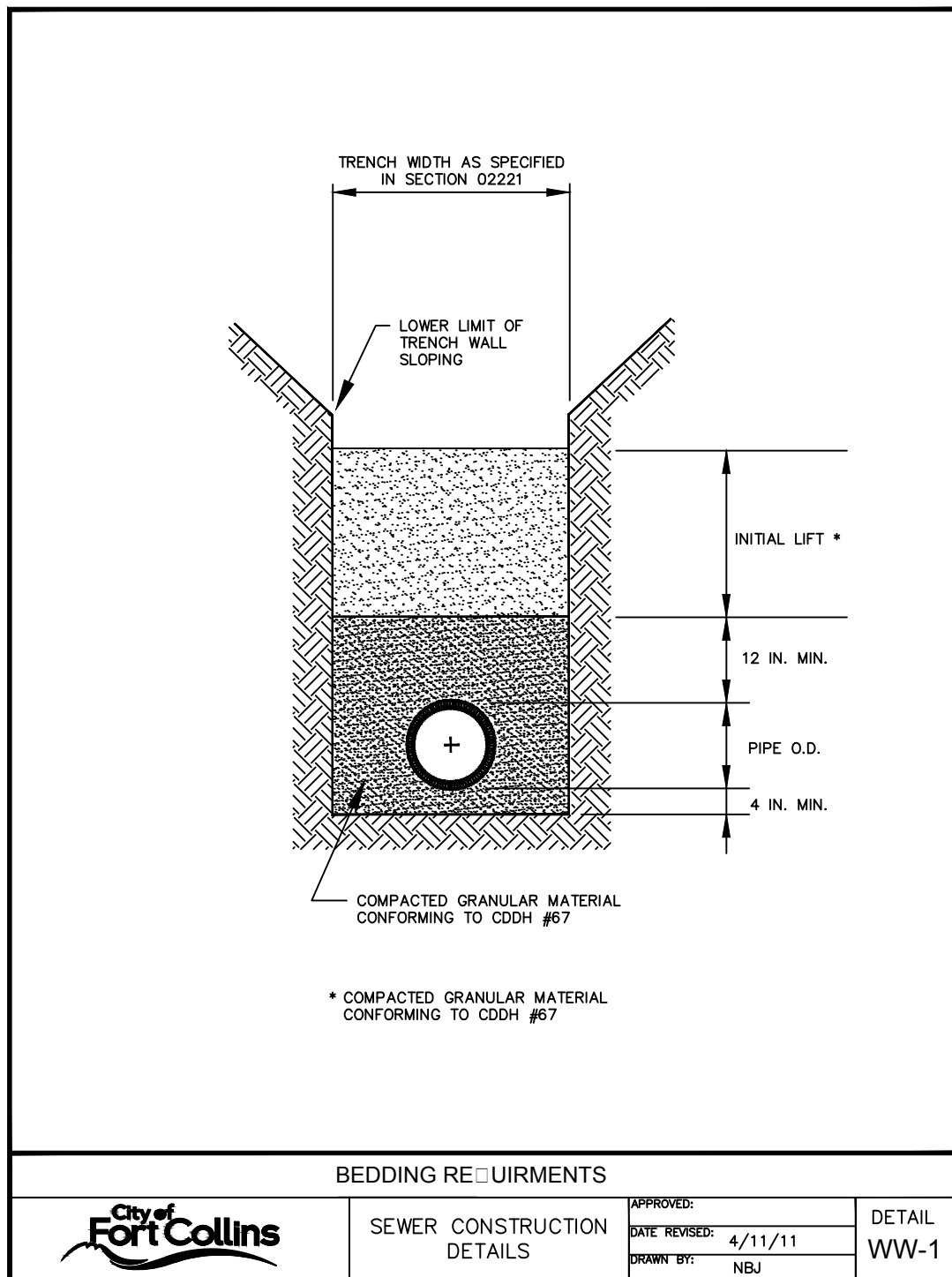
CHECKED BY: \_\_\_\_\_ DATE \_\_\_\_\_  
PARKS AND RECREATION

CHECKED BY: \_\_\_\_\_ DATE \_\_\_\_\_  
TRAFFIC ENGINEER

CHECKED BY: \_\_\_\_\_ DATE \_\_\_\_\_

Drawing name: X:\S\_DRAWING\16845002\_CORE\_FortCollins\_UTILITY PLAN\10-12 OF 12 - CONSTRUCTION DETAILS.dwg Jun 09, 2017 11:34am by steve.paine  
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DRAWN BY: JPM  
CHECKED BY: LEN

**CORE**  
CONSTRUCTION  
DETAILS

CHOICE CENTER  
THIRD FILING  
FORT COLLINS, CO 80525

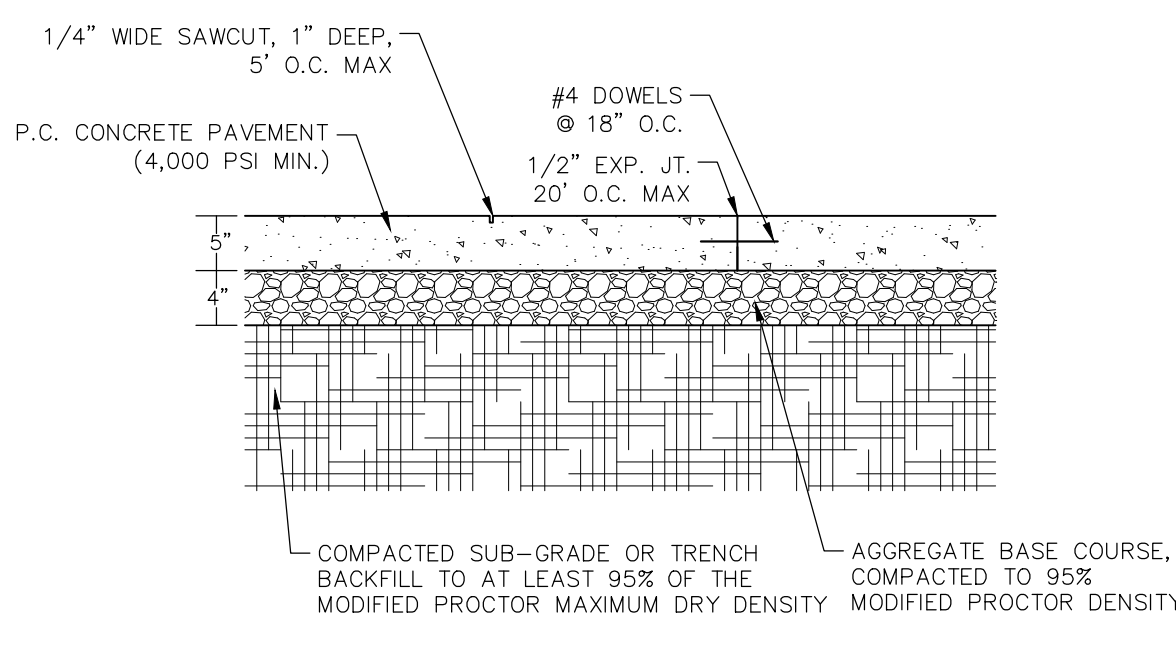
CITY OF FORT COLLINS  
UTILITY PLAN APPROVAL

APPROVED:	CITY ENGINEER	DATE
CHECKED BY:	WATER AND WASTEWATER UTILITY	DATE
CHECKED BY:	STORMWATER UTILITY	DATE
CHECKED BY:	PARKS AND RECREATION	DATE
CHECKED BY:	TRAFFIC ENGINEER	DATE
CHECKED BY:		DATE

ORIGINAL ISSUE:  
12/07/2016  
KHA PROJECT NO.  
168524002  
SHEET NUMBER  
11 OF 12

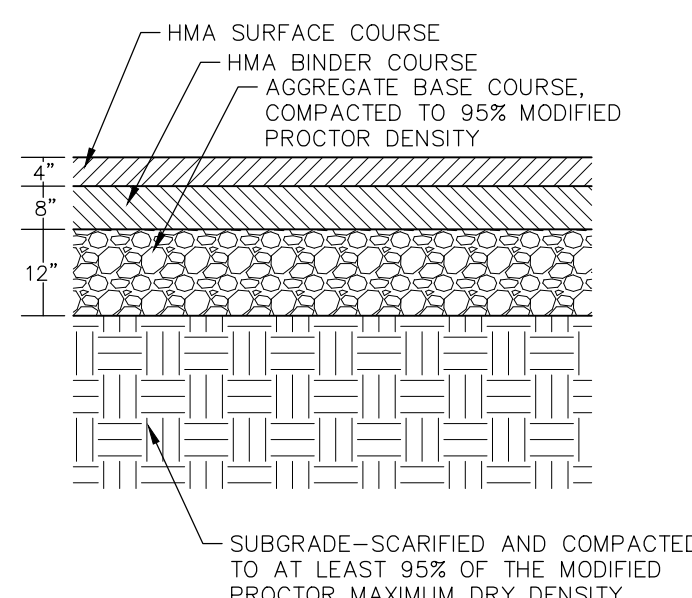
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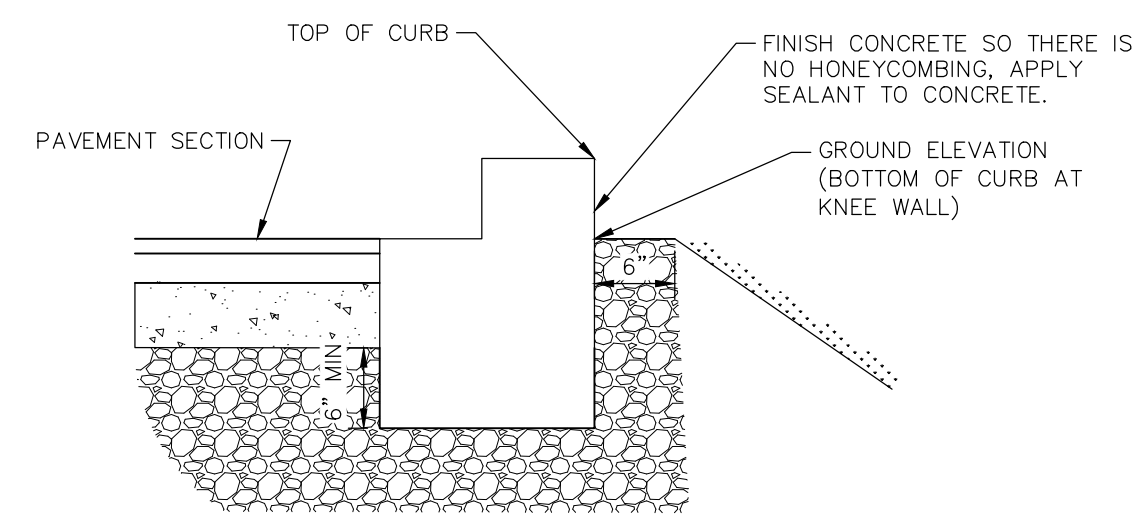
NOTES:  
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 CONCRETE COMPRESSIVE STRENGTH SHALL COMPLY WITH ASTM DESIGNATION C31. MINIMUM 28-DAY STRENGTH = 4000 PSI

**ONSITE HEAVY DUTY CONCRETE PAVEMENT SECTION**  
 N.T.S.

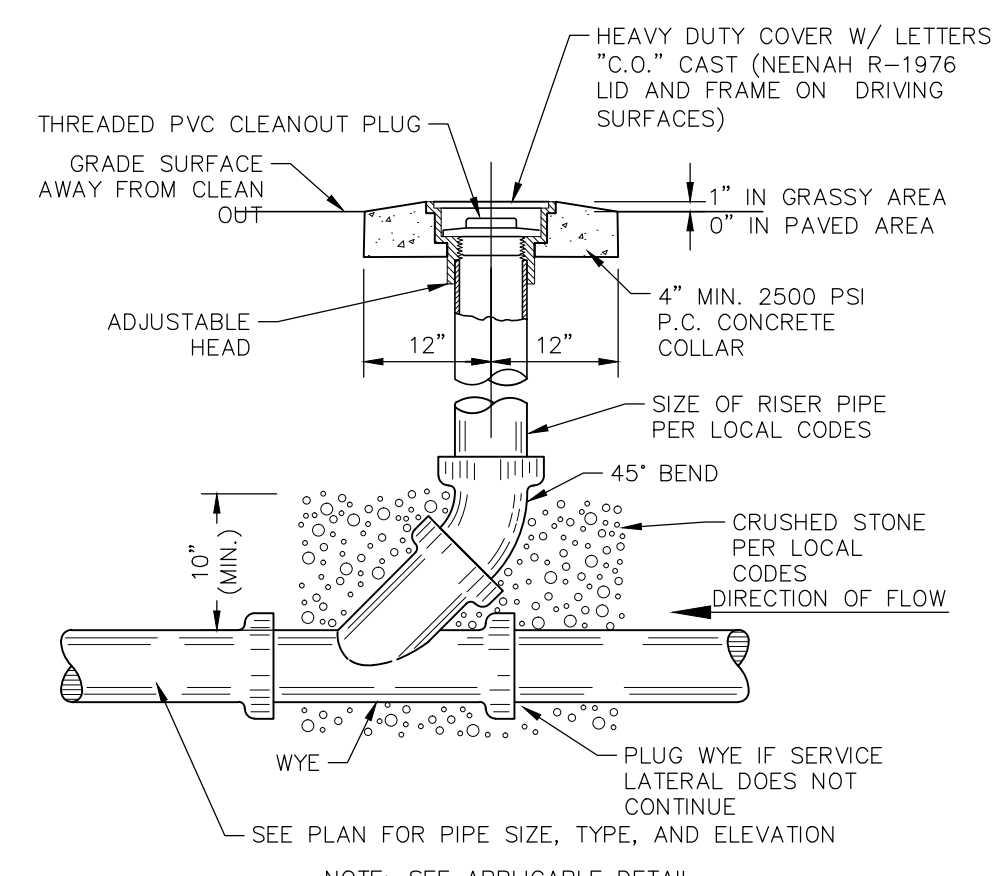


NOTE:  
 1. PAVEMENT THICKNESS, SUBGRADE PREP, ETC. DETERMINED FROM GEOTECHNICAL REPORT PERFORMED BY EARTH ENGINEERING CONSULTANTS, LLC. DATED 10/27/2016. REFER TO REPORT FOR ALL INFORMATION RELATED TO PAVING.

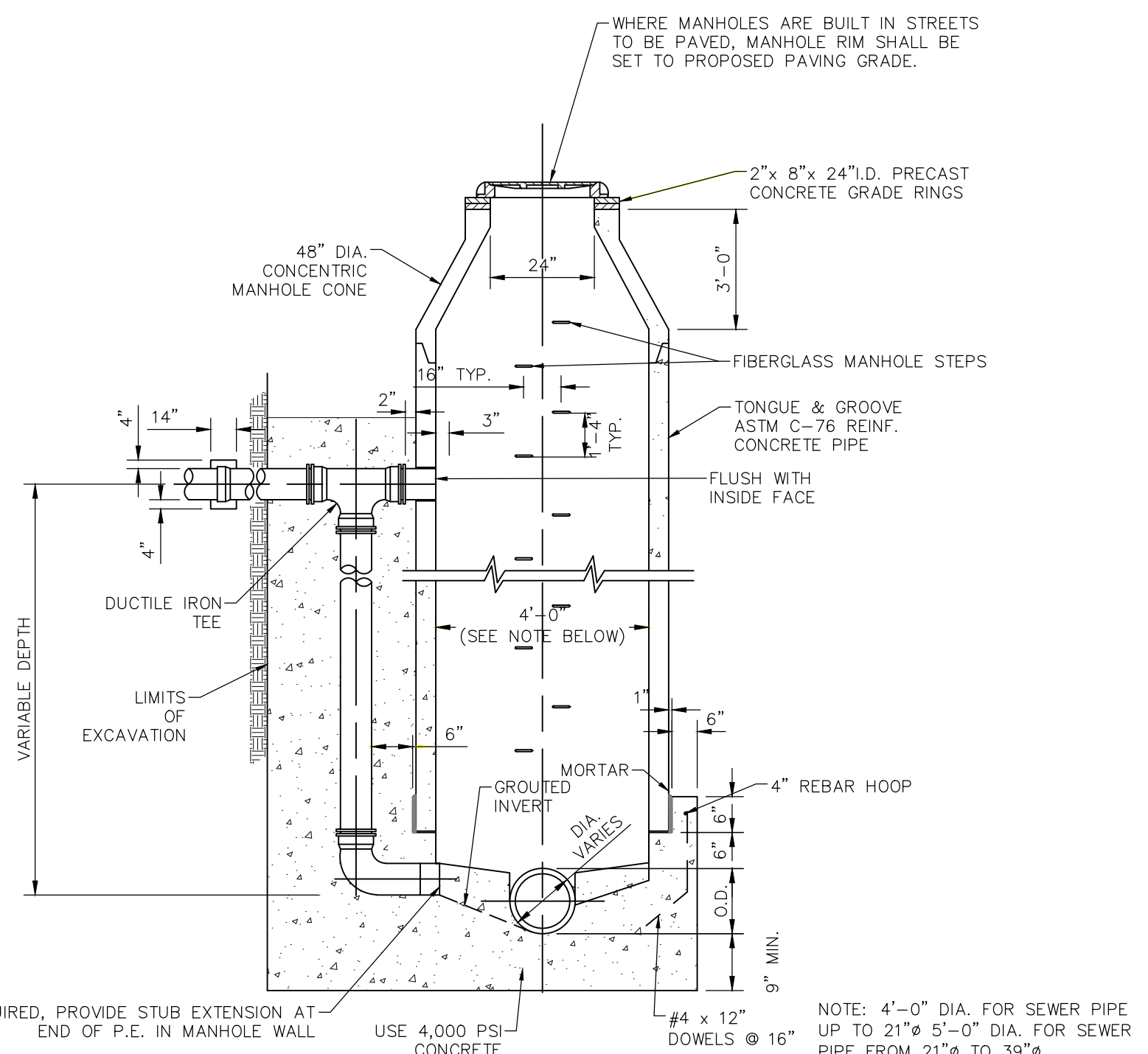
**ONSITE HEAVY DUTY ASPHALTIC PAVEMENT SECTION**  
 N.T.S.



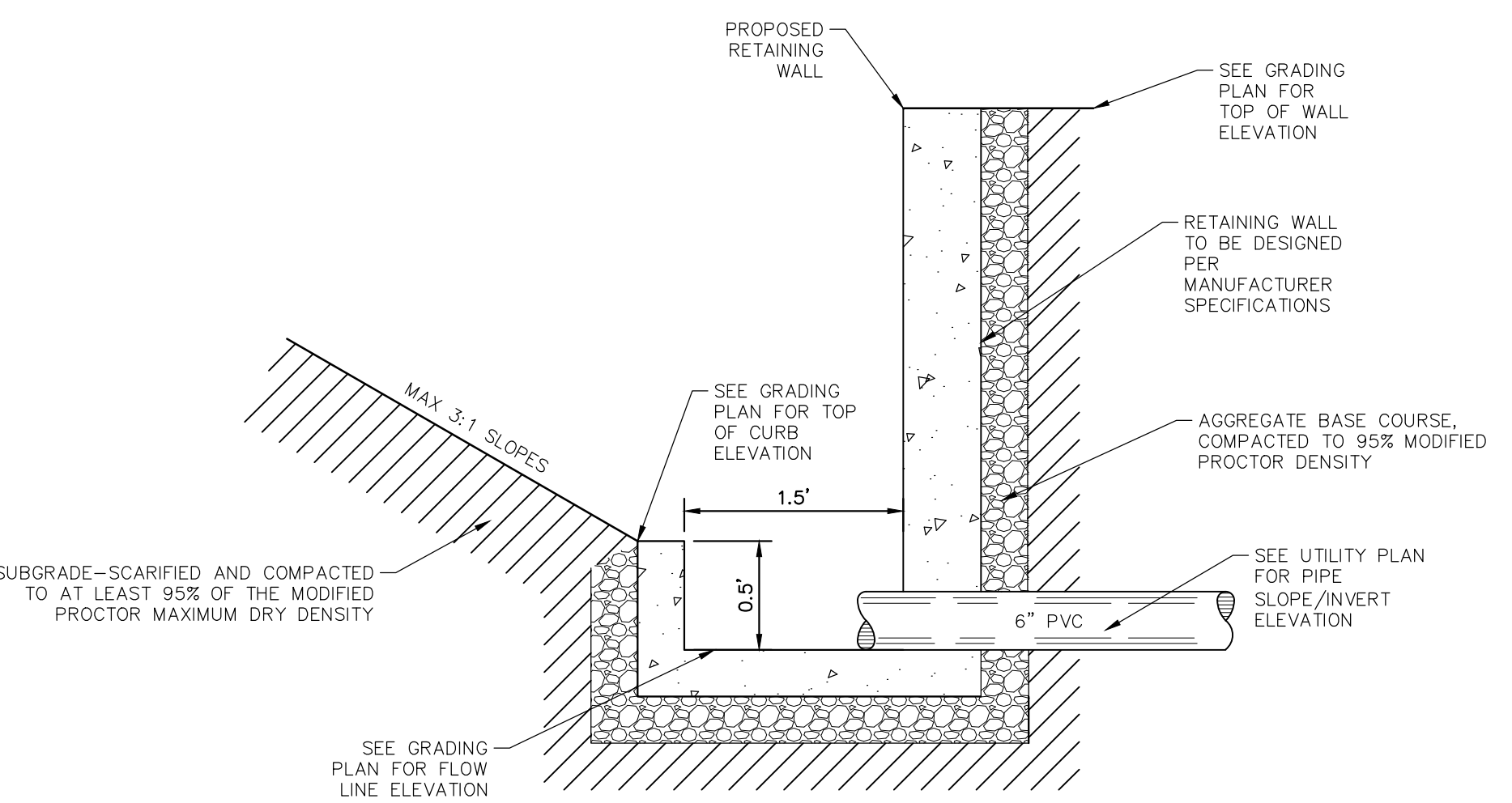
**KNEE WALL CURB**  
 N.T.S.



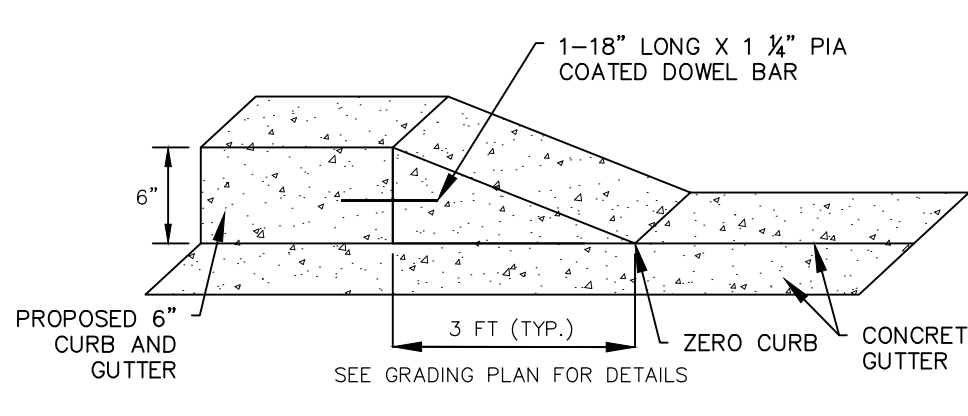
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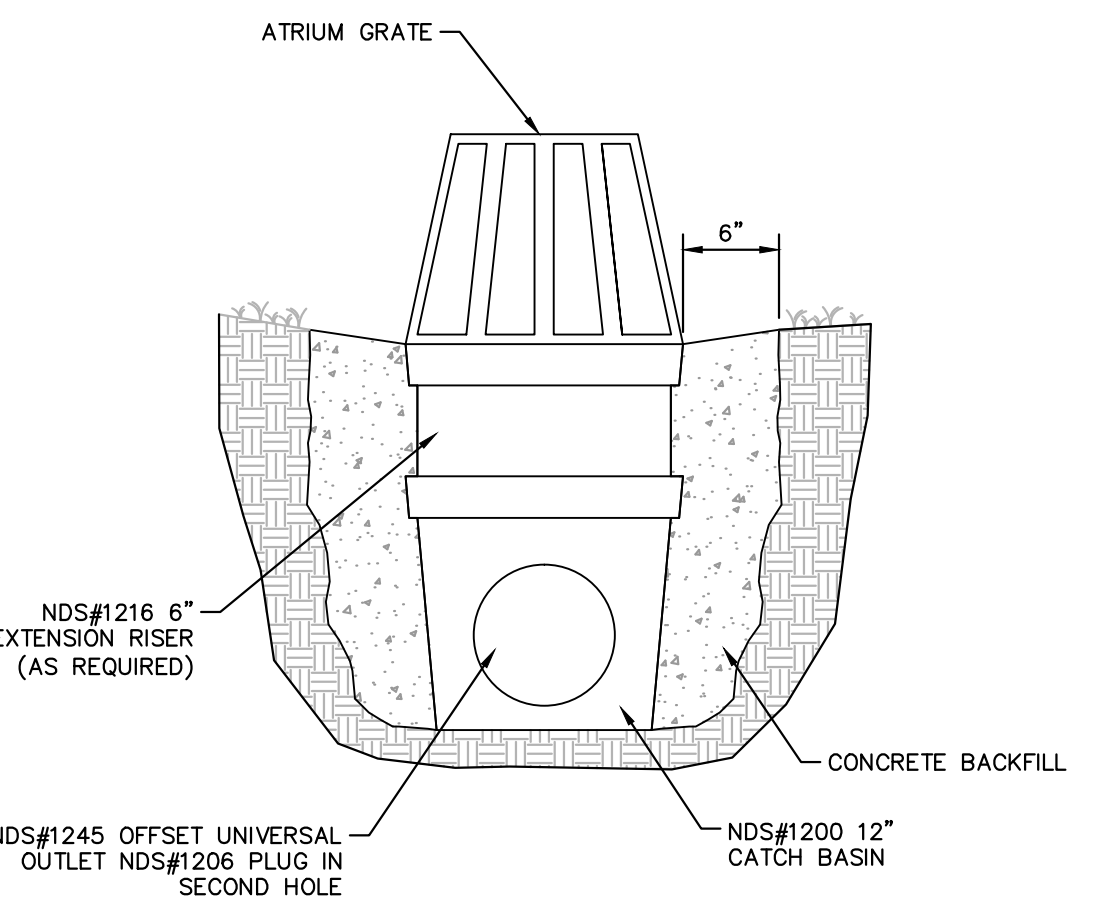
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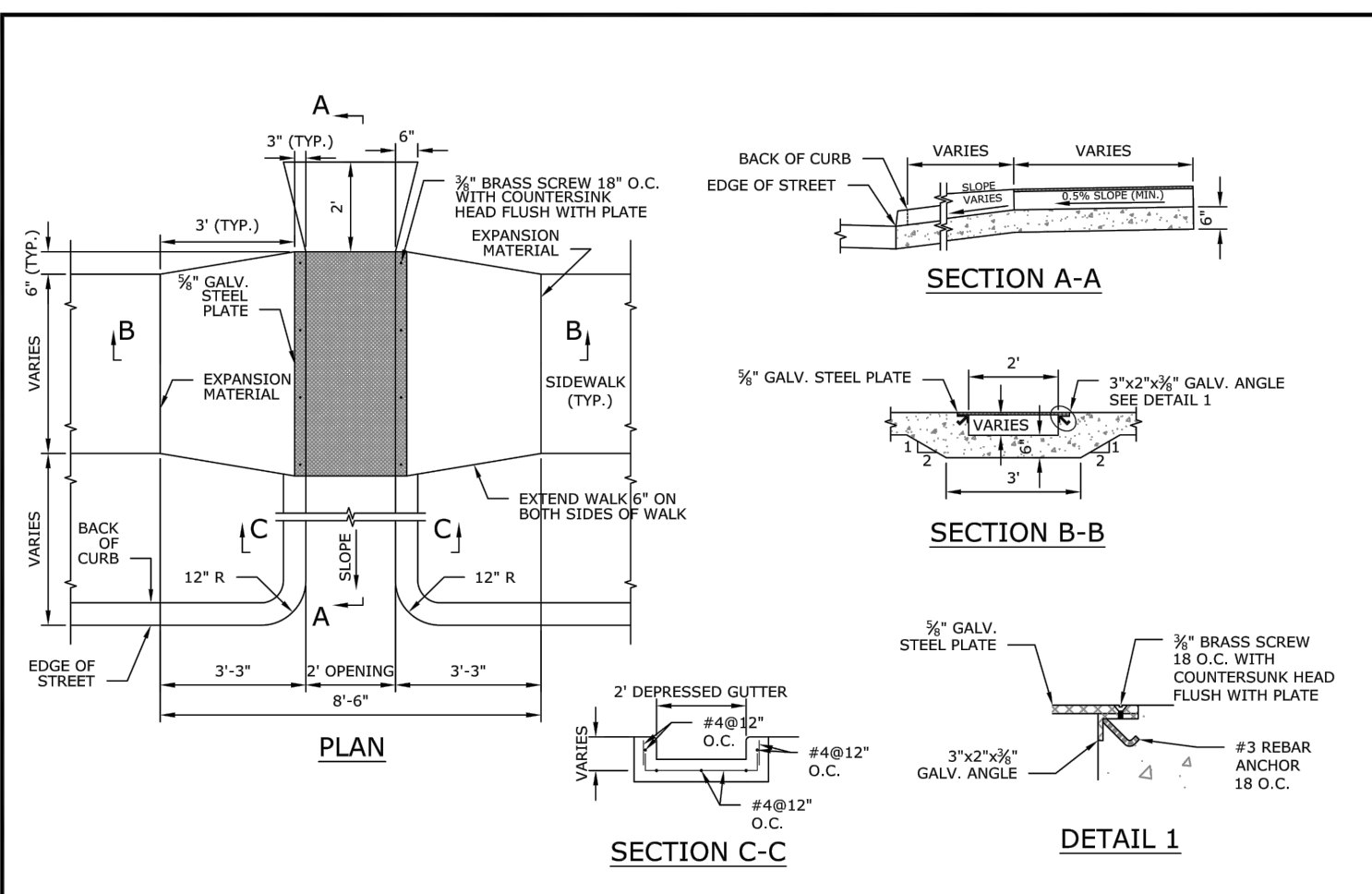
**CONCRETE CHANNEL/STORM OUTFALL TO CHANNEL (LOCATED AT SOUTHWEST CORNER OF SITE)**  
 N.T.S.



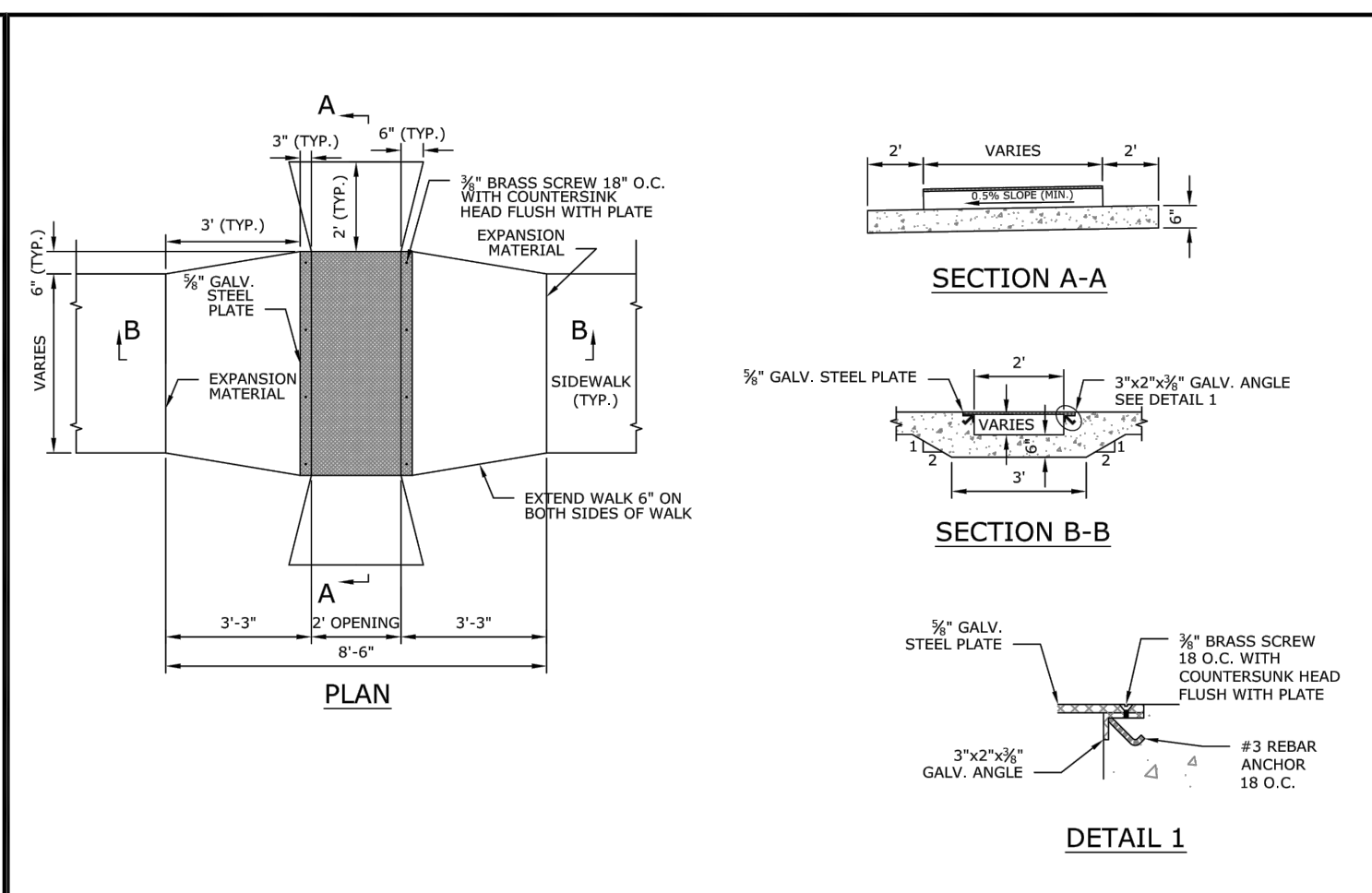
**ZERO CURB TRANSITION**  
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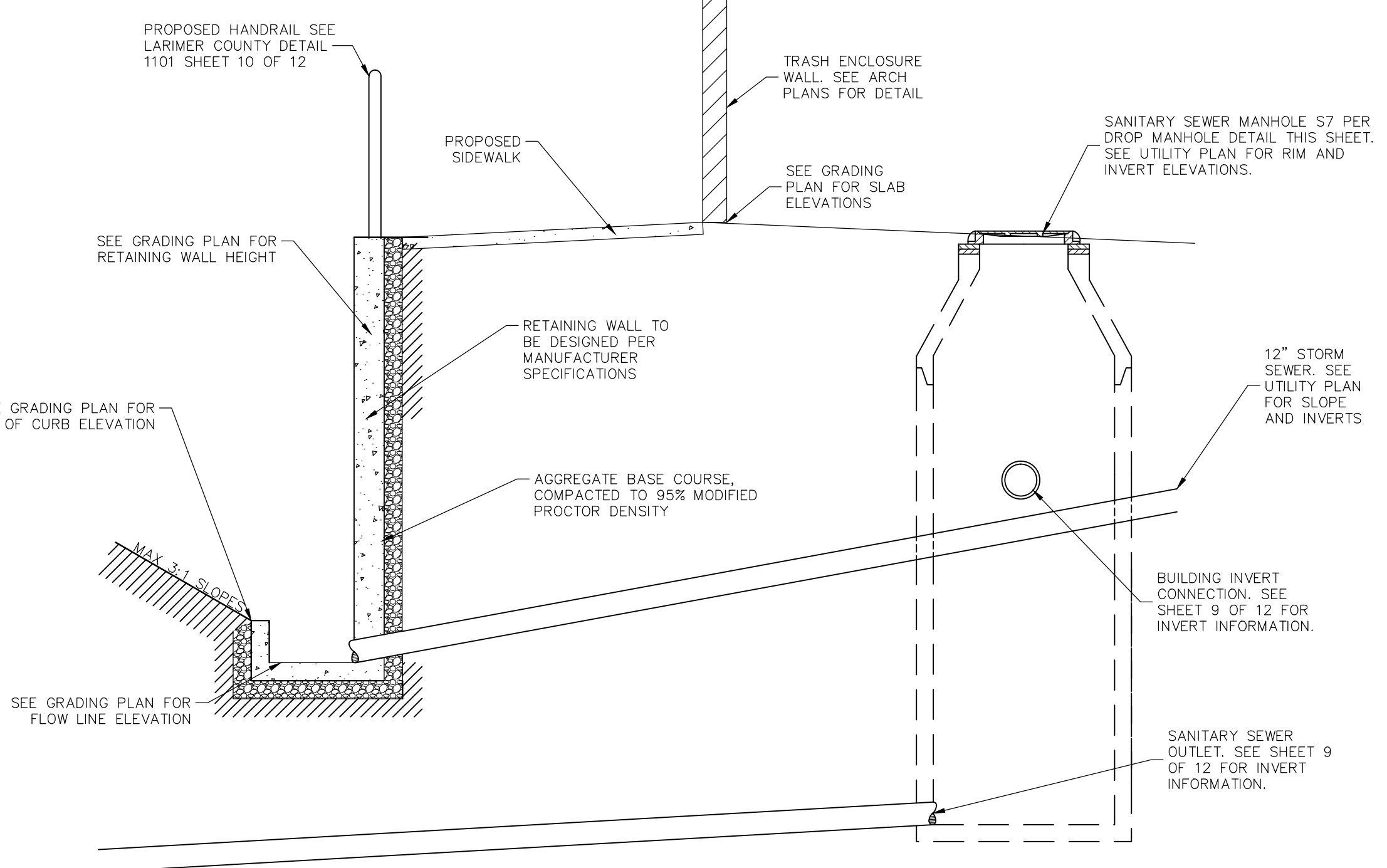
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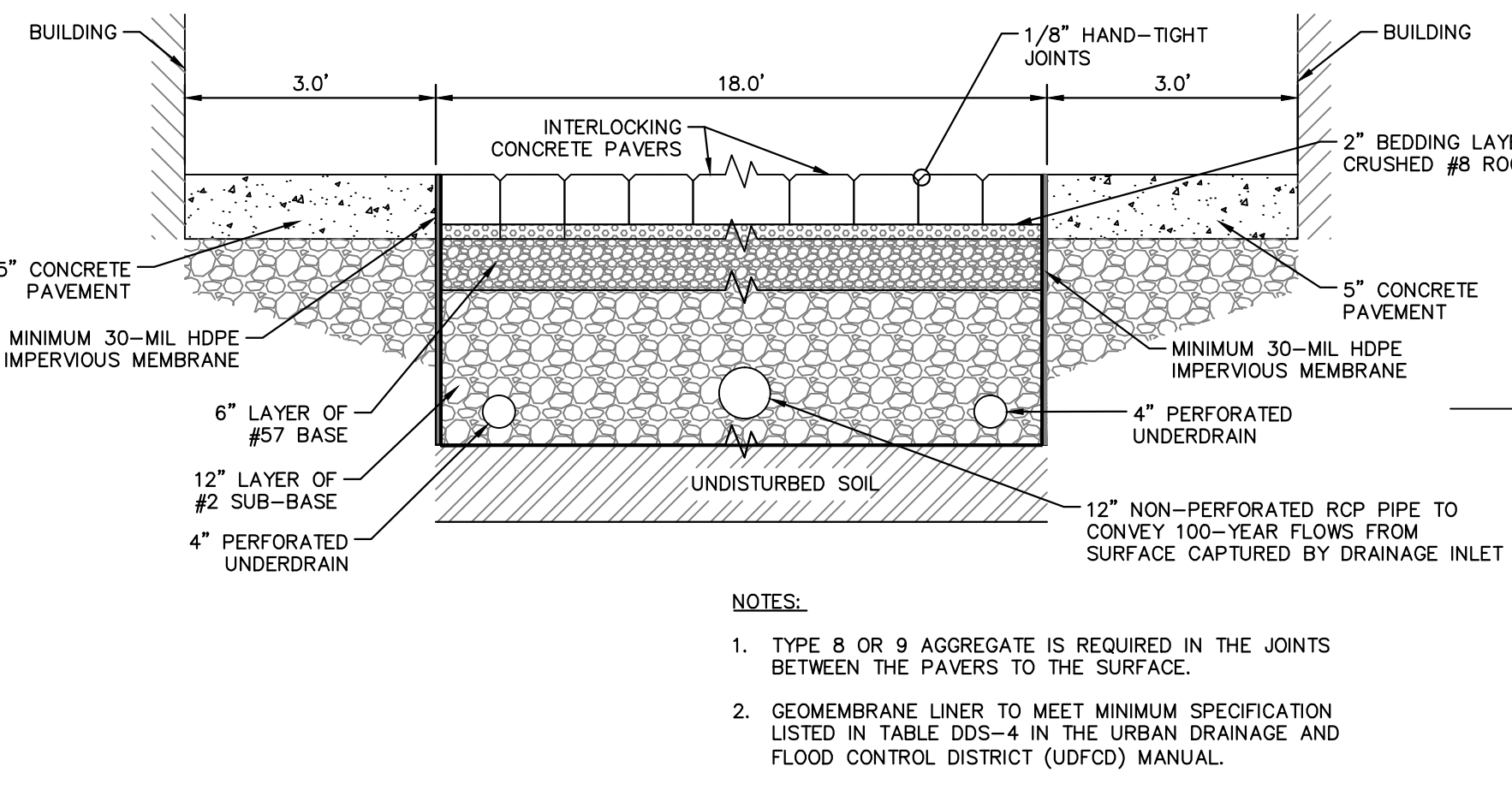
**METAL SIDEWALK CULVERT FOR VERT. CURB & GUTTER AND DETACHED SIDEWALK - OUTFALL TO CURB**  
 CITY OF FORT COLLINS UTILITY DEPARTMENT  
 APPROVED: DATE: 7/30/2013  
 DRAWN BY: D. MOSEN



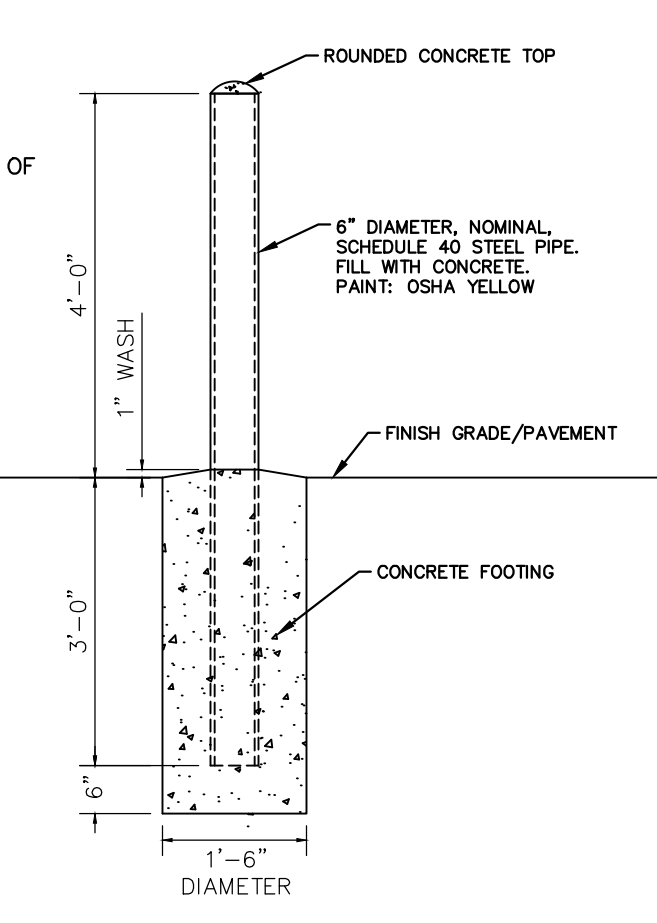
**METAL SIDEWALK CULVERT WITH NO CURB**  
 CITY OF FORT COLLINS UTILITY DEPARTMENT  
 APPROVED: DATE: 7/30/2013  
 DRAWN BY: D. MOSEN



**SECTION C-C: DUMPSTER DRAIN CROSS SECTION**  
 N.T.S.



**PERVIOUS CONCRETE PAVERS**  
 N.T.S.



**6\"/>
 N.T.S.**

CITY OF FORT COLLINS UTILITY PLAN APPROVAL		
APPROVED:	_____	DATE _____
CHECKED BY:	CITY ENGINEER	DATE _____
CHECKED BY:	WATER AND WASTEWATER UTILITY	DATE _____
CHECKED BY:	STORMWATER UTILITY	DATE _____
CHECKED BY:	PARKS AND RECREATION	DATE _____
CHECKED BY:	TRAFFIC ENGINEER	DATE _____
CHECKED BY:	_____	DATE _____

PDP COMMENTS/SITE PLAN REVISION	1		02/07/17	LEN
	2		05/02/17	LEN
	3		06/09/17	LEN
PDP COMMENTS				BY
NO				DATE

SCALE: AS NOTED	DESIGNED BY: JPM
DRAWN BY: JPM	CHECKED BY: LEN

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**CORE**

**CONSTRUCTION DETAILS**

**CHOICE CENTER THIRD FILING**  
 FORT COLLINS, CO 80525

ORIGINAL ISSUE: 12/07/2016  
 KHA PROJECT NO. 168524002  
 SHEET NUMBER: 12 OF 12

Drawing name: K:\GIS\_LIVE\16846002\_CORE\_FortCollins\_CO\_Collage\2\_Design\CAD\PlanSheets\UTILITY PLAN\10-12 - CONSTRUCTION DETAILS.dwg 12 OF 12 CONSTRUCTION DETAILS Jun 09, 2017 11:35am by steven.paine  
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November 30, 2016

Mr. Chad J. Matesi  
Core Spaces  
2234 W North Avenue  
Chicago, IL 60647

Re: Trip Generation Letter  
The Hub Town Homes  
West Side of College Avenue (US-287), Between Parker Street and Stuart Street  
1751 South College Avenue  
Fort Collins, Colorado

Dear Mr. Matesi,

The purpose of this letter is to provide trip generation for the proposed Hub Town Homes to be located along the west side of College Avenue (US-287), between Parker Street and Stuart Street, in Fort Collins, Colorado. Specifically, the project is to be located at 1751 South College Avenue. A vicinity map of the project area is illustrated in **Figure 1** attached. The new Hub Town Homes project is proposed to include 10 town home units in two buildings with 29 beds. Site plans are attached with this letter.

The Hub Town Homes are located at 1751 South College Avenue, next to the Summit student housing development. The site is developing with 10 two-story town homes. Nine of the units will include three bedrooms, while one of the units will include two bedrooms, for a total of 29 beds. Each unit has an attached two car garage, except for the two-bedroom unit which has an attached one car garage. Circulation through the site will be provided by a north-south access roadway in between the two buildings, with five (5) town homes located on each side with garage door access fronting the interior roadway. Of relevance, this proposed development is near the Mason Corridor Transitway to the west of the site. This project is also located within the City of Fort Collins identified Transit Oriented Development (TOD) zone. Based on this, it is believed that alternate modes of travel will be used by the residents of these town homes.

Primary access to the site will be provided by Prospect Road and College Avenue (US-287). Direct access to the project is proposed from the existing right-in/right-out access along College Avenue (US-287) along the south side of the site, which is located approximately 180 feet (measured edge to edge) north of Stuart Street. The existing northern right-in/right-out access, located approximately 300 feet (again measured edge to edge) north of Stuart Street will be removed with development of the project. The removal of this access will likely require a Colorado Department of Transportation (CDOT) access permit. Shared cross access will be available through this project site and is available through the existing adjacent properties as well. Due to this, it is believed that project traffic will use the existing three-quarter movement intersection along College Avenue located approximately 350 feet (edge to edge) north of the existing southern project access. Project traffic may also use the existing internal drive aisles through the shopping center to the north to access Prospect Road.

College Avenue (US-287) is owned and maintained by CDOT. The State Highway Access Category Schedule categorizes the segment of College Avenue (US-287) through the study area as NR-B: Non Rural Arterial. College Avenue provides three lanes of travel each direction, northbound and southbound, and has a posted speed limit of 40 miles per hour. College Avenue has a raised median through the project study area. The existing intersections of College Avenue (US-287) with Prospect Road and Stuart Street are signalized.

Site-generated traffic estimates are determined through a process known as trip generation. Rates and equations are applied to the proposed land use to estimate traffic generated by the development during a specific time interval. Based on the unique nature of this use with close proximity to the Colorado State University campus and being within the TOD zone, it was determined through discussions with City staff that the City of Fort Collins developed Student Housing trip generation rates were a more accurate representation of expected site generated trips for this project. These rates are 2.65 trips per bed per day, with a morning peak hour trip rate of 0.19 trips per bed and the afternoon peak hour rate being 0.42 trips per bed. Parking counts at the District as identified within “The Hub Parking Generation Letter”, completed by Kimley-Horn and Associates, Inc in October 2016 were used in determining the proportion of entering and exiting traffic during the peak hours. Based on this, the Hub Town Homes are expected to generate approximately 77 daily weekday trips. Of these, six (6) trips are expected to occur during the morning peak hour, while 12 trips are expected during the afternoon peak hour.

To provide comparative trip generation calculation, Kimley-Horn used the ITE Trip Generation, 9<sup>th</sup> Edition (current), average rate equations that apply to Apartment (ITE Code 220). The acknowledged source for trip generation rates is the *Trip Generation Report*<sup>1</sup> published by the Institute of Transportation Engineers (ITE). ITE has established trip rates in nationwide studies of similar land uses. Based on this, 10 units of the Hub Town Homes if anticipated to be apartments would be expected to generate approximately 66 daily weekday trips. Of these, five (5) trips would be expected to occur during the morning peak hour, while six (6) driveway trips are expected during the afternoon peak hour. Therefore, the equations for the student housing provide a more conservative analysis than the average rates for the ITE Apartment land use code. Again, this is provided for informational purposes only.

**Trip Generation – The Hub Town Homes**

USE AND SIZE	DAILY VEHICLE TRIPS	WEEKDAY VEHICLE TRIPS					
		AM Peak Hour			PM Peak Hour		
		In	Out	Total	In	Out	Total
<b>The Hub Town Homes</b>							
<b>City of Fort Collins Rates</b>	<b>77</b>	<b>1</b>	<b>5</b>	<b>6</b>	<b>6</b>	<b>6</b>	<b>12</b>
ITE Rates	66	1	4	5	4	2	6

As summarized in the table, The Hub Town Homes are anticipated to generate 77 daily weekday trips. Of these, six (6) trips are expected to occur during the weekday morning peak hour while 12 trips are expected to occur during the afternoon peak hour.

Distribution of site traffic on the street system was based on the area street system characteristics, existing traffic patterns, existing and anticipated surrounding development areas and type, expected roadway improvements, and the proposed access system for the project. The directional distribution of traffic is a means to quantify the percentage of site-generated traffic that approaches the site from a given direction and departs the site back to the original source. Traffic assignment was obtained by applying the distribution to the estimated traffic generation of the project shown in the above **Table**. The attached **Figure 2** illustrates the expected project trip distribution and traffic assignment. As

<sup>1</sup> Institute of Transportation Engineers, *Trip Generation: An Information Report*, Ninth Edition, Washington DC, 2012.

shown in the Figure, the resultant traffic assignment is very small at all anticipated points of access to the adjacent public street network.

Based on these results, development of The Hub Town Homes, proposed to include 10 town home units with 29 beds at 1751 South College Avenue is not anticipated to cause a significant impact to the surrounding street network due to the low trip generation. The Hub Town Homes are anticipated to generate 77 daily weekday trips. Of these, six (6) trips are expected to occur during the morning peak hour while 12 trips are expected to occur during the afternoon peak hour. Therefore, based on conversations with City staff, it is believed that additional traffic analysis will not be needed for this project. If you have any questions or require anything further, please feel free to call me at (303) 228-2304.

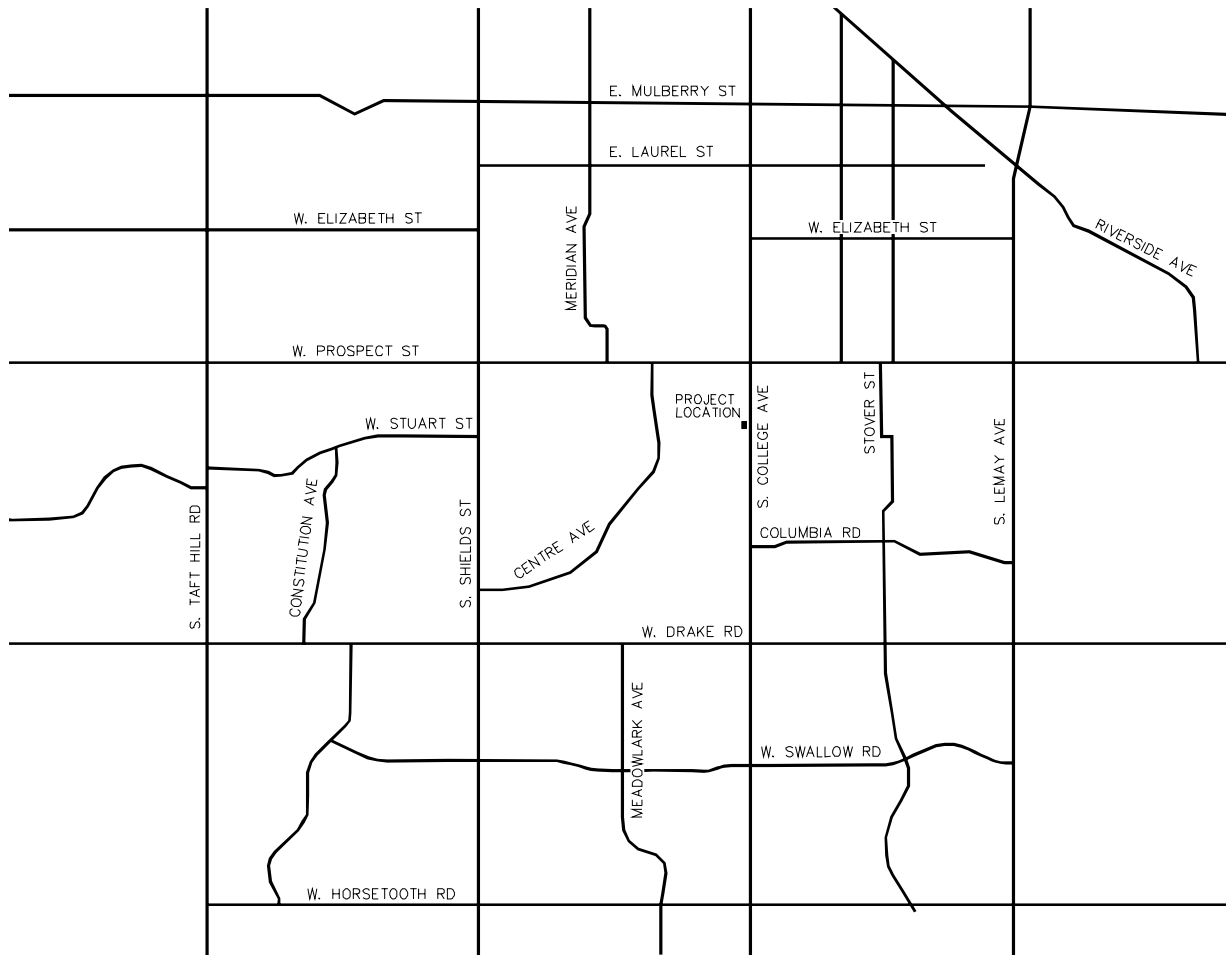
Sincerely,

KIMLEY-HORN AND ASSOCIATES, INC.



Curtis D. Rowe, P.E., PTOE  
Vice President

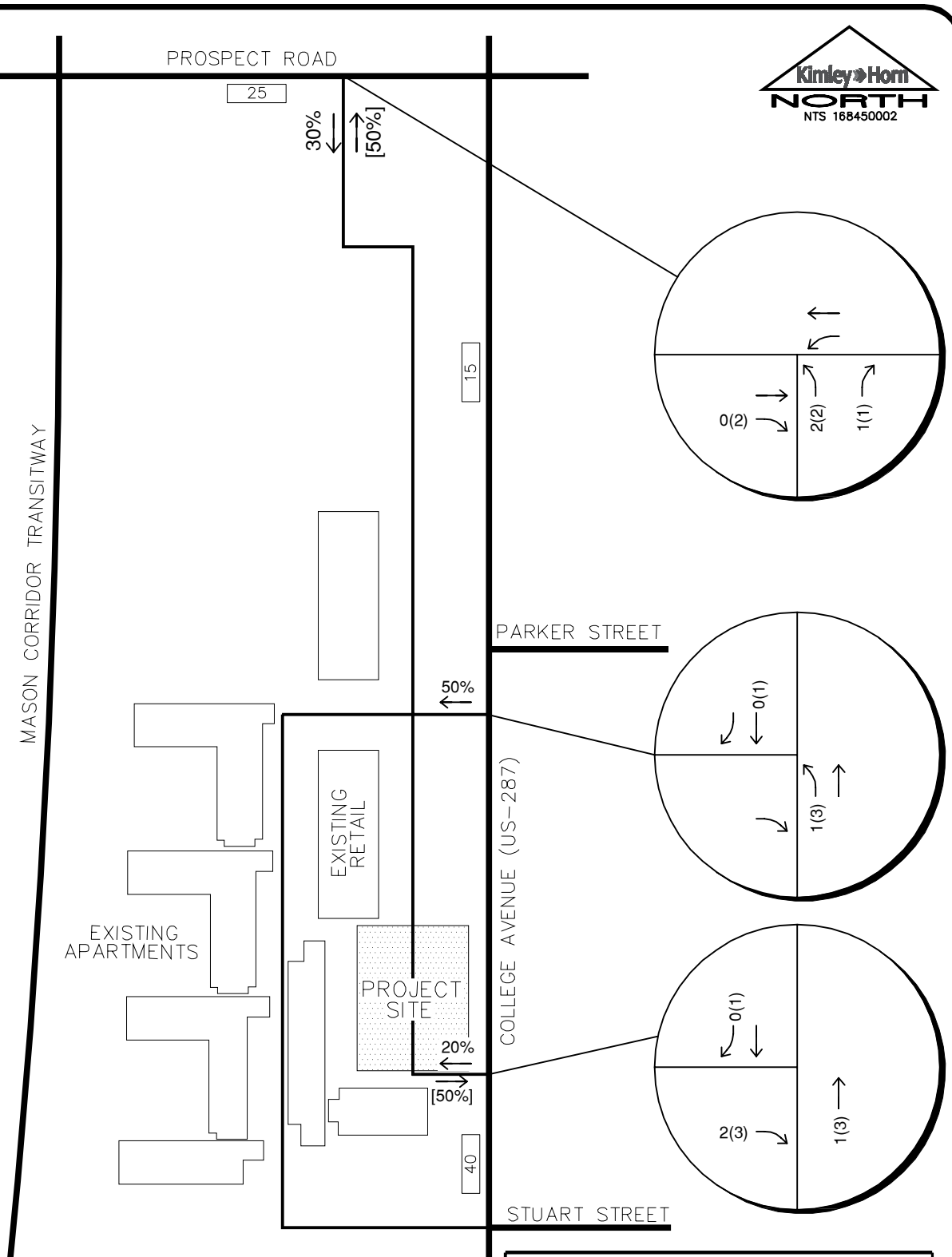




HUB TOWN HOMES  
1751 SOUTH COLLEGE AVENUE  
FORT COLLINS, COLORADO  
VICINITY MAP

FIGURE 1





HUB TOWN HOMES  
 1751 SOUTH COLLEGE AVENUE  
 FORT COLLINS, COLORADO  
 TRIP DISTRIBUTION & ASSIGNMENT

**LEGEND**

XX%[XX%] Entering[Exiting]  
 Trip Distribution Percentage

XXX(XXX) Weekday AM(PM)  
 Peak Hour Traffic Volumes

[XX,X00] Estimated Daily Traffic Volume

FIGURE 2

**Student  
Housing**

ADT            2.65 /bed  
 AM peak hr    0.19  
 PM peak hr    0.42

Directional Distribution: AM 24% in and 76% out, PM 52% in and 48% out

Scenario	Vehicles Trips						
	Daily	Weekday AM Peak Hour			Weekday PM Peak Hour		
		In	Out	Total	In	Out	Total
<b>Student Housing (29 Beds)</b>	77	1	5	6	6	6	12

Project Hub Town Homes  
 Subject Trip Generation for Apartment  
 Designed by Matt Farnen Date November 16, 2016 Job No. 168450002  
 Checked by Curtis Rowe Sheet No. 1 of 1

## TRIP GENERATION MANUAL TECHNIQUES

ITE Trip Generation Manual 9th Edition, Average Rate

Land Use Code - Apartment, (220)

Independent Variable - Dwelling Units (X)

$$X = 10$$

T = Average Vehicle Trip Ends

### Peak Hour of Adjacent Street Traffic, One Hour Between 7 and 9 a.m. (page 334)

Average Weekday  
 T = 0.51 (X)  
 T = 0.51 \* 10.0

Directional Distribution: 20% ent. 80% exit.  
 T = 5 Average Vehicle Trip Ends  
 1 entering 4 exiting  
 1 + 4 = 5

### Peak Hour of Adjacent Street Traffic, One Hour Between 4 and 6 p.m. (page 335)

Average Weekday  
 T = 0.62 (X)  
 T = 0.62 \* 10.0

Directional Distribution: 65% ent. 35% exit.  
 T = 6 Average Vehicle Trip Ends  
 4 entering 2 exiting  
 4 + 2 = 6

### Weekday (page 333)

Average Weekday  
 T = 6.65 (X)  
 T = 6.65 \* 10.0

Directional Distribution: 50% entering, 50% exiting  
 T = 66 Average Vehicle Trip Ends  
 33 entering 33 exiting  
 33 + 33 = 66

**SOUTH COLLEGE AVE.**



